

**CITY OF UNLEY**  
**COUNCIL ASSESSMENT PANEL**

Dear Member

I write to advise of the Council Assessment Panel Meeting to be held on Tuesday 19 July 2022 at 6:00pm in the Unley Council Chambers, 181 Unley Road Unley.



**Don Donaldson**  
**ASSESSMENT MANAGER**

**Dated 11/07/2022**

**KAURNA ACKNOWLEDGEMENT**

*Ngadlurlu tampinhi, ngadlu Kurna yartangka inparrinhi. Ngadlurlu parnuku tuwila yartangka tampinhi.*

*Ngadlurlu Kurna Miyurna yaitya yarta-mathanya Wama Tarntanyaku tampinhi. Parnuku yaitya, parnuku tapa purruna yalarra puru purruna.\**

We would like to acknowledge this land that we meet on today is the traditional lands for the Kurna people and that we respect their spiritual relationship with their country.

We also acknowledge the Kurna people as the traditional custodians of the Adelaide region and that their cultural and heritage beliefs are still as important to the living Kurna people today.

\*Kurna Translation provided by Kurna Warra Karrpanthi

**CITY OF UNLEY**  
**COUNCIL ASSESSMENT PANEL**

**19 July 2022**

**MEMBERS:**

Mr Brenton Burman  
Mrs Colleen Dunn  
Mr Ross Bateup  
Ms Carol Muzyk  
Ms Emma Wright

**APOLOGIES:**

Mr Michael McKeown

**CONFLICT OF INTEREST:**

**CONFIRMATION OF MINUTES:**

MOVED:

SECONDED:

That the Minutes of the City of Unley, Council Assessment Panel meeting held on Tuesday 21 June 2022, as printed, and circulated, be taken as read and signed as a correct record.



## **A G E N D A**

Apologies  
Conflict of Interest  
Confirmation of the minutes

<b>Item No</b>	<b>Development Act Applications</b>	<b>Page</b>
	Nil	

<b>Item No</b>	<b>Planning, Development Infrastructure Act Applications</b>	<b>Page</b>
1.	17-23 Foster Street, Parkside - 22015437	4-382

<b>Item No</b>	<b>Appeals Against Decision of Assessment Manager (PDI Act)</b>	<b>Page</b>
	Nil	

<b>Item No</b>	<b>ERD Court Compromise Reports - CONFIDENTIAL</b>	<b>Page</b>
	<b>Motion to move into confidence</b>	
	Nil	
	<b>Motion to move out of confidence</b>	

<b>Item No</b>	<b>Council Reports</b>	<b>Page</b>
2.	CAP Annual Report 2021-22	<i>Under Separate Cover</i>

**Any Other Business**  
**Matters for Council's consideration**

<b>DEVELOPMENT NO:</b>	22015437
<b>APPLICANT:</b>	St Louis Aged Care C-/ Future Urban
<b>ADDRESS:</b>	17 FOSTER ST PARKSIDE SA 5063 23 FOSTER ST PARKSIDE SA 5063 19-21 FOSTER ST PARKSIDE SA 5063
<b>NATURE OF DEVELOPMENT:</b>	Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)
<b>ZONING INFORMATION:</b>	<b>Zones:</b> <ul style="list-style-type: none"> <li>• Established Neighbourhood</li> </ul> <b>Overlays:</b> <ul style="list-style-type: none"> <li>• Airport Building Heights (Regulated)</li> <li>• Historic Area</li> <li>• Prescribed Wells Area</li> <li>• Regulated and Significant Tree</li> <li>• Stormwater Management</li> <li>• Urban Tree Canopy</li> <li>• Water Resources</li> </ul> <b>Technical Numeric Variations (TNVs):</b> <ul style="list-style-type: none"> <li>• Maximum Building Height (Metres)</li> <li>• Minimum Frontage</li> <li>• Minimum Site Area</li> <li>• Maximum Building Height (Levels)</li> <li>• Minimum Side Boundary Setback</li> <li>• Site Coverage</li> </ul>
<b>LODGEMENT DATE:</b>	11 May 2022
<b>RELEVANT AUTHORITY:</b>	Assessment Panel
<b>PLANNING &amp; DESIGN CODE VERSION:</b>	28 April 2022 - 2022.7
<b>CATEGORY OF DEVELOPMENT:</b>	Code Assessed - Performance Assessed
<b>NOTIFICATION:</b>	Yes
<b>RECOMMENDING OFFICER:</b>	Mark Troncone Planning Officer
<b>REFERRALS STATUTORY:</b>	N/A
<b>REFERRALS NON-STATUTORY:</b>	Anthony Barbara - Works (Crossover) Jacob Avery – Traffic

## CONTENTS:

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<b>ATTACHMENT 1:</b>	Application Documents
<b>ATTACHMENT 2:</b>	Representations
<b>ATTACHMENT 3:</b>	Response to Representations
<b>ATTACHMENT 4:</b>	Internal Referral Advice
<b>ATTACHMENT 5:</b>	Relevant P&D Code Policies

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## DETAILED DESCRIPTION OF PROPOSAL:

The application proposes the following development the existing St Louis Aged Care facility:

- 10-bedroom addition located at 17 Foster Street
- 11 on-site car parks including acoustic fencing located at 23 Foster Street

### 10 Bedroom Addition

The proposed addition will include 10 bedrooms and a dining/lounge room which will connect to the existing aged care facility on 19-21 Foster Street. The proposed addition will maintain the existing verandah and front façade of the existing character dwelling. A brick/steel fence will be erected along the northern boundary adjacent Foster Street. Landscaping is proposed within the front yard area which will include a mixture of hedges, climbers, bushes and ground cover.

### Car Park and Fencing

The proposed car park will include 11 spaces (9 visitor parking and 2 staff parking) with an existing disabled park to be relocated to the front of 23 Foster Street. In addition to the existing 7 car parks (6 visitor parking and 1 accessible park) a total of 18 car parks including 1 disabled park will be located upon the site. A 2.4 colorbond acoustic fence has been proposed along the western and southern boundaries adjacent 25 Foster Street & 38 Blyth Street. A 0.6m landscaping buffer will be located between the car parks and the fences. The car park will utilise the existing driveway with the crossover to be widened to allow for two-way access and egress.

## BACKGROUND:

A number of development applications have been lodged for the subject land of 17-23 Foster Street relative to the St Louis Aged Care facility including:

### **17 Foster Street**

- **589/2019/C2/1&2:** Carry out alterations to existing aged care facility and undertake a change of use to 17 Foster St (from detached dwelling to dwelling and aged care facility) including the demolition of swimming pool, sheds, verandah and construction of an aged care addition (comprising of 4 bed rooms, lounge and storage areas).

### **19-21 Foster Street**

- **7/1980/DA:** To establish a lodging house
- **40/1982/DA:** To construct additions to an existing nursing home
- **158/1982/BA:** Additions to kitchen of nursing home
- **269/1996/DA:** Refurbish existing nursing home maintaining existing beds & covering courtyard
- **1036/2002/DX:** Demolish one existing dwelling at 19 Foster Street and construct alterations and additions and alterations to existing Aged Care Facility Stage 2
- **552/2003/DL:** Land division - torrens title (realignment of internal boundaries)
- **589/2019/C2/1&2:** Carry out alterations to existing aged care facility and undertake a change of use to 17 Foster St (from detached dwelling to dwelling and aged care facility) including the demolition of swimming pool, sheds, verandah and construction of an aged care addition (comprising of 4 bed rooms, lounge and storage areas).



## SUBJECT LAND & LOCALITY:

### Site Description:

**Location reference:** 17 FOSTER ST PARKSIDE SA 5063

**Title ref.:** CT 5473/264 **Plan Parcel:** D1005 AL401 **Council:** CITY OF UNLEY

**Location reference:** 23 FOSTER ST PARKSIDE SA 5063

**Title ref.:** CT 5780/30 **Plan Parcel:** D1005 AL405 **Council:** CITY OF UNLEY

**Location reference:** 19-21 FOSTER ST PARKSIDE SA 5063

**Title ref.:** CT 5916/667 **Plan Parcel:** D64057 AL67 **Council:** CITY OF UNLEY

The subject land comprises of (3) three allotments as outlined above.

17 Foster Street has a frontage to Foster Street of approx. 24.5m and a total area of approx. 1175m<sup>2</sup>. The allotment contains an existing single storey bungalow style dwelling circa 1910 towards the front portion of the land and the eastern wing of the aged care facility at the rear portion. Existing vehicle access is located adjacent to the western boundary. The land is relatively flat and contains no regulated or significant vegetation upon the site.



**Figure 1:** View of 17 Foster Street as taken from Foster Street looking south

19-21 Foster Street has a frontage to Foster Street of approx. 55m and a total area of approx. 3360m<sup>2</sup>. The allotment contains the majority of the existing St Louis Aged Care facility including associated car parking (7 car parks). Existing vehicle access is via Foster Street adjacent to the western boundary. The land is relatively flat and contains no regulated or significant vegetation upon the site.





**Figure 2:** View of 19-21 Foster Street as taken from Foster Street looking south

23 Foster Street has a frontage to Foster Street of approx. 18m and a total area of approx. 860m<sup>2</sup>. The allotment contains a single storey bungalow style dwelling circa 1920. Existing vehicle access is located adjacent to the western boundary. The land is relatively flat and contains no regulated or significant vegetation upon the site.



**Figure 3:** View of 23 Foster Street as taken from Foster Street looking south

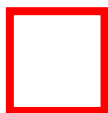


## Locality

In forming an opinion as to the extent of the locality I have considered the extent to which the proposed development on the subject land is likely to be evident to the surrounding occupiers and landowners.

The locality is characterised by dwellings on the northern side of Foster Street and a mixture of the St Louis Aged Care facility and dwellings on the southern side of the street. The northern side of Blyth Street adjacent the subject land consists of a mixture of dwellings and independent living units also operated by St Louis.

The development adjacent to the site consists of the secondary frontage of the dwelling at 102 Kenilworth Road (Figure 4) and the bungalow style dwelling to the west at 25 Foster Street (Figure 5).



Subject site



Locality



Representor





**Figure 4:** View of the secondary frontage of 102 Kenilworth Road as taken from Foster Street looking south



**Figure 5:** View of 25 Foster Street as taken from Foster Street looking south



**CONSENT TYPE REQUIRED:**

Planning Consent

**CATEGORY OF DEVELOPMENT:**

- **PER ELEMENT:**  
Other - Community - Residential aged care: Code Assessed - Performance Assessed
- **OVERALL APPLICATION CATEGORY:**  
Code Assessed - Performance Assessed
- **REASON**  
P&D Code

**PUBLIC NOTIFICATION**

- **REASON**  
Residential care/aged care facility is not listed as an exempt development within Table 5 of the Established Neighbourhood Zone. Public notification was therefore required.
- **LIST OF REPRESENTATIONS**

	Representor Name/Address	Support/Support with Concerns/Oppose	Request to be heard	Represented By
1	[REDACTED]	Oppose	No	
2	[REDACTED]	Support with some concerns	No	
3	[REDACTED]	Support with some concerns	Yes	[REDACTED]
4	[REDACTED]	Support with some concerns	Yes	
5	[REDACTED]	Support with some concerns	Yes	[REDACTED]
6	[REDACTED]	Support with some concerns	Yes	[REDACTED]
7	[REDACTED]	Support with some concerns	Yes	[REDACTED]
8	[REDACTED]	Support with some concerns	Yes	[REDACTED]



9	[REDACTED]	Support with some concerns	Yes	[REDACTED]
10	[REDACTED]	Support with some concerns	Yes	[REDACTED]
11	[REDACTED]	Support with some concerns	Yes	[REDACTED]
12	[REDACTED]	Support with some concerns	Yes	[REDACTED]
13	[REDACTED]	Support with some concerns	Yes	[REDACTED]
14	[REDACTED]	Support with some concerns	Yes	[REDACTED]

#### SUMMARY OF REPRESENTOR COMMENTS

- Design and siting
- Amenity impacts
  - Air pollution
  - Noise
  - Light
  - Privacy
- Traffic management and parking
- Delivery and waste collection
- Fencing

#### AGENCY REFERRALS

N/A

#### INTERNAL REFERRALS

- Anthony Barbara - Works: I have had a look at the proposed extent the existing crossover from Foster Street. From an assets perspective the proposed crossover extension would be supported.
- Jacob Avery - Traffic: I have reviewed the proposed extension of the existing Parkside St Louis Aged Care facility including ancillary car park, as well as the traffic report prepared by MFY, and have provided my comments (refer Attachment 5).

Note: No issues raised.

## PLANNING ASSESSMENT

The application has been assessed against the relevant provisions of the Planning & Design Code, which are contained in Attachment 4. Those of particular relevance are:

### Built Form

PO 1.3 of the Established Neighbourhood Zone states that non-residential development should be '*sited and designed to complement the residential character and amenity of the neighbourhood*'.

The proposed addition will be designed to maintain the verandah, front façade walls and fenestration of the existing character dwelling whilst an additional eastern portion will be constructed to match the appearance of the dwelling. The overall height, side and rear setbacks will be consistent with that outlined within the zone for a 'dwelling' type development which will ensure that the building will be consistent with the residential streetscape character without impacting the visual amenity of the street or surrounding dwellings.

Given the above, it is considered that the proposed built form will be consistent with PO 1.3 of the Established Neighbourhood Zone.

### Land Use and Amenity

PO 1.1 of the Established Neighbourhood Zone states that the zone should consist of '*predominantly residential development with complementary non-residential activities compatible with the established development pattern of the neighbourhood.*'

The proposed development will be an extension to the existing St Louis Aged Care facility which occupies 17-23 Foster Street (as outlined on the locality map above) and includes independent living units operated by St Louis at 34-36 Blyth Street south of the subject land. The proposal is therefore considered to be acceptable given the long established use and the significant presence of the facility within the locality.

PO 2.1 of the Interface between Land Uses section states that non-residential development should be designed so as to '*not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having regard to:*

*(a) the nature of development*

*(b) measures to mitigate off site impacts*

*(c) the extent to which the development is desired in the zone*

*(d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land'*

The proposed car park on 23 Foster Street will be located adjacent residential properties 25 Foster Street to the west and 38 Blyth Street to the south.

An acoustic report prepared by Echo Acoustic Consulting has been provided which details an assessment against the standards of the Planning and Design Code (the Code) and the *Environment Protection (Noise) Policy 2007* (the Policy).

The report concluded that the proposal would achieve the relevant standards of the Code and the Policy by implementing the following:

- Construction of a 2.4m high solid acoustic fence along the shared boundaries of the car park
- Delivery vehicles to enter the site between the hours of 7:00am and 7:00pm
- Maintaining the current location on the site for unloading deliveries (deliveries to be on-site)
- Ensuring delivery vehicles turn off any roof mounted refrigeration systems before entering the site
- Locating mechanical plant away from the residential boundaries to ensure compliance with the EPA Noise guidelines (not part of the approval).

Whilst Council would not be able to condition the turning off of roof mounted refrigeration associated with delivery vehicles, Council is of the opinion that the current arrangements for on-site delivery, the frequency and the restricted hours of the deliveries will sufficiently minimise adverse impacts on neighbouring properties.

The height of the 2.4m high acoustic fence is also considered to be appropriate it will provide noise mitigation towards the adjacent properties whilst being of a height that will not create any significant visual or overshadowing impacts. In any case, the fence will be 300mm higher than standard 2.1m high fence which does not require approval.

### Car Parking and Deliveries

Table 1 - General Off-Street Car Parking Requirements states that Supported Accommodation has a recommended rate of 0.3 car parking spaces per bed.

The existing aged care facility has a total of 7 on-site car parks (6 visitor parks and 1 accessible park) which services the existing 45 beds. Given the additional 10 rooms, the aged care facility has a theoretical car parking demand of 17 spaces for 55 beds. The application proposes an additional 11 car parking spaces (9 visitor parks and 2 staff parks), therefore increasing the total to 18 car parks including 1 disabled park which meets the above parking requirement.

### Front Fencing

The relevant Historic Area Statement (Residential Compact Parkside (South) Historic Area Statement (Un27)) within the Historic Area Overlay states that fencing on larger sites (16 metre frontages) may include *'vertical elements up to 1.8 metres in total height...open, see through and maintaining an open streetscape presence of the associated building'*.

The proposed front boundary fence along 17 Foster Street comprises of a steel fence on a 500mm high brick plinth wall which was chosen to match the style of the existing fence along 19-21 Foster Street. The proposed fence will provide openness and visibility towards the retained character frontage of 17 Foster Street as viewed from the street. The proposed fencing style is considered to be consistent with the relevant Historic Area Statement and therefore is acceptable.

## **CONCLUSION**

Having considered all the relevant assessment provisions, the proposal is considered to be not seriously at variance with the Planning and Design Code and is considered to satisfy the provisions of the Development Plan for the following reasons:

- The proposed addition will be consistent with PO 1.1 and 1.3 of the Established Neighbourhood Zone as it will be sited and designed to complement the surrounding residential character and will be compatible with the existing development pattern within the locality;
- The proposed car park will meet the demand outlined within Table 1 – Off Street Car Parking Requirement for supported accommodation which will help alleviate the existing levels of on-street car parking;
- Noise impacts will be appropriately managed so as to be consistent with PO 2.1 of the Interface between Land Uses section; and
- The proposed open style front fencing will be consistent with the relevant Historic Area Statement (Residential Compact Parkside (South) Historic Area Statement (Un27)) as it will provide visibility towards the retained character frontage of the addition.

## RECOMMENDATION

It is recommended that the Council Assessment Panel resolve that:

1. Pursuant to Section 107(2)(c) of the Planning, Development and Infrastructure Act 2016, and having undertaken an assessment of the application against the Planning and Design Code, the application is NOT seriously at variance with the provisions of the Planning and Design Code; and
2. Development Application Number 22015437, by St Louis Aged Care C-/ Future Urban is granted Planning Consent subject to the following conditions:

## CONDITIONS

### Planning Consent

#### Condition 1

The approved development shall be undertaken and completed in accordance with the stamped plans and documentation, except where varied by conditions below (if any).

#### Condition 2

That all landscaping shall be planted in accordance with the approved plans (as prepared by ON Architecture, Rev A dated 09/02/2022). The approved landscaping shall be established prior to the occupation of the development and shall be irrigated, maintained and nurtured with any dead, diseased or dying plants being replaced within the next available growing season and to the reasonable satisfaction of the Council.

#### Condition 3

All stormwater from the building and site shall be disposed of so as to not adversely affect any properties adjoining the site or the stability of any building on the site. Stormwater shall not be disposed of over a crossing place. The stormwater shall be managed in accordance with the approved Stormwater Management Plan (as prepared by Jack Adcock Consulting Pty Ltd dated 04/05/2022 and quoted with job reference number JAC220043).

#### Condition 4

That the acoustic fencing associated with the car park be erected prior to commissioning of the car park. The fence should be constructed in accordance with the approved acoustic report (as prepared by Echo Acoustic Consulting dated 27/04/2022 and quoted with job reference number 20-3).

#### Condition 5

Waste collection and general delivery vehicles shall only service the development between the hours of 7am and 7pm on any day. General delivery vehicles (not including waste collection vehicles) shall utilise the on-site loading area.

## ADVISORY NOTES

### General Notes

1. No work can commence on this development unless a Development Approval has been obtained. If one or more consents have been granted on this Decision Notification Form, you must not start any site works or building work or change of use of the land until you have received notification that Development Approval has been granted.
2. Appeal rights – General rights of review and appeal exist in relation to any assessment, request, direction or act of a relevant authority in relation to the determination of this application, including conditions.
3. This consent or approval will lapse at the expiration of 2 years from its operative date, subject to the below or subject to an extension having been granted by the relevant authority.

4. Where an approved development has been substantially commenced within 2 years from the operative date of approval, the approval will then lapse 3 years from the operative date of the approval (unless the development has been substantially or fully completed within those 3 years, in which case the approval will not lapse).
5. A decision of the Commission in respect of a development classified as restricted development in respect of which representations have been made under section 110 of the Act does not operate—
  - a. until the time within which any person who made any such representation may appeal against a decision to grant the development authorisation has expired; or
  - b. if an appeal is commenced—
    - i. until the appeal is dismissed, struck out or withdrawn; or
    - ii. until the questions raised by the appeal have been finally determined (other than any question as to costs).

## Planning Consent

### Advisory Note 1

It is recommended that as the applicant is undertaking work on or near the boundary, the applicant should ensure that the boundaries are clearly defined, by a Licensed Surveyor, prior to the commencement of any building work.

### Advisory Note 2

The applicant is reminded of the requirements of the Fences Act 1975. Should the proposed works require the removal, alteration or repair of an existing boundary fence or the erection of a new boundary fence, a 'Notice of Intention' must be served to adjoining owners. Please contact the Legal Services Commission for further advice on 1300 366 424 or refer to their web site at [www.lsc.sa.gov.au](http://www.lsc.sa.gov.au).

### Advisory Note 3

That any damage to the road reserve, including road, footpaths, public infrastructure, kerb and guttering, street trees and the like shall be repaired by Council at full cost to the **applicant**.

### Advisory Note 4

Driveway crossovers shall be designed to reflect the following:

- Driveways crossovers are not to be constructed from concrete over the footpath area between the kerb to boundary.
- Driveways and boundary levels at fence line must be between 2% and 2.5% above kerb height
- Crossover not to exceed 2.5% or 1:40 cross fall gradient from boundary to kerb invert .
- If a driveway crossover or portion of a driveway crossover is no longer required due to the relocation of a new crossover or alteration to an existing crossover.
- The redundant driveway crossover or part of, is required to be closed and returned back to kerb and gutter, also raising the footpath level to match the existing paved footpath levels at either side of the crossover being closed.

## OFFICER MAKING RECOMMENDATION

**Name:** Mark Troncone

**Title:** Planning Officer

**Date:** 06/05/2022

## **ATTACHMENT 1**



# ST. LOUIS AGED CARE FOSTER STREET PARKSIDE • NEW 10 BEDROOM ADDITION

PLANNING ISSUE		
Rev	Amendment	Date
-	-	-



VIEW LOOKING FROM NORTH-WESTERN SIDE

COVER SHEET

DRAWING NO.	DRAWING TITLE
DA01	COVER
DA02	LOCATION PLAN,
DA03	PROPOSED CAR PARK PLAN
DA04	OVERALL SITE PLAN
DA05	SITE DEMOLITION PLAN
DA06	SITE PLAN/FLOOR PLAN
DA07	ROOF PLAN
DA08	ELEVATIONS
DA09	ELEVATIONS
DA10	SECTION
DA11	RENDERS

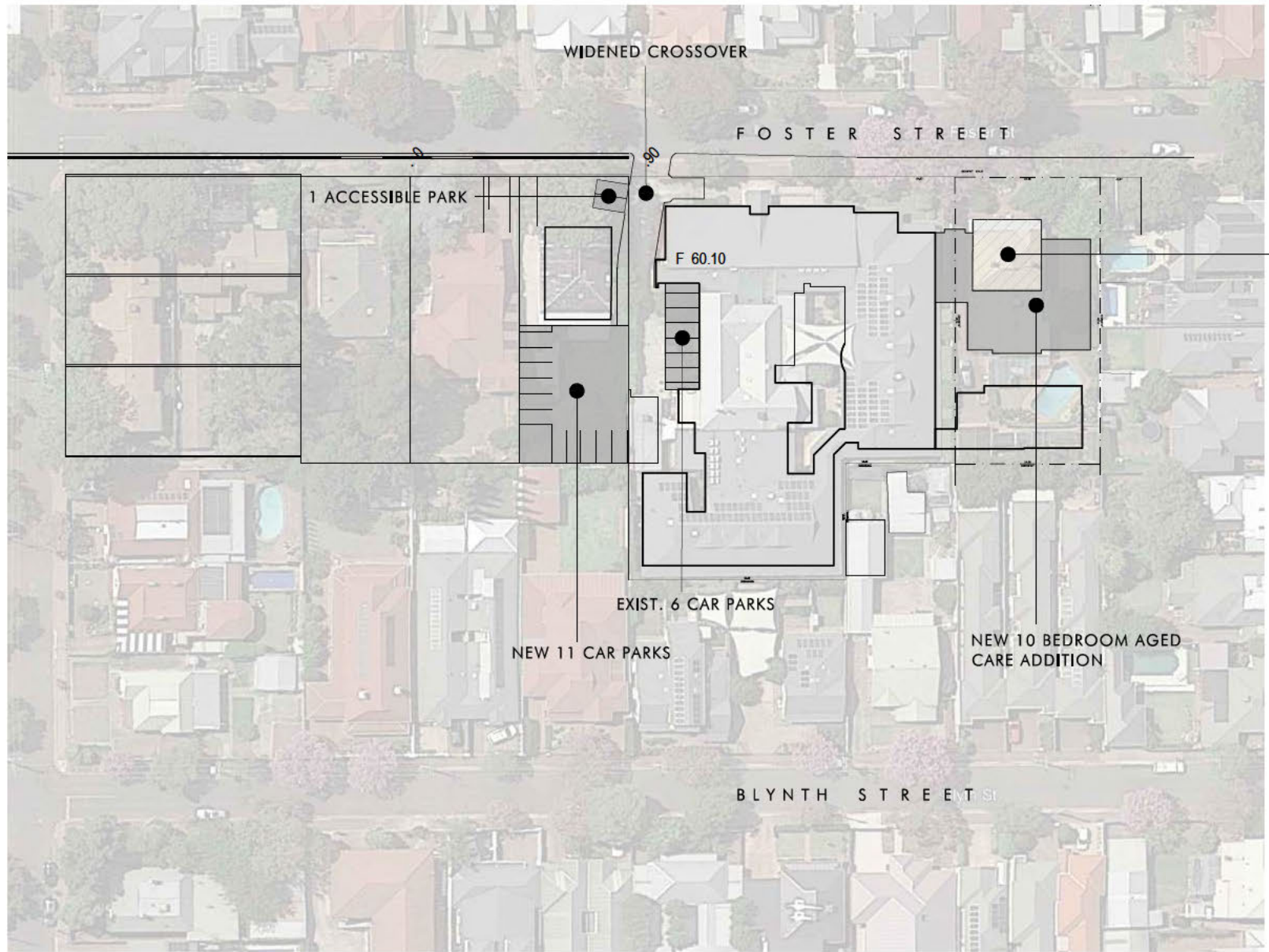


Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
CO ER

Scale	-	Drawn	JT	
Client	-			
Date	09/02/2022			
Job No.	202100069			
Dwg No.	<b>DA01</b>	Rev.	A	A3 SHEET





## LOCATION PLAN

0 25 50M

SCALE 1:1000 @ A3

## PLANNING ISSUE

Rev	Amendment	Date
-	-	-



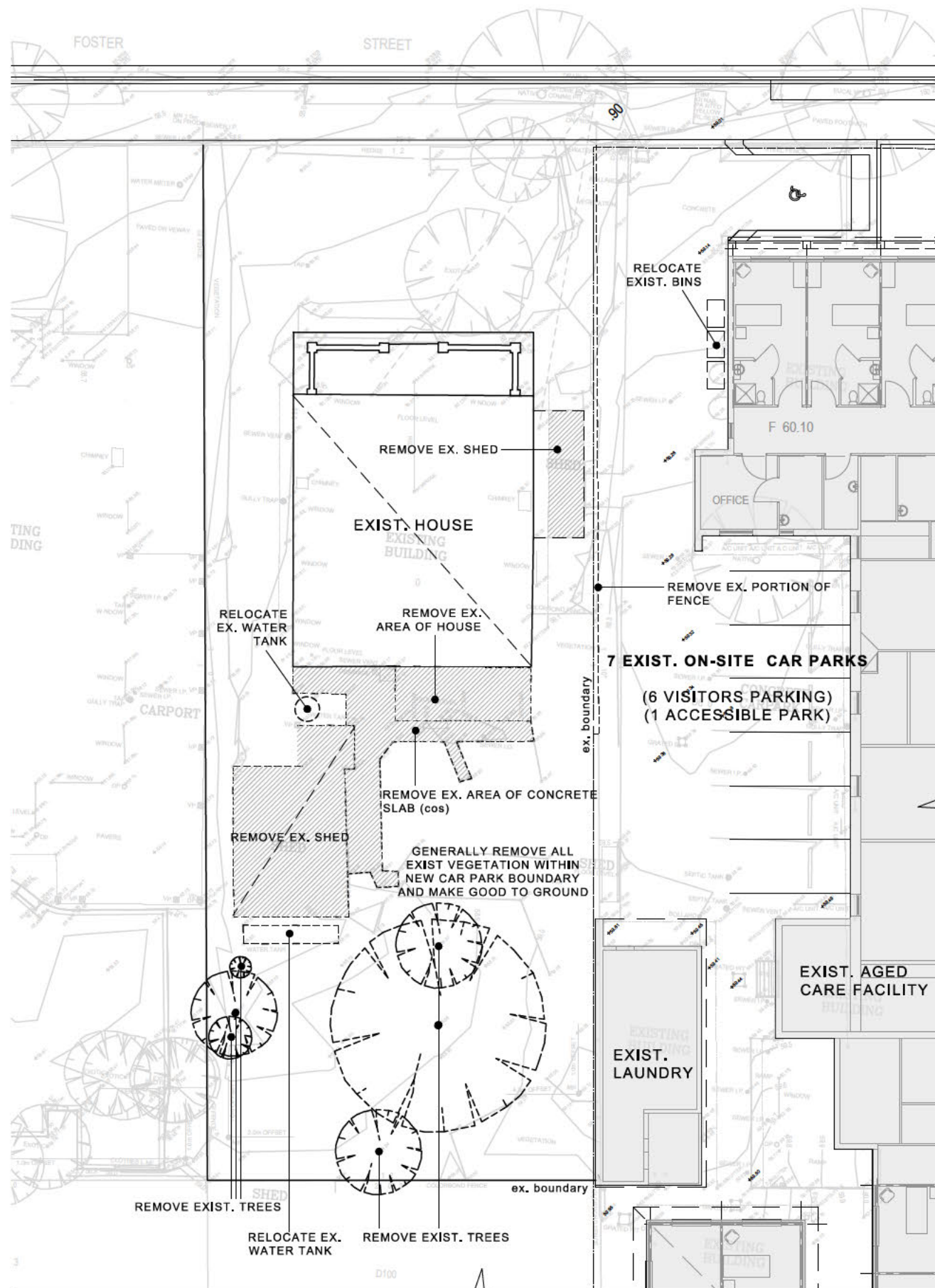
Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
LOCATION PLAN  
PROPOSED CAR PARK PLAN

Scale	AS SHOWN	Drawn	JT
Client	-		
Date	09/02/2022		
Job No.	202100069		
Dwg No.	<b>DA02</b>	Rev.	A

A3 SHEET





## CAR PARK DEMOLITION PLAN

0 5 10M

SCALE 1:250 @ A3

### LEGEND

--- ITEMS TO BE DEMOLISHED



## PROPOSED CAR PARK PLAN

0 5 10M

SCALE 1:250 @ A3

## PLANNING ISSUE

Rev	Amendment	Date

### LEGEND

SYNTHETIC LAWN, PAUL MUNNS SUMMER PRESTIGE 20MM ON 100MM RUBBLE BASE TO MANUFACTURERS DETAILS

NEW LANDSCAPING AREA( ALLOW 150mm TOP SOIL )

NEW 1.8m COLORBOND FENCE

NEW 2.4m COLORBOND FENCE

### NOTES:

PROVIDE NEW AUTOMATIC IRRIGATION SYSTEM (OR EXTEND EXISTING) TO ALL NEW PLANTING AREAS

REFER CIVIL DRAWINGS FOR NEW STORMWATER REQUIREMENTS

REFER TO DA06 FOR LANDSCAPE PLANTING DATA

PROVIDE TRELLIS OR CLIMBING WIRE TO FENCE FOR STAR JASMINE CLIMBER



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
CAR PARK DEMO ITION P AN  
PROPOSED CAR PARK P AN

Scale AS SHO N Drawn JT

Client

Date 09/02/2022

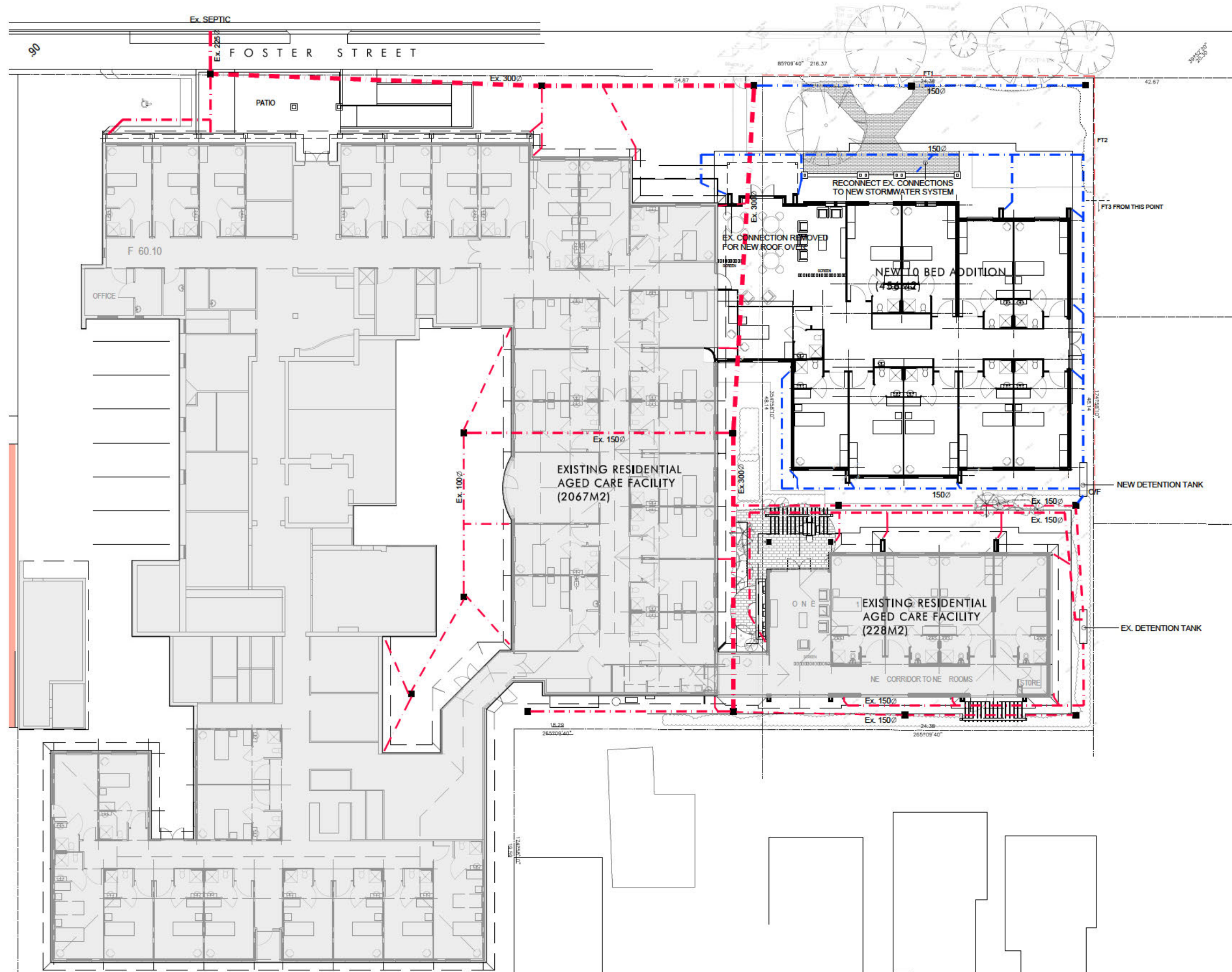
Job No. 202100069

Dwg No. DA03

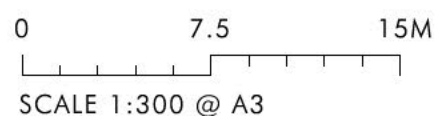
Rev. A

A3 SHEET





OVERALL SITE PLAN



## PLANNING ISSUE

Rev	Amendment	Date
-	-	-

### LEGEND

- EXISTING LEVELS
- E ISTIN STORM ATER
- CONNECT NE STORM ATER INTO E ISTIN S STEM
- RATED STORM ATER PIT
- FT1 STEE FENCE TO MATCH E ISTIN NORTHERN FENCE, ON 00mmH RICK A
- FT2 1 00H CO OR OND FENCE TO MATCH E ISTIN
- FT3 1 00H CO OR OND FENCE TO MATCH E ISTIN ON RETAININ A TO EN RS DETAI



Project  
**ST LOUIS AGED CARE PARKSIDE**  
 21 FOSTER STREET, PARKSIDE

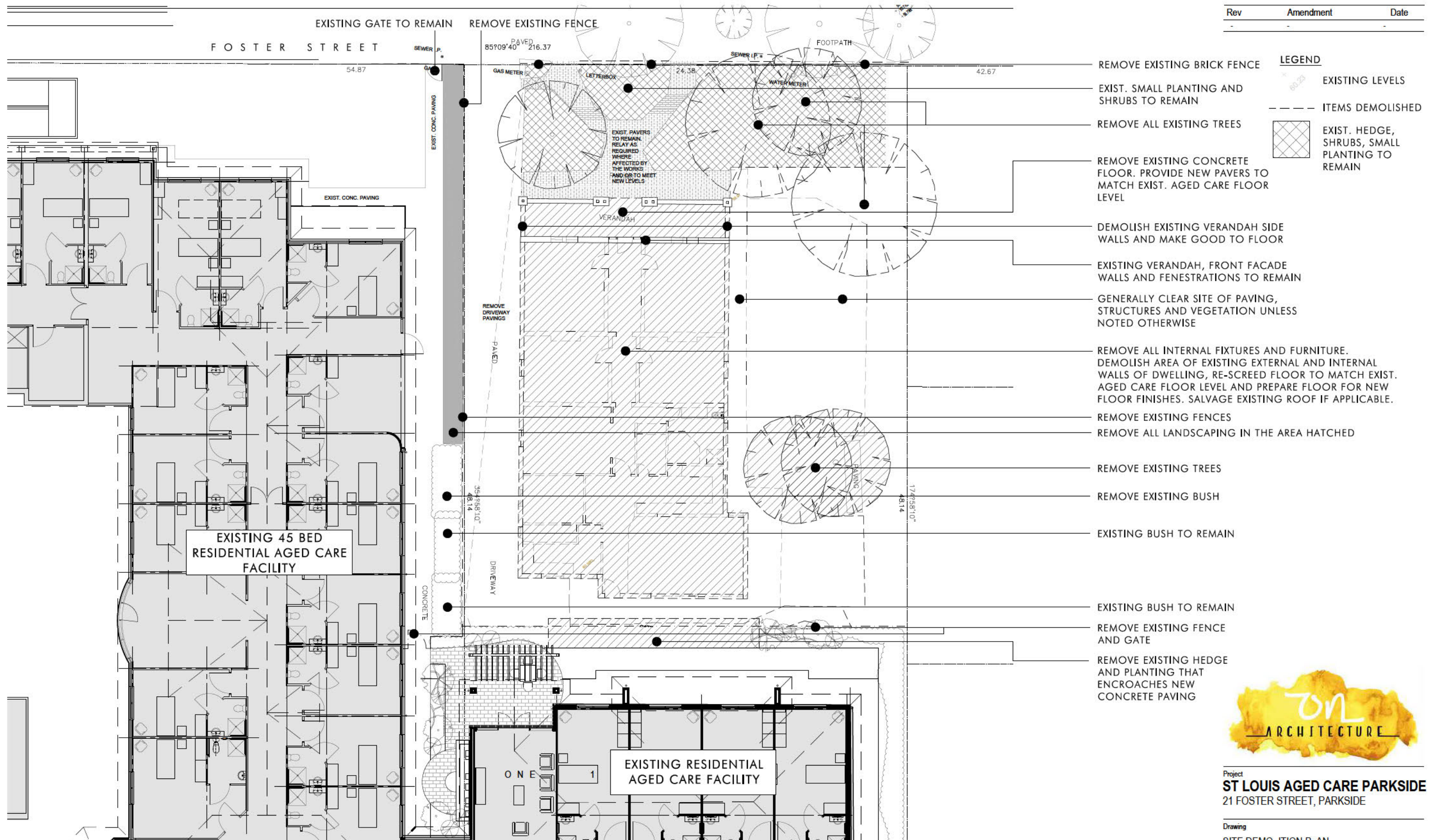
Drawing  
 O ERA SITE PLAN

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 Client -  
 Date 09/02/2022  
 Job No. 202100069  
 Dwg No. **DA03** Rev. A A3 SHEET



# PLANNING ISSUE

Rev	Amendment	Date
-	-	-



## SITE DEMOLITION PLAN

0 5 10M

SCALE 1:200 @ A3



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
SITE DEMOLITION PLAN

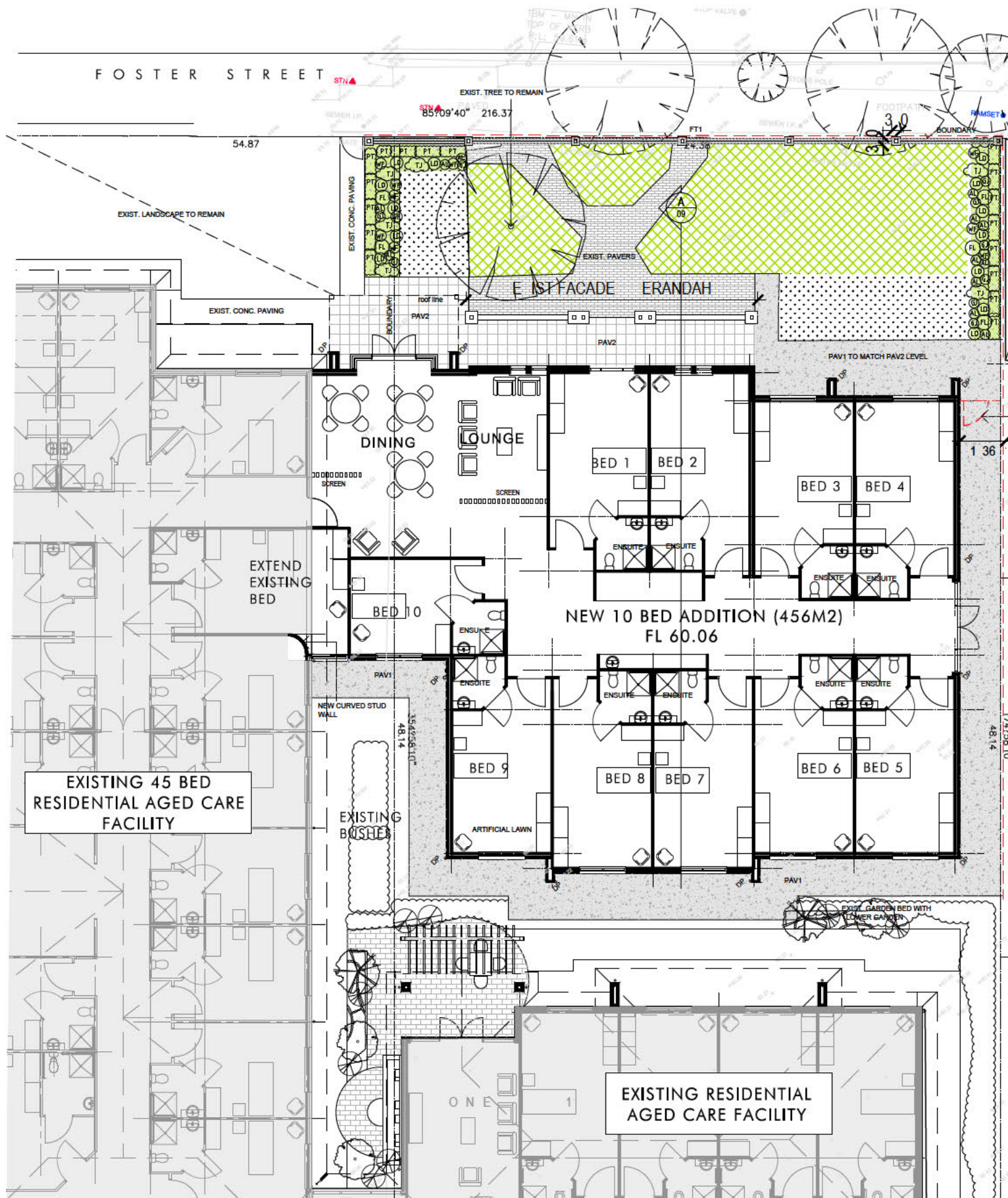
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Date	09/02/2022		
Job No.	202100069		
Dwg No.	<b>DA04</b>	Rev.	A

A3 SHEET



PLANNING ISSUE

Rev	Amendment	Date
-	-	-



- LEGEND**
- EXISTING LEVELS
  - RAINWATER DOWNPIPES
  - FT1 STEEL FENCE TO MATCH NEIGHBOURING NORTHERN FENCE, ON 500mm BRICK WALL
  - FT2 COLORBOND FENCE TO MATCH EXISTING
  - FT3 COLORBOND FENCE TO MATCH EXISTING ON RETAINING WALL TO ENGR'S DETAIL
  - EXIST. HEDGE, SHRUBS, SMALL PLANTING
  - NEW LANDSCAPING AREA (ALLOW 150mm TOP SOIL)
  - PA 1 NEW REINFORCED CONCRETE PAVING TO MATCH EXISTING
  - EXISTING CONCRETE PAVING
  - SYNTHETIC LAWN, PAUL MUNNS SUMMER PRESTIGE 20MM ON 100MM RUBBLE BASE TO MANUFACTURERS DETAILS
  - PA 2 BEST PAVERS, BLACK SERIES FLINDERS CHARCOAL 400X400X40 TERRAZZO PAVERS
  - EXISTING CLAY PAVERS, RELAY AS REQUIRED WHERE AFFECTED BY THE WORKS AND/OR TO MEET NEW LEVELS

**NOTES:**

- PROVIDE NEW AUTOMATIC IRRIGATION SYSTEM (OR EXTEND EXISTING) TO ALL NEW PLANTING AREAS
- REFER CIVIL DRAWINGS FOR NEW STORMWATER REQUIREMENTS
- PROVIDE TRELLIS OR CLIMBING WIRE TO FENCE FOR STAR JASMINE CLIMBER

P ANT SCHED E

KEY	BOTANICAL NAME	COMMON NAME	POT SIZE	SPACING	FULL SIZE
HEDGES AND CLIMBERS					
PT	PITTOSPORUM TENUIFOLIUM	PITTOSPORUM HEDGE	140mm	1/m2	1500mm X 2000mm
TJ	TRACHELOSPERMUM JASMINOIDES	STAR JASMINE (CLIMBER)	140mm	1M APART	2000mm X 500mm
BUSH/GROUND COVER					
LD	LANDANDULA DENTATA	FRENCH LAVENDER	140mm	4/m2	500mm X 600mm
AL	LIRIOPE MUSCARI AMETHYST	AMETHYST LIRIOPE	140mm	6/m2	400mm X 400mm
WF	WESTRINGIA FRUITCOSA	NATIVE ROSEMARY	140mm	4/m2	450mm X 450mm
FL	DIANELLA CAERULEA	FLAX LILLY	140mm	4/m2	600mm X 600mm
TJ	TRACHELOSPERMUM JASMINOIDES	STAR JASMINE (GROUND COVER)	140mm	2/m2	2000mm X 500mm
GJ	GARDENIA JASMINOIDES STAR	GARDENIA STAR	140mm		500mm X 600mm

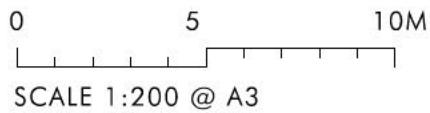
ITEM	DESCRIPTION	FINISH / APPLICATION
Imported Topsoil	Jeffries 'Special Soil'	150mm Depth mixed 50/50 with existing site topsoil.
Herbicide	Non-residual glysophate herbicide	Apply to subgrade as per manufacturers specification
Soil conditioner	Terracottem	20g per plant (at time of planting)
Fertiliser	Osmocote 'Total'	Mix with topsoil around rootball of plants at time of planting 5g per 1 litre of potting mix.
Mulch	Jeffries Forest Mulch	50mm Deep. Finish 25mm below adjacent pavers.



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
SITE P AN/F OOR P AN

SITE LANDSCAPE PLAN/FLOOR PLAN

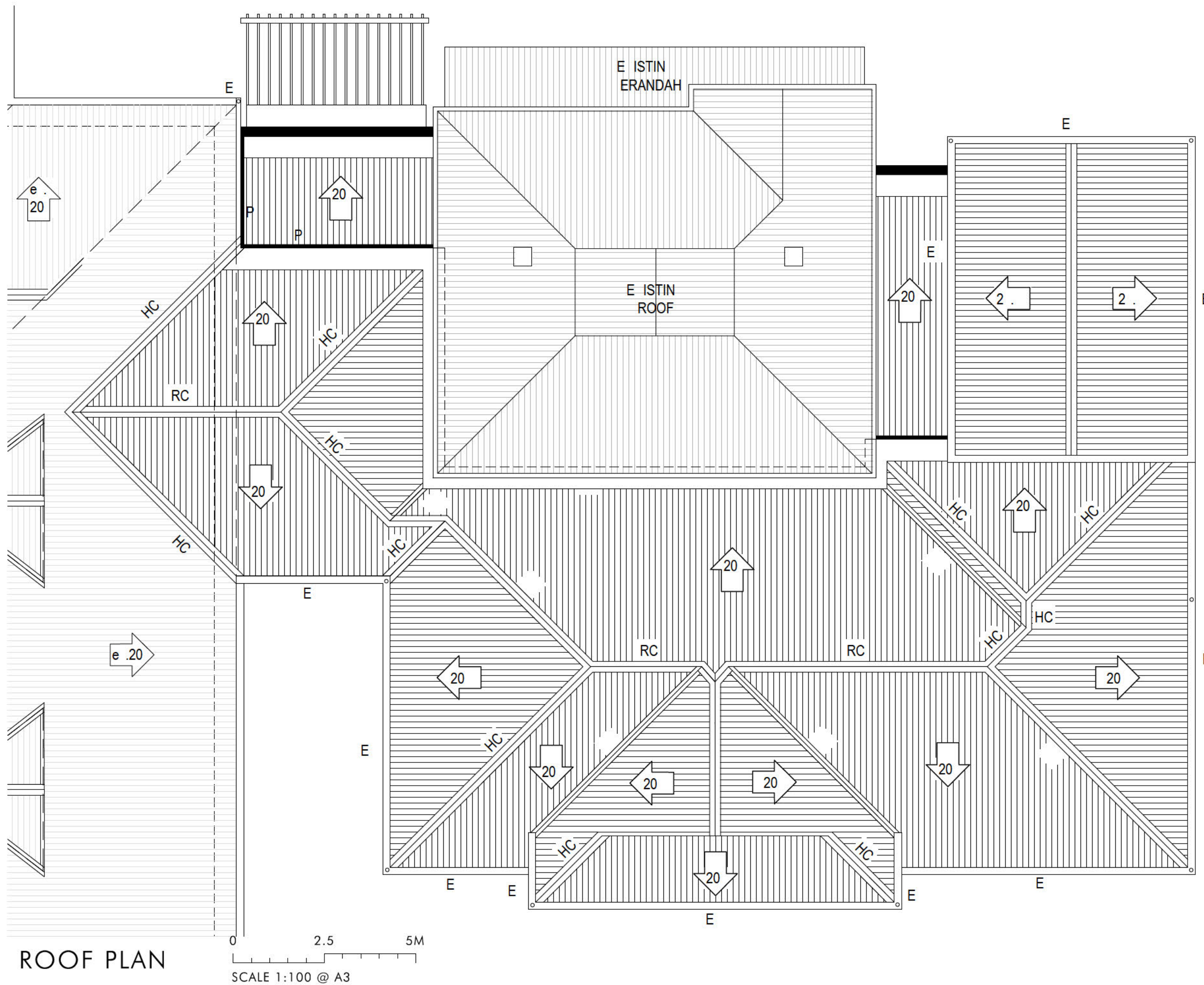


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Date	09/02/2022		
Job No.	202100069		
Dwg No.	DA05	Rev.	A
			A3 SHEET





## PLANNING ISSUE

Rev - Amendment - Date -

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Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
ROOF P AN

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Client -  
Date 09/02/2022  
Job No. 202100069  
Dwg No. **DA06** Rev. A A3 SHEET

# PLANNING ISSUE

Rev	Amendment	Date
-	-	-



## NORTH ELEVATION

1:100



## SOUTH ELEVATION

1:100

## ELEVATIONS

0 2.5 5M

SCALE 1:100 @ A3

NOTE:  
LANDSCAPING OMITTED  
FOR CLARITY



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
ELEVATIONS

Scale	1:100	Drawn	JT
Client	-		
Date	09/02/2022		
Job No.	202100069		
Dwg No.	<b>DA07</b>	Rev.	A
			A3 SHEET



Rev	Amendment	Date
-	-	-

SPLIT FACE SANDSTONE CLADDING TO  
MATCH FRONT OF EXISTING RESIDENCE

COLORBOND ROOF CLADDING @  
20° PITCH TO MATCH EXISTING

TOP ROOF LEVEL 6200

CEILING LEVEL 2700  
EAVES LEVEL 2400

GROUND FLOOR 0 (RL 60.06)



## EAST ELEVATION

1:100

ALUMINIUM DOORS AND  
WINDOWS TO MATCH EXISTING

RED BRICK TO MATCH EXISTING

EXISTING RESIDENCE BEYOND

EXISTING RESIDENCE BEYOND

EXISTING HERITAGE FRONT

TOP ROOF LEVEL 6200

CEILING LEVEL 2700  
EAVES LEVEL 2400

GROUND FLOOR 0 (RL 60.06)



## WEST ELEVATION

1:100

NEW VERANDAH TO MATCH EXISTING

EXISTING AGED CARE FACILITY

## ELEVATIONS

0 2.5 5M

SCALE 1:100 @ A3



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

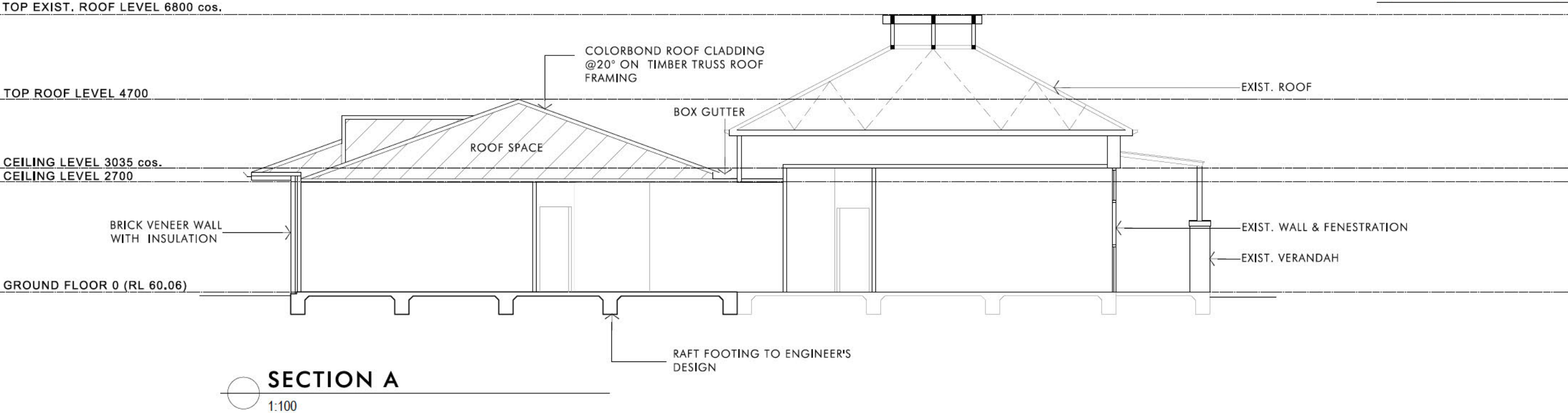
Drawing  
E E ATIONS

Scale	1:100	Drawn	JT
Client	-		
Date	09/02/2022		
Job No.	202100069		
Dwg No.	DA08	Rev.	A

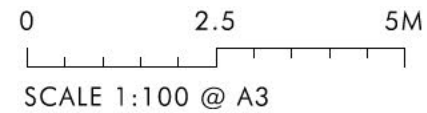


A3 SHEET

NOTE:  
LANDSCAPING OMITTED  
FOR C. ART



SECTION



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
SECTION

Scale	1:100	Drawn	JT	
Client	-			
Date	09/02/2022			
Job No.	202100069			
Dwg No.	DA09	Rev.	A	A3 SHEET

NOTE:  
LANDSCAPING OMITTED  
FOR C. ARIT




Rev	Amendment	Date
-	-	-



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
RENDERS

Scale	-	Drawn	JT	
Client	-			
Date	09/02/2022			
Job No.	202100069			
Dwg No.	<b>DA10</b>	Rev.	A	A3 SHEET





## **PLANNING REPORT**

### **ADDITION TO ST LOUIS AGED CARE**

17-23 FOSTER STREET, PARKSIDE

Prepared for:  
**St Louis Aged Care**

Date:  
**06.05.2022**

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## **APPENDICES**

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- APPENDIX 2. ARCHITECTURAL PLANS*
- APPENDIX 3. STORMWATER*
- APPENDIX 4. TRAFFIC AND PARKING*
- APPENDIX 5. ACOUSTIC*

## 1. INTRODUCTION

This report has been prepared to accompany a development application by St Louis Aged Care ('the Proponent') to construct an addition to the existing Parkside St Louis Aged Care facility at 17-23 Foster Street, Parkside ('the site').

In preparing this report, we have:

- inspected the site and its immediate surroundings;
- identified and subsequently reviewed what we consider to be the most pertinent provisions of the Planning and Design Code ('the Code');
- had regard to the *Planning, Development and Infrastructure Act 2016* ('the Act') and to the *Planning, Development and Infrastructure (General) Regulations 2017* ('the Regulations');
- also had regard to the certificate of title in **Appendix 1**;
- examined the architectural drawings in **Appendix 2**;
- reviewed the supporting documents, including:
  - » stormwater management plan and civil drawings prepared by XXX in **Appendix 3**;
  - » traffic and parking assessment prepared by MFY in **Appendix 4**; and
  - » acoustic report prepared by Echo Acoustics in **Appendix 5**.

This report contains our description of the site, its surroundings and the proposal, and our assessment of the proposal against what we consider to be the most relevant provisions of the Code.

## 2. BACKGROUND

Situated in the suburb of Parkside, St Louis' Nursing Home has been caring for South Australians for more than 70 years. With a close-knit family of 45 residents, the nursing home is a comfortable and happy home for the St Louis community – residents, their families and our staff.

St Louis' aged care services include the boutique nursing home in Parkside, with independent living units next door and home care support services delivered to people in the comfort of their own homes.

St Louis is a third-generation family business started by Dorothy Naish, followed by her son Tony Naish, who continued expanding St Louis' aged care services pioneering the first high level dementia care service at home in Adelaide. Dr Rane Naish, Tony's wife supported St Louis through that expansion as a medical consultant and is still involved in the business.

Maris Naish who runs the business today is the third-generation administrator of St Louis. Throughout the generational history of the business, family members have been actively involved in St Louis spending time with residents getting to know them and developing an understanding of how to foster a culture of care with family values.

### **3. PROPOSED DEVELOPMENT**

#### **3.1 Built Form**

The Proponent seeks to obtain planning consent to undertake a 10 bed addition to the existing St Louis Aged Care facility and associated 2.4 metre high acoustic fencing.

The addition will utilise design cues from existing buildings in the locality and the historic area statement with gable roof forms, chimneys, symmetry and the chosen materials and colours.

#### **3.2 Traffic Management**

##### **3.2.1 Access**

The existing crossover via Foster Street is proposed to be widened to allow for simultaneous two-way vehicle movement.

The existing crossover at Allotment 401 will be reinstated.

##### **3.2.2 Parking**

An additional 11 onsite car parks are proposed as a result of the additional development, and will be located on Allotment 405. The existing disabled park is also proposed to be relocated to the front of Allotment 405.

The site will therefore accommodate a total of 18 car parks, including one for disabled access.

#### **3.3 Stormwater Management**

Stormwater will be connected into the existing system however, will be largely detained on site utilising rain gardens in accordance with the principles of water sensitive urban design.

Additional rain water will be captured in a 4000 litre rainwater tank for re-use on site.



## 4. SPATIAL ATTRIBUTES

### 4.1.1 Subject Site

The subject site comprises of three allotments, legally described as:

- Allotment 401 on Certificate of Title 5473/264;
- Allotment 405 on Certificate of Title 5780/30; and
- Allotment 67 on Certificate of Title 5916/667.

The site is otherwise known as 17-23 Foster Street, Parkside.

Together the allotments have a 97.56 metre frontage to Foster Street and a total site area of 5,413 square metres.

The allotments comprise the existing St Louis Aged Care facility and two detached dwellings.

There are no registered easements or encumbrances which could impede or avert the proposal altogether, and no regulated or significant trees are located on the subject site. None of the sites include any heritage listing on them.

### 4.1.2 Zoning

The subject site is situated within the Established Neighbourhood Zone as shown in Figure 3.1 below.

The land is also within the following Overlays and Technical and Numeric Variations (TNVs):

- Airport Building Heights (Regulated) – all structures over 45 metres;
- Historic Area (Un27);
- Prescribed Wells Area;
- Regulated and Significant Tree;
- Stormwater Management;
- Urban Tree Canopy;
- Water Resources;
- Maximum Building Height (metres) – 5.7 metres;
- Maximum Building Height (levels) – 1 level;
- Minimum Frontage;
- Minimum Site Area;
- Minimum Side Boundary Setback; and
- Site Coverage – 50 percent.

### 4.1.3 Locality

The locality, in our opinion, is defined by the allotments directly abutting the subject site. Residential dwellings are the predominant land use within the area.

The site in relation to its immediate surroundings and zoning of the area, is captured in Figure 3.1 below.

**Figure 4.1** *Locality and Zoning plan*



Locality & Zoning

**LEGEND**

— Subject land boundary    — Locality boundary    — Zone boundary

Data Source: Imagery - SA Property & Planning Atlas dated 7 Jan-22; Jan 2021. Policy - Planning & Design Code version 2022.6, published on 31 March 2022.



## 5. PROCEDURAL MATTERS

At the time of preparing this report, the relevant version of the Planning and Design Code was gazetted and subsequently consolidated on V2022.6 (31 March 2022). Due to amendments, the version of the Code used to prepare this report may not be the relevant version at the time of lodgement of the application. To the extent of any inconsistency, the version of the Code at the time of lodgement will be relevant for the processing and assessment of the application.

The subject site is within the Established Neighbourhood Zone ('the Zone').

### 5.1 Verification

For the purposes of regulation 31(1)(a), (b) and (c) of the Regulations, the following applies:

**Table 5.1** *Verification snapshot*

Verification matter	Comment
Nature of Development	Additions to existing Aged Care facility and associated acoustic fencing.
Elements	Residential Care
Category of Development	Performance Assessed
Relevant Authority	Council Assessment Panel at the City of Unley

#### 5.1.2 Nature of Development

Part 7 – *Land Use Definitions* of the Code includes residential care within the definition of 'supported accommodation':

***"Supported accommodation** means premises in which residential accommodation is provided to persons requiring/together with regular medical and/or personal care assistance, but does not include home care.*

*including: residential care."*

Residential care is defined as:

***"Residential care** has the same meaning as in the Commonwealth Aged Care Act 1997."*

Further, the *Commonwealth Aged Care Act 1997* define residential care as:

***"Residential care** is personal care or nursing care, or both personal care and nursing care, that:*

*(a) is provided to a person in a residential facility in which the person is also provided with accommodation that includes:*

- i. appropriate staffing to meet the nursing and personal care needs of the person; and*
- ii. meals and cleaning services; and*
- iii. furnishings, furniture and equipment for the provision of that care and accommodation; and*



*(b) meets any other requirements specified in the Subsidy Principles.*

*However, residential care does not include any of the following:*

- (a) care provided to a person in the person's private home;*
- (b) care provided in a hospital or in a psychiatric facility;*
- (c) care provided in a facility that primarily provides care to people who are not frail and aged;*
- (d) care that is specified in the Subsidy Principles not to be residential care."*

### **5.1.3 Relevant Authority**

Pursuant to Section 93(1)(a) of the Act, the Council Assessment Panel is the relevant authority for the assessment and determination of the application.

Notwithstanding the above, we note that pursuant to Regulation 22(a)(ii) of the *Planning, Development and Infrastructure (General) Regulations 2017*, the Council's Assessment Manager may act as the relevant authority where a development is exempt from the requirements in Section 107(3) of the Act to give notice of the application.

## **5.2 Referrals**

There are no referrals required for this land use.

## **5.3 Public Notification**

Pursuant to section 107(6) of the Act, the Planning and Design Code may exclude specified classes of development from the requirement to undergo public consultation. Accordingly, Table 5 of the Established Neighbourhood Zone does not list residential care, as such the proposed development application requires public notification.

## 6. ASSESSMENT AGAINST PLANNING AND DESIGN CODE

The applicable policies include Desired Outcomes (DOs) which “*automatically apply in relation to a performance assessed development*” and Performance Outcomes (POs). It is also worth noting that some POs have a standard outcome that is considered to satisfy the corresponding PO, referred to as Designated Performance Features (DPFs). The Rules of Interpretation within Part 1 of the Code state the following in relation to DPFs (underlining our emphasis):

*“A DPF provides a guide to a relevant authority as to what is generally considered to satisfy the corresponding performance outcome but does not need to necessarily be satisfied to meet the performance outcome and does not derogate from the discretion to determine that the outcome is met in another way, or from the need to assess development on its merits against all relevant policies.”*

As a result of the above, the assessment below focusses on the applicable DOs and POs and may only refer to the DPF in instances where it assists in the exercise of discretion.

### 6.1 Land Use

The performance outcomes (‘PO’) within the Zone advise the following:

**PO 1.1** *Predominantly residential development with complementary non-residential activities compatible with the established development pattern of the neighbourhood.*

It is first worth noting that this proposal is for the expansion of the existing residential care facility located on the site. The expansion, by its very nature, is to improve community accessibility to such a service. Residential care, is a residential land use as it is where elderly people live and therefore is considered to be a compatible land use in an area where other people reside.

It is therefore considered that residential care is a suitable use within the Zone.

### 6.2 Built Form and Design

The Zone PO 4.2 and 5.1 advises:

**PO 4.2** *Additions and alterations do not adversely impact on the streetscape character.*

**PO 5.1** *Buildings are set back from primary street boundaries consistent with the existing streetscape.*

As discussed in further detail below, it is considered that the proposed development has been designed to complement the character and amenity of the neighbourhood and street in particular, therefore satisfying PO 4.2 above.

Additionally, as the existing dwelling is incorporated into the overall design, the building setbacks from the street will remain entirely consistent with the existing streetscape as sought by PO 5.1.

#### 6.2.1 Historic Area Overlay

The Historic Area Statement for Residential Compact Parkside (South) (Un27) identifies important characteristics to the area which include architectural features, materials and colours. The proposed development has been designed to incorporate these features and adhere to the desired outcome (1) for the Historic Area Overlay. The proposal:

- as the roof is proposed to remain and the external additions are smaller, the proposal is consistent with the prevailing heights in the historic area (PO 2.2 of the Historic Area Overlay);

- the front boundary setback remains unchanged and the side (east) boundary setback follows that of similar development in the locality which are predominantly built within close proximity to the side boundaries (PO 2.4 of the Historic Area Overlay);
- materials and colours proposed are consistent with the historic area, satisfying PO 2.5;
- reuses the existing dwelling façade, as sought by PO 3.2 of the Historic Area Overlay;
- the additions complement the existing historic design, taking cues while not dominating on the façade being retained in the process (PO 3.1 of the Historic Area Overlay);
- landscaping is both retained and improved on to ensure the streetscape character is maintained, as intended by PO 6.2 of the Historic Area Overlay; and
- as stated by PO 7.2 of the Historic Area Overlay, the portion proposed to be demolished does not contribute to the historic character of the streetscape, as the façade is retained.

### 6.3 Interface between Land Uses

Whilst the proposal is a residential land use, consideration has been had to ensuring the minimisation of any adverse impacts on adjoining land uses as set out in PO 2.1 within the Interface between Land Uses general module advises that:

*PO 2.1 Non-residential development does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having regard to:*

- (a) the nature of development*
- (b) measures to mitigate off site impacts*
- (c) the extent to which the development is desired in the zone*
- (d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land*

The proposed land use, residential in nature and already existing on site is considered to be appropriate within the Zone as discussed earlier.

The Proponent has engaged an acoustic engineer to ensure potential adverse noise impacts are minimised. The engineer concluded that many of the noise sources remain unchanged by this proposal. In relation to the new components of the development, the following is implemented to ensure the minimisation of noise levels towards adjoining sensitive receivers:

- construction of a 2.4 metres solid acoustic fence around the boundary of the new carparking area on Allotment 405;
- restricting deliveries to the site to between the hours of 7:00am and 10:00pm and maintaining the current location for deliveries on site;
- ensuring delivery trucks turn off any roof mounted refrigeration systems before entering the site; and
- locating the mechanical plant away from the residential boundary and ensure compliance with the EPA noise guidelines.

There will be no other adverse impacts from overshadowing, air quality, light spill, solar reflectivity, or electrical interference as sought by the Interface between Land Uses module in the Code.



## 6.4 Traffic Management

### 6.4.1 Access

MFY Traffic Consultants have undertaken an assessment to confirm that the proposed addition does not result in unreasonable traffic implications, and assist in improving the current access arrangements.

The proposal has therefore been designed to satisfy the policies within the Transport, Access and Parking module in the following ways:

- The crossover on Foster Street has been widened to allow for two-way simultaneous access and so vehicles can enter and exit the site, safely, in a forward direction as sought by PO 1.4 and PO 3.3.
- A triangle area adjacent the crossover will be provided and kept clear of obstructions, in order to provide appropriate sightlines to pedestrians in accordance with PO 2.2.
- It has been designed to allow adequate movement and manoeuvrability of commercial vehicles who will also be accessing the site (PO 3.8).
- No existing street trees will be removed or impacted by the proposal.
- The existing crossover at Allotment 401 is no longer utilised and will be reinstated to Council's requirements.

### 6.4.2 Parking

The Code designates a parking rate for supported accommodation of 0.3 spaces per bed. Based on this rate, the proposed 10-bedroom addition has a theoretical car parking demand of 3 additional spaces. The proposal includes 11 new car parking spaces, and therefore satisfying the Code.

The parking spaces have been designed to comply with the requirements of relevant Australian/New Zealand Standard.

## 6.5 Stormwater Management

Stormwater will be collected and either retained in three raingardens on-site for irrigation or collected in a 4000 litre rainwater tank with 2000 litre usable storage cell plumbed to toilet and 2000 litre acting as detention will overflow for disposal to existing stormwater disposal system and released at an appropriate rate in accordance with the Council's requirements.

The proposal is considered to satisfy the relevant policies within the Infrastructure and Renewable Energy Facilities (PO 11.1) and Design in Urban Areas (PO 42.2 and PO 42.3) modules of the Code which advise that:

**PO 42.2** *Water discharged from a development site is of a physical, chemical and biological condition equivalent to or better than its pre-developed state.*

**PO 42.3** *Development includes stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater from the site to ensure that development does not increase peak flows in downstream systems.*

The stormwater calculations and accompanying civil plans at **Appendix 3** confirm that the post-development flow rates will not exceed the pre-development flow rates. The proposal is not, therefore expected to overload the Council's existing stormwater drainage networks.

Stormwater from car parks will be treated through a waste filtration system (ECOSOL GPT 4200 Gross Pollutant Trap) prior to release to the Council's stormwater infrastructure network to remove suspended solids and hydrocarbons from the surface water collected, thereby improving the quality of stormwater and minimising pollutant transfer to receiving waters, as sought by PO 42.2 above.

## 6.6 Landscaping and Open Space

The Zone does not include any policies relating to landscaping, as such the Design in Urban Areas general module advises the following for supported accommodation and retirement facilities:

**PO 39.3** *Communal open space is of sufficient size and dimensions to cater for group recreation.*

**PO 39.5** *Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.*

Additional open spaces and landscaping is included in the proposal for the addition. A large open area at the front of the new addition is considered of a sufficient size to cater for group recreation and capable of containing chairs and the like to create a functional space for the residents. In addition, this area allows the residents to look out into Foster Street, creating a sense of community for them.

Other smaller areas are both proposed and existing within the grounds of the facility for communal meeting areas and including landscaping.

Additional landscaping is also provided around the perimeter of the car park area with a larger open space provided in the south west corner of the car parking area.

## 7. CONCLUSION

We have concluded from our assessment of the proposal that it is deserving of consent.

In support of our conclusion, we wish to highlight that:

- the land use existing already on a portion of the site, is appropriate within the Zone;
- the retention of the building façade, front verandah and roof assists in retaining the historic amenity of the streetscape;
- the additions have been designed to incorporate features outlines in the historic area statement;
- additional open space and landscaping is provided to increase the amenity of the residents;
- acoustic treatments are provided to minimise any adverse noise impacts towards adjoining sensitive receivers;
- stormwater runoff will be captured and disposed of in an environmentally sound manner;
- adequate car parking is achieved on site in excess of the minimum requirements in the Code and ameliorating an existing car parking shortfall; and
- all expected vehicles will be able to be driven into, and out of, the site in a safe and convenient manner.



## **APPENDIX 1. CERTIFICATE OF TITLE**

REAL PROPERTY ACT, 1886



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



## Certificate of Title - Volume 5473 Folio 264

Parent Title(s) CT 4216/944  
Creating Dealing(s) CONVERTED TITLE  
Title Issued 20/11/1997 Edition 4 Edition Issued 15/05/2019

## Estate Type

FEE SIMPLE

## Registered Proprietor

17 FOSTER PTY. LTD. (ACN: 632 561 098)  
OF 172 FULLARTON ROAD DULWICH SA 5065

## Description of Land

ALLOTMENT 401 DEPOSITED PLAN 1005  
IN THE AREA NAMED PARKSIDE  
HUNDRED OF ADELAIDE

## Easements

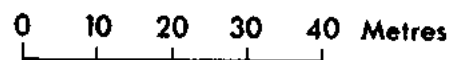
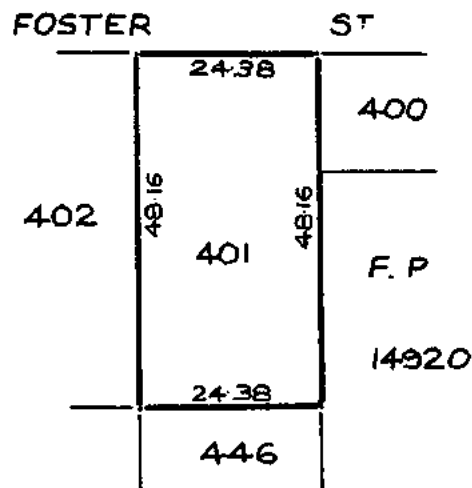
NIL

## Schedule of Dealings

NIL

## Notations

Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL





## Certificate of Title

**Title Reference:** CT 5473/264  
**Status:** CURRENT  
**Parent Title(s):** CT 4216/944  
**Dealing(s) Creating Title:** CONVERTED TITLE  
**Title Issued:** 20/11/1997  
**Edition:** 4

## Dealings

Lodgement Date	Completion Date	Dealing Number	Dealing Type	Dealing Status	Details
01/05/2019	15/05/2019	13104057	TRANSFER	REGISTERED	17 FOSTER PTY. LTD. (ACN: 632 561 098)
22/10/2009	30/10/2009	11278051	DISCHARGE OF MORTGAGE	REGISTERED	8810054
22/12/1999	19/01/2000	8810054	MORTGAGE	REGISTERED	AUSTRALIA & NEW ZEALAND BANKING GROUP LTD.

REAL PROPERTY ACT, 1886



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



## Certificate of Title - Volume 5780 Folio 30

**Parent Title(s)** CT 1102/113  
**Creating Dealing(s)** CONVERTED TITLE  
**Title Issued** 05/06/2000 **Edition** 2 **Edition Issued** 15/11/2018

## Estate Type

FEE SIMPLE

## Registered Proprietor

ST LOUIS NURSING HOME PTY. LTD. (ACN: 007 667 138)  
OF 21 FOSTER STREET PARKSIDE SA 5063

## Description of Land

ALLOTMENT 405 DEPOSITED PLAN 1005  
IN THE AREA NAMED PARKSIDE  
HUNDRED OF ADELAIDE

## Easements

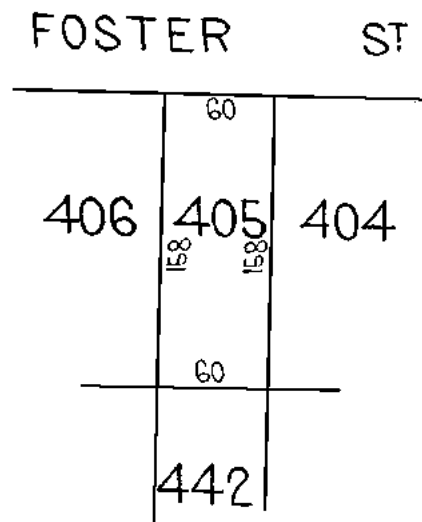
NIL

## Schedule of Dealings

NIL

## Notations

<b>Dealings Affecting Title</b>	NIL
<b>Priority Notices</b>	NIL
<b>Notations on Plan</b>	NIL
<b>Registrar-General's Notes</b>	NIL
<b>Administrative Interests</b>	NIL



100 50 0 100 FT

**DISTANCES ARE IN FEET AND INCHES  
FOR METRIC CONVERSION**  
**1 FOOT = 0.3048 METRES**  
**1 INCH = 0.0254 METRES**



## Certificate of Title

**Title Reference:** CT 5780/30  
**Status:** CURRENT  
**Parent Title(s):** CT 1102/113  
**Dealing(s) Creating Title:** CONVERTED TITLE  
**Title Issued:** 05/06/2000  
**Edition:** 2

## Dealings

Lodgement Date	Completion Date	Dealing Number	Dealing Type	Dealing Status	Details
01/11/2018	15/11/2018	13013934	TRANSFER	REGISTERED	ST LOUIS NURSING HOME PTY. LTD. (ACN: 007 667 138)

REAL PROPERTY ACT, 1886



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



## Certificate of Title - Volume 5916 Folio 667

Parent Title(s) CT 5241/999, CT 5242/292, CT 5616/608, CT 5633/669

Creating Dealing(s) RTC 9774570

Title Issued 20/05/2004 Edition 2 Edition Issued 05/08/2004

## Estate Type

FEE SIMPLE

## Registered Proprietor

ST. LOUIS NURSING HOME PTY. LTD. (ACN: 007 667 138)  
OF 17 GLEN OSMOND ROAD EASTWOOD SA 5063

## Description of Land

ALLOTMENT 67 DEPOSITED PLAN 64057  
IN THE AREA NAMED PARKSIDE  
HUNDRED OF ADELAIDE

## Easements

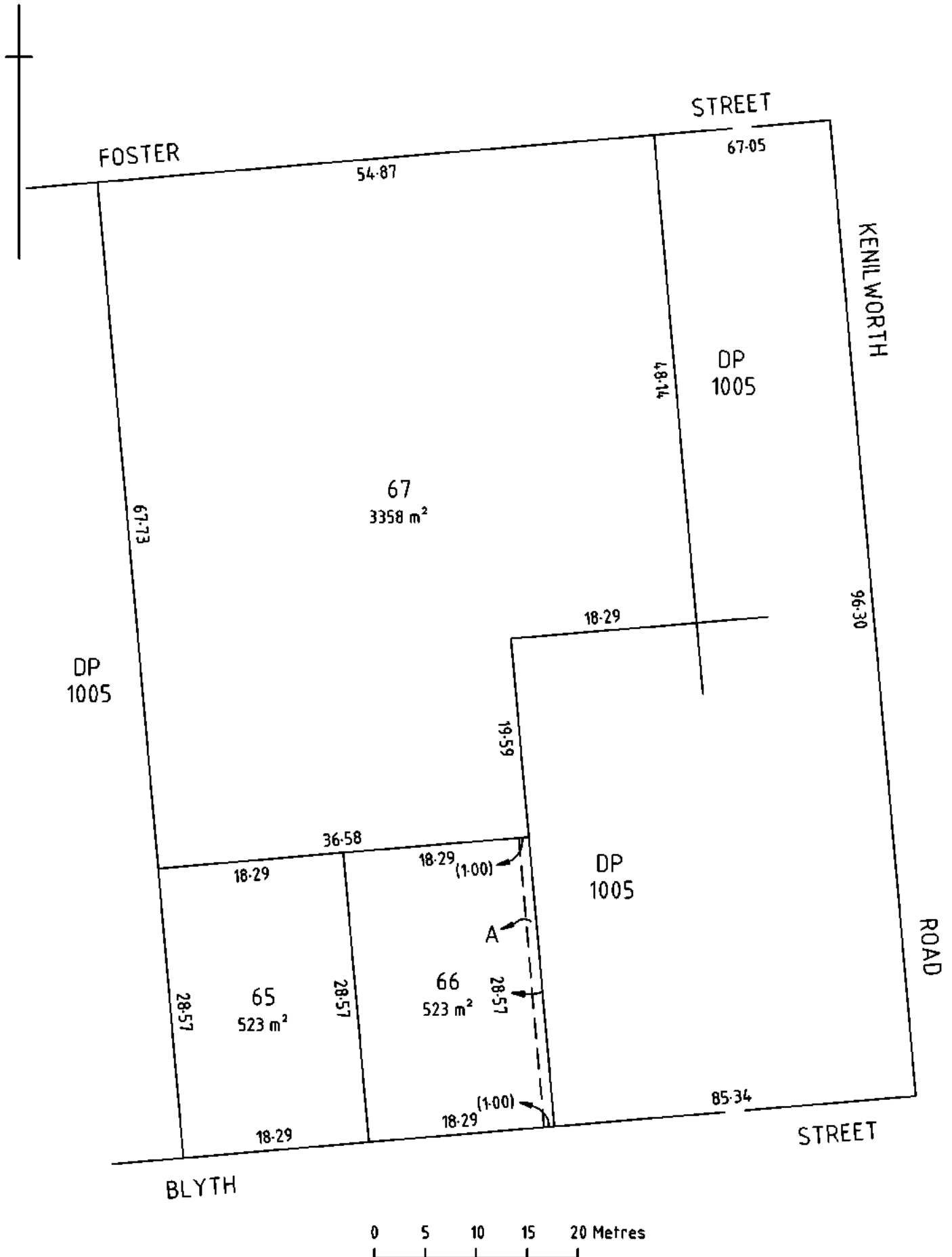
TOGETHER WITH EASEMENT(S) OVER THE LAND MARKED A FOR THE TRANSMISSION OF ELECTRICITY BY UNDERGROUND CABLE (RTC 9774570)

## Schedule of Dealings

Dealing Number	Description
10036534	MORTGAGE TO COMMONWEALTH BANK OF AUSTRALIA

## Notations

Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL





## **APPENDIX 2. ARCHITECTURAL PLANS**



# ST. LOUIS AGED CARE FOSTER STREET PARKSIDE • NEW 10 BEDROOM ADDITION

PLANNING ISSUE		
Rev	Amendment	Date
-	-	-



VIEW LOOKING FROM NORTH-WESTERN SIDE

COVER SHEET

DRAWING NO.	DRAWING TITLE
DA01	COVER
DA02	LOCATION PLAN,
DA03	PROPOSED CAR PARK PLAN
DA04	OVERALL SITE PLAN
DA05	SITE DEMOLITION PLAN
DA06	SITE PLAN/FLOOR PLAN
DA07	ROOF PLAN
DA08	ELEVATIONS
DA09	ELEVATIONS
DA10	SECTION
DA11	RENDERS

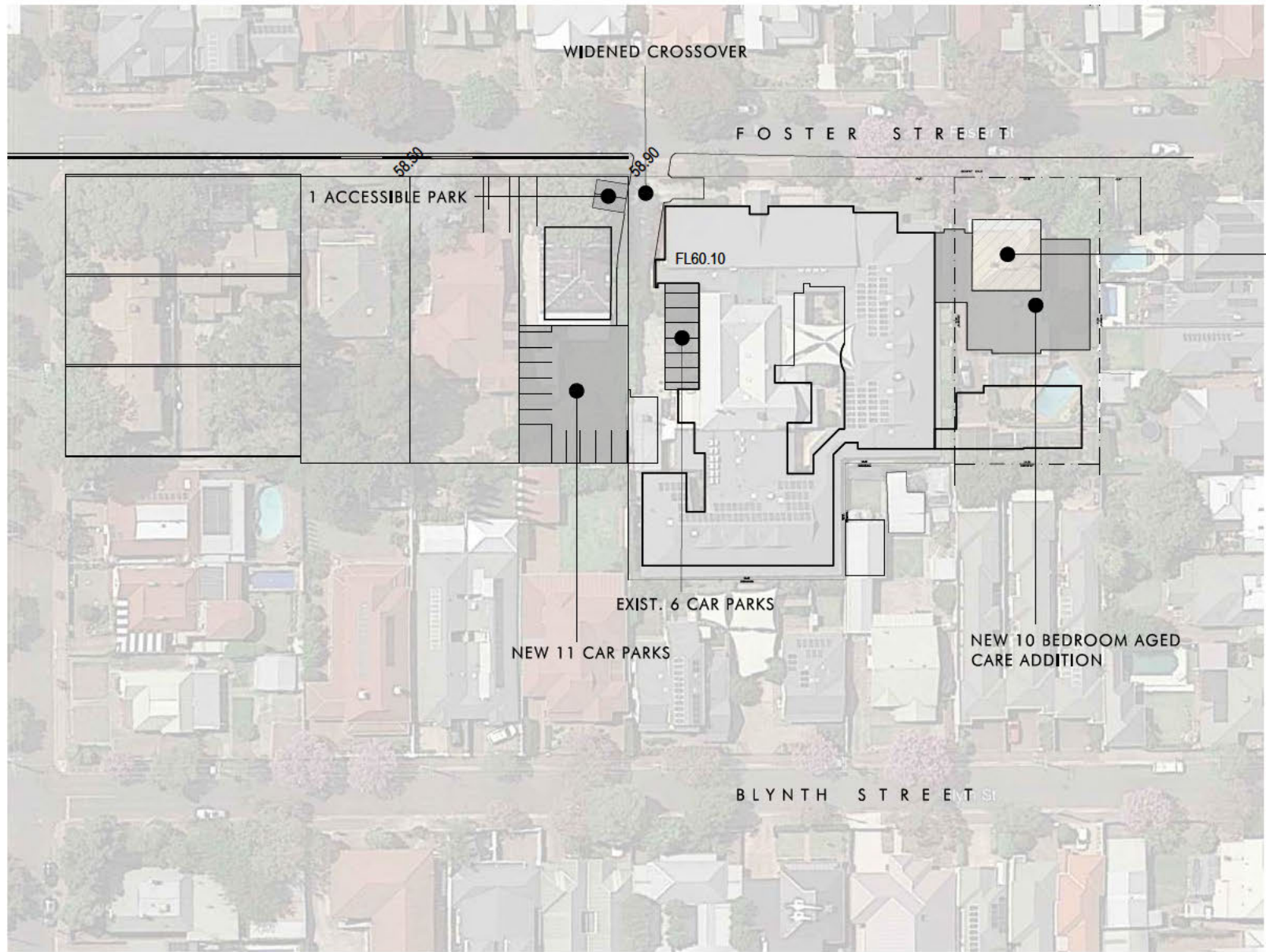


Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

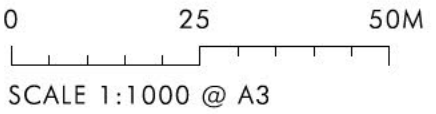
Drawing  
COVER

Scale	-	Drawn	JT	
Client	-			
Date	09/02/2022			
Job No.	202100069			
Dwg No.	<b>DA01</b>	Rev.	A	A3 SHEET





# LOCATION PLAN



## PLANNING ISSUE

Rev	Amendment	Date
-	-	-



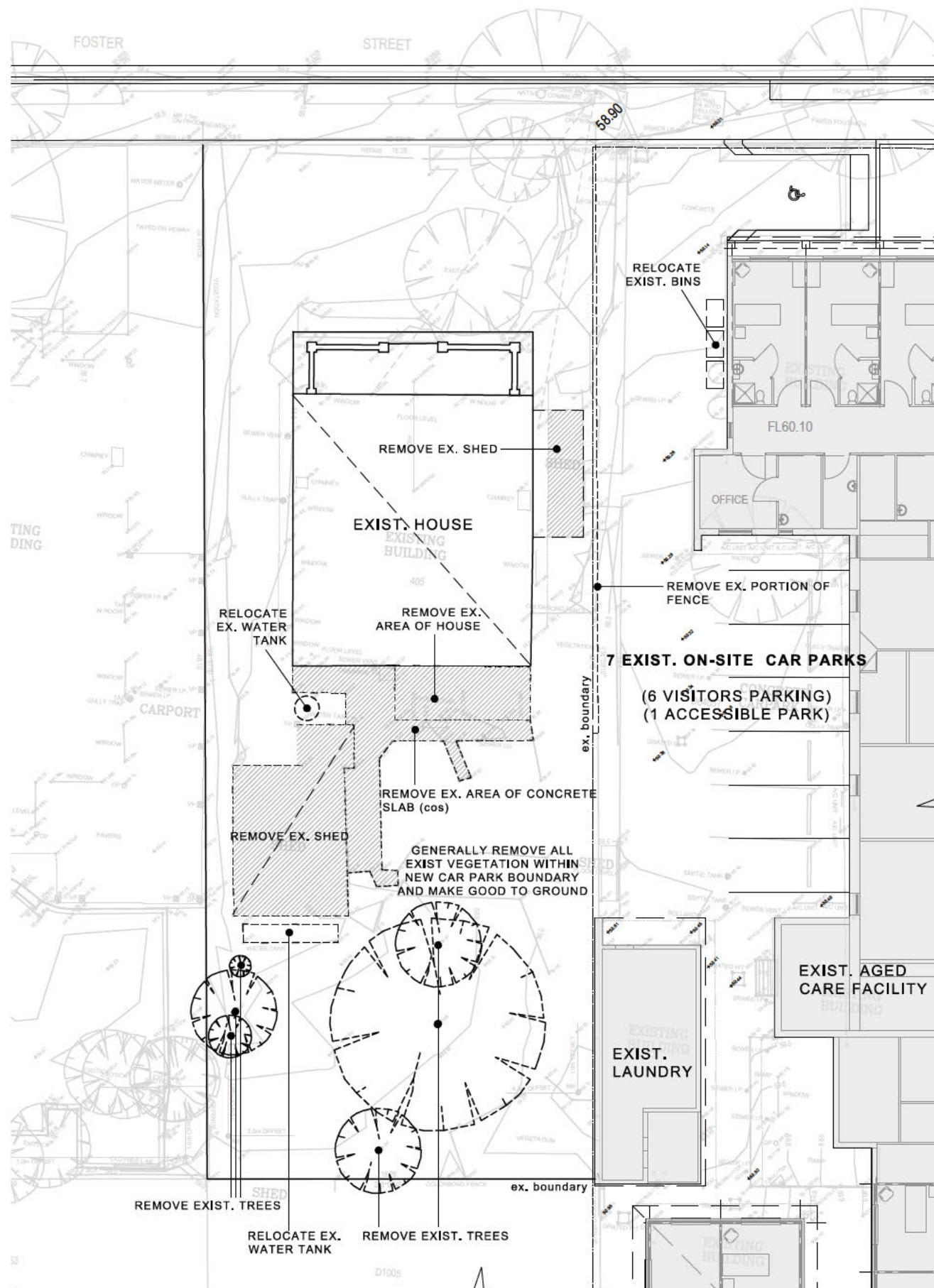
Project  
**ST LOUIS AGED CARE PARKSIDE**  
 21 FOSTER STREET, PARKSIDE

Drawing  
 LOCATION PLAN &  
 PROPOSED CAR PARK PLAN

Scale	AS SHOWN	Drawn	JT
Client	-		
Date	09/02/2022		
Job No.	202100069		
Dwg No.	<b>DA02</b>	Rev. A	A3 SHEET







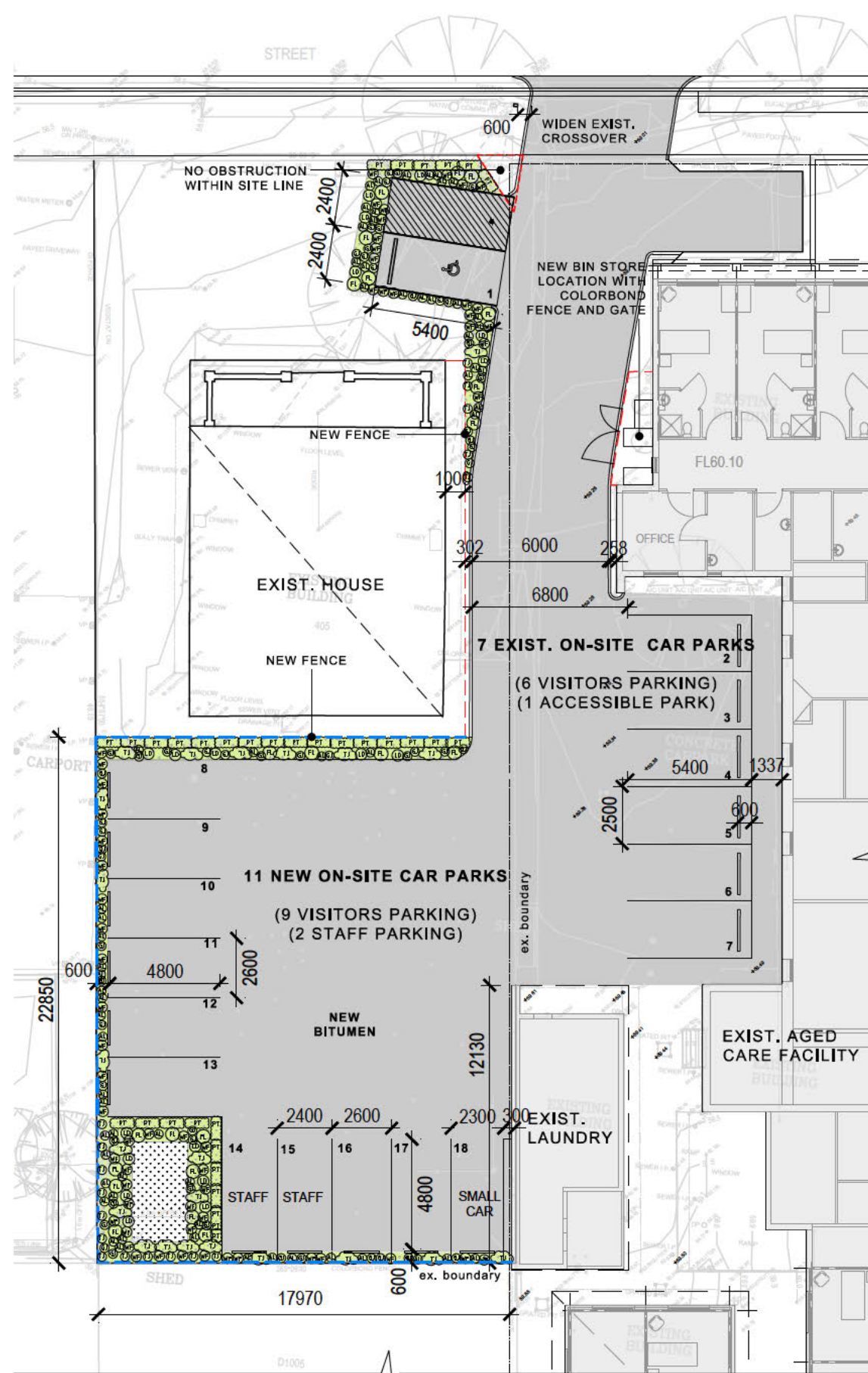
## CAR PARK DEMOLITION PLAN

0 5 10M

SCALE 1:250 @ A3

### LEGEND

--- ITEMS TO BE DEMOLISHED



## PROPOSED CAR PARK PLAN

0 5 10M

SCALE 1:250 @ A3

## PLANNING ISSUE

Rev	Amendment	Date

### LEGEND

SYNTHETIC LAWN, PAUL MUNNS SUMMER PRESTIGE 20MM ON 100MM RUBBLE BASE TO MANUFACTURERS DETAILS

NEW LANDSCAPING AREA( ALLOW 150mm TOP SOIL )

NEW 1.8m COLORBOND FENCE

NEW 2.4m COLORBOND FENCE

### NOTES:

PROVIDE NEW AUTOMATIC IRRIGATION SYSTEM (OR EXTEND EXISTING) TO ALL NEW PLANTING AREAS

REFER CIVIL DRAWINGS FOR NEW STORMWATER REQUIREMENTS

REFER TO DA06 FOR LANDSCAPE PLANTING DATA

PROVIDE TRELLIS OR CLIMBING WIRE TO FENCE FOR STAR JASMINE CLIMBER



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
CAR PARK DEMOLITION PLAN &  
PROPOSED CAR PARK PLAN

Scale AS SHOWN Drawn JT

Client

Date 09/02/2022

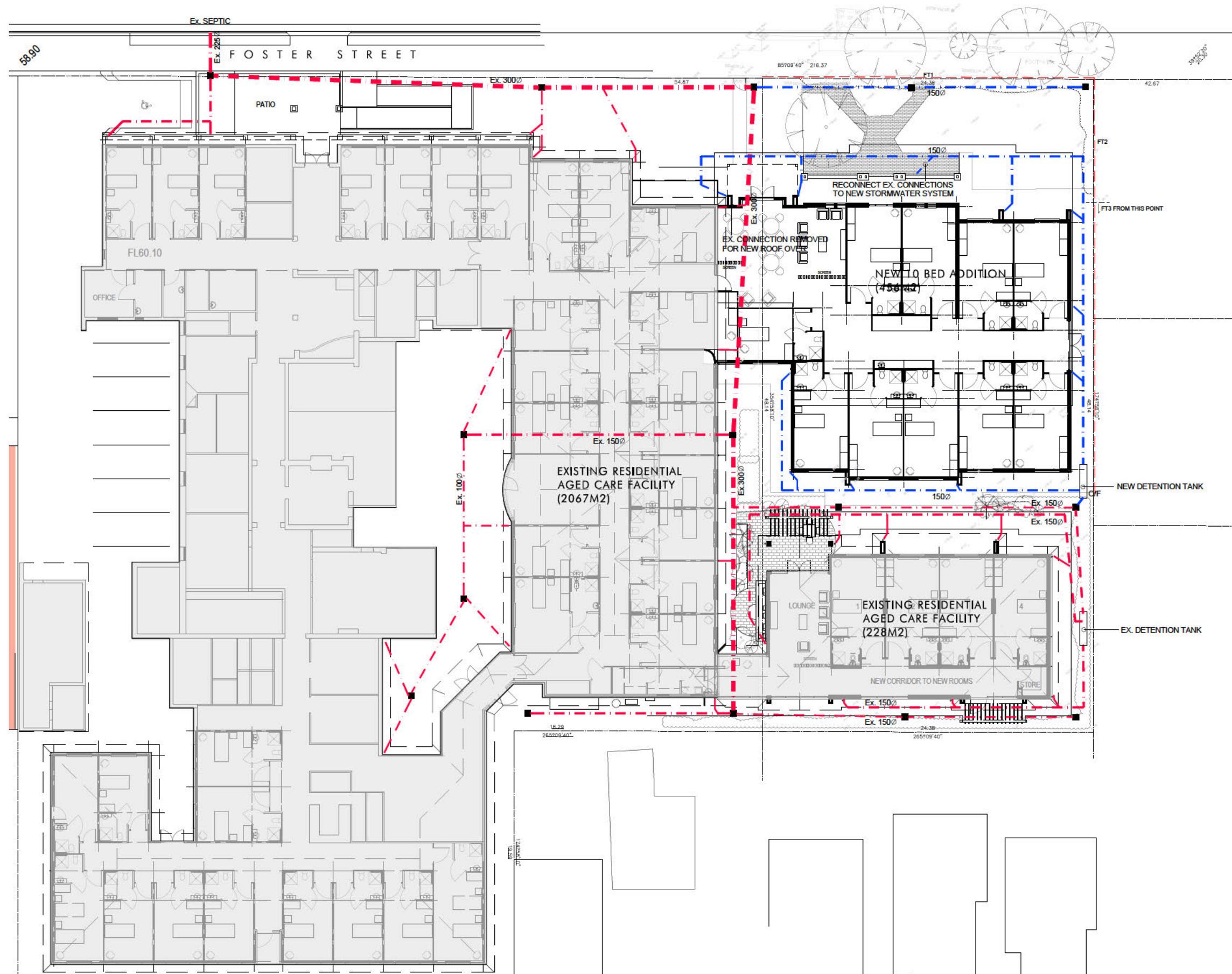
Job No. 202100069

Dwg No. DA03

Rev. A

A3 SHEET





# OVERALL SITE PLAN

0 7.5 15M  
SCALE 1:300 @ A3

## PLANNING ISSUE

Rev	Amendment	Date
-	-	-

### LEGEND

- EXISTING LEVELS
- EXISTING STORMWATER
- CONNECT NEW STORMWATER INTO EXISTING SYSTEM
- GRATED STORMWATER PIT
- FT1 STEEL FENCE TO MATCH EXISTING NORTHERN FENCE, ON 500mmH BRICK WALL
- FT2 1800H COLORBOND FENCE TO MATCH EXISTING
- FT3 1800H COLORBOND FENCE TO MATCH EXISTING ON RETAINING WALL TO ENGR'S DETAIL



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

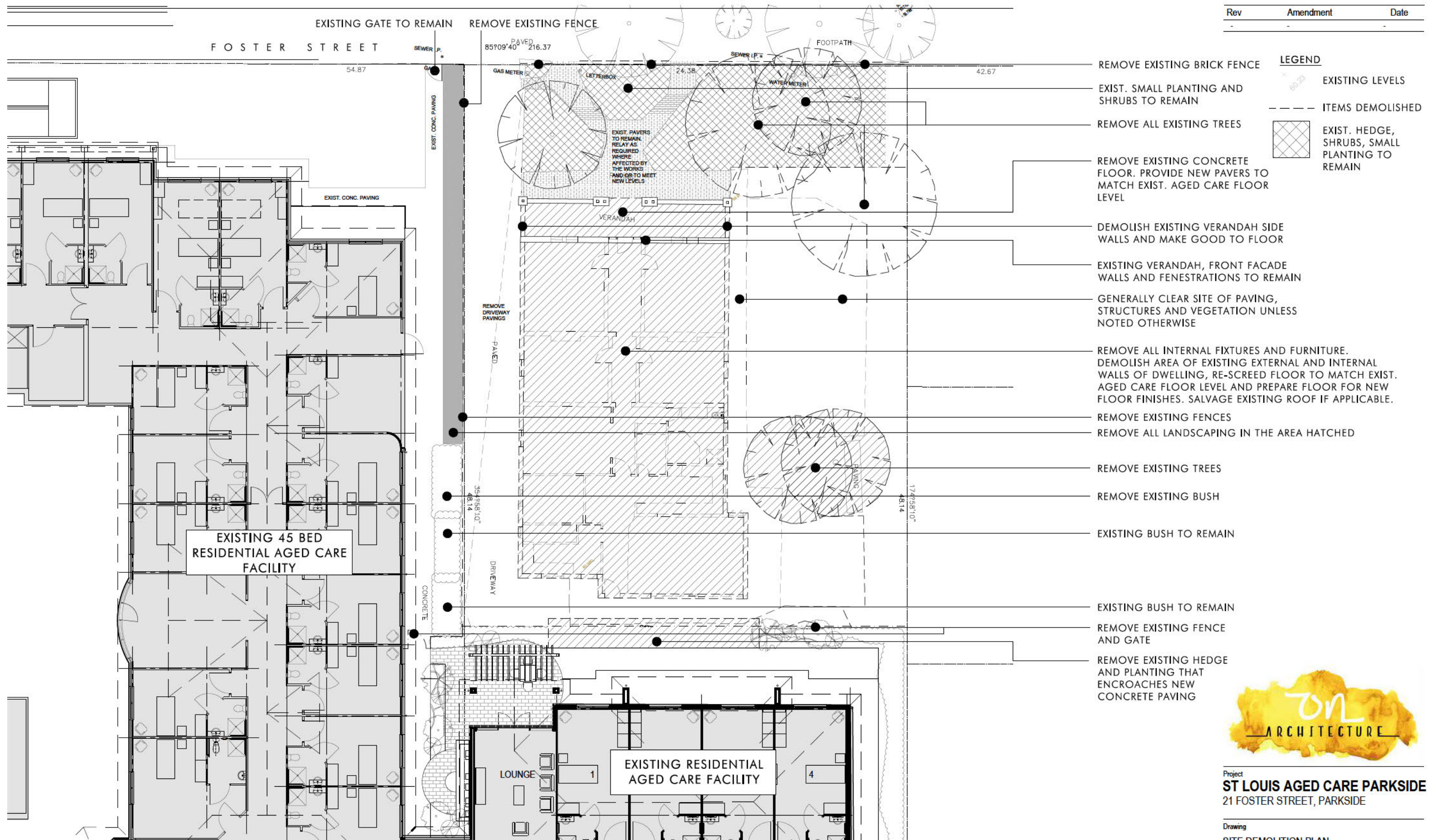
Drawing  
**OVERALL SITE PLAN**

Scale	1:300	Drawn	JT
Client	-		
Date	09/02/2022		
Job No.	202100069		
Dwg No.	<b>DA03</b>	Rev.	A A3 SHEET



# PLANNING ISSUE

Rev	Amendment	Date
-	-	-



## SITE DEMOLITION PLAN

0 5 10M

SCALE 1:200 @ A3



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

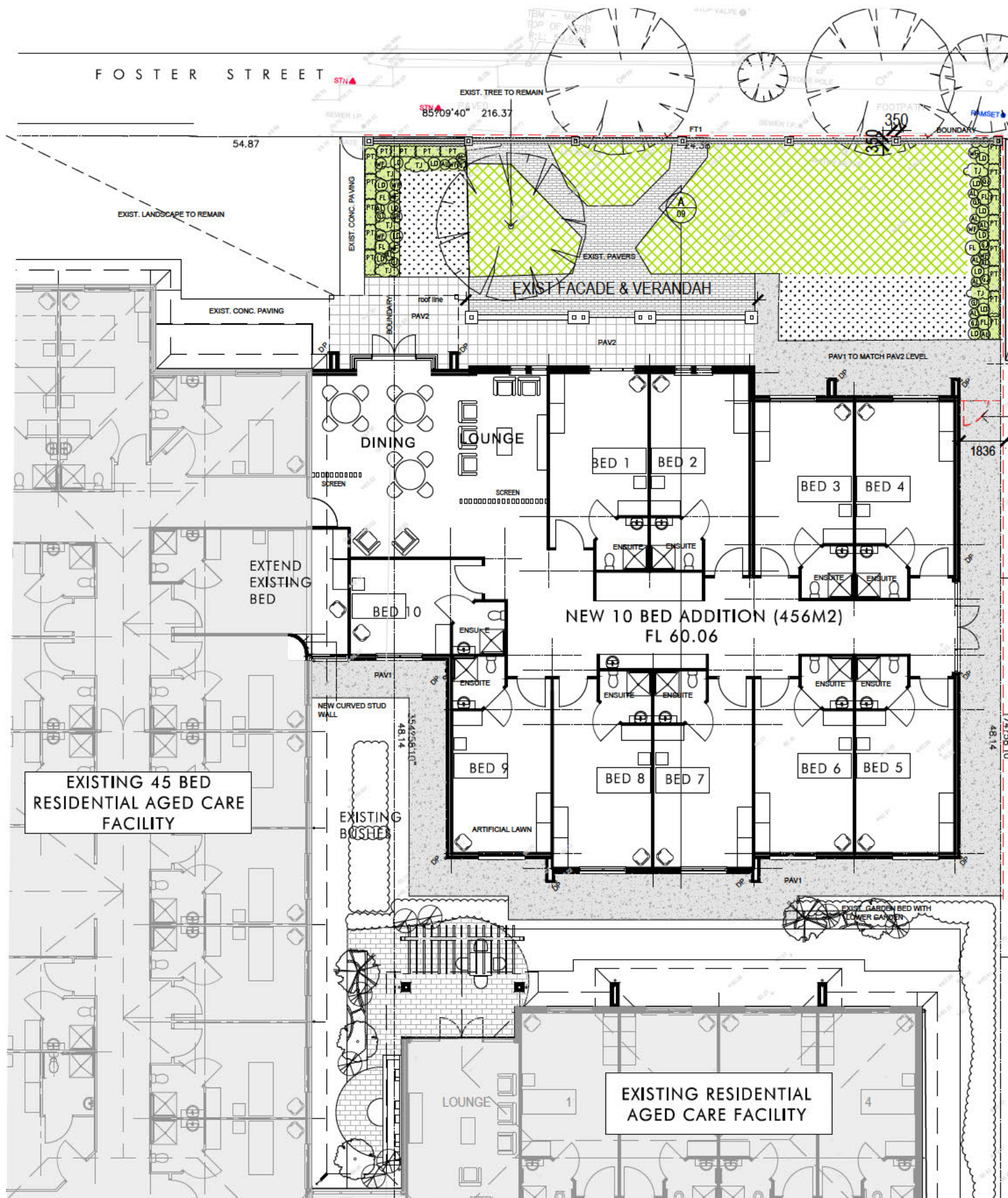
Drawing  
**SITE DEMOLITION PLAN**

Scale	1:200	Drawn	JT
Client	-		
Date	09/02/2022		
Job No.	202100069		
Dwg No.	<b>DA04</b>	Rev.	A
			A3 SHEET



PLANNING ISSUE

Rev	Amendment	Date
-	-	-



**LEGEND**

EXISTING LEVELS

RAINWATER DOWNPIPES

FT1 STEEL FENCE TO MATCH NEIGHBOURING NORTHERN FENCE, ON 500mmH BRICK WALL

FT2 COLORBOND FENCE TO MATCH EXISTING

FT3 COLORBOND FENCE TO MATCH EXISTING ON RETAINING WALL TO ENGR'S DETAIL

EXIST. HEDGE, SHRUBS, SMALL PLANTING

NEW LANDSCAPING AREA (ALLOW 150mm TOP SOIL)

PAV1 NEW REINFORCED CONCRETE PAVING TO MATCH EXISTING

EXISTING CONCRETE PAVING

SYNTHETIC LAWN, PAUL MUNNS SUMMER PRESTIGE 20MM ON 100MM RUBBLE BASE TO MANUFACTURERS DETAILS

PAV2 BEST PAVERS, BLACK SERIES FLINDERS CHARCOAL 400X400X40 TERRAZZO PAVERS

EXISTING CLAY PAVERS, RELAY AS REQUIRED WHERE AFFECTED BY THE WORKS AND/OR TO MEET NEW LEVELS

**NOTES:**

PROVIDE NEW AUTOMATIC IRRIGATION SYSTEM (OR EXTEND EXISTING) TO ALL NEW PLANTING AREAS

REFER CIVIL DRAWINGS FOR NEW STORMWATER REQUIREMENTS

PROVIDE TRELLIS OR CLIMBING WIRE TO FENCE FOR STAR JASMINE CLIMBER

PLANT SCHEDULE

KEY	BOTANICAL NAME	COMMON NAME	POT SIZE	SPACING	FULL SIZE
HEDGES AND CLIMBERS					
PT	PITTOSPORUM TENUIFOLIUM	PITTOSPORUM HEDGE	140mm	1/m2	1500mm X 2000mm
TJ	TRACHELOSPERMUM JASMINOIDES	STAR JASMINE (CLIMBER)	140mm	1M APART	2000mm X 500mm
BUSH/GROUNDCOVER					
LD	LANDANDULA DENTATA	FRENCH LAVENDER	140mm	4/m2	500mm X 600mm
AL	LIRIOPE MUSCARI AMETHYST	AMETHYST LIRIOPE	140mm	6/m2	400mm X 400mm
WF	WESTRINGIA FRUITCOSA	NATIVE ROSEMARY	140mm	4/m2	450mm X 450mm
FL	DIANELLA CAERULEA	FLAX LILLY	140mm	4/m2	600mm X 600mm
TJ	TRACHELOSPERMUM JASMINOIDES	STAR JASMINE (GROUND COVER)	140mm	2/m2	2000mm X 500mm
GJ	GARDENIA JASMINOIDES 'STAR'	GARDENIA STAR	140mm		500mm X 600mm

ITEM	DESCRIPTION	FINISH / APPLICATION
Imported Topsoil	Jeffries 'Special Soil'	150mm Depth mixed 50/50 with existing site topsoil.
Herbicide	Non-residual glysophate herbicide	Apply to subgrade as per manufacturers specification
Soil conditioner	Terracottem	20g per plant (at time of planting)
Fertiliser	Osmocote 'Total'	Mix with topsoil around rootball of plants at time of planting 5g per 1 litre of potting mix.
Mulch	Jeffries Forest Mulch	50mm Deep. Finish 25mm below adjacent pavers.

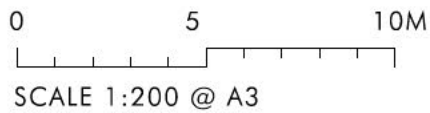


Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
SITE PLAN/FLOOR PLAN

Scale	1:100	Drawn	JT
Client	-		
Date	09/02/2022		
Job No.	202100069		
Dwg No.	DA05	Rev.	A
			A3 SHEET

SITE LANDSCAPE PLAN/FLOOR PLAN



PLANT IMAGES



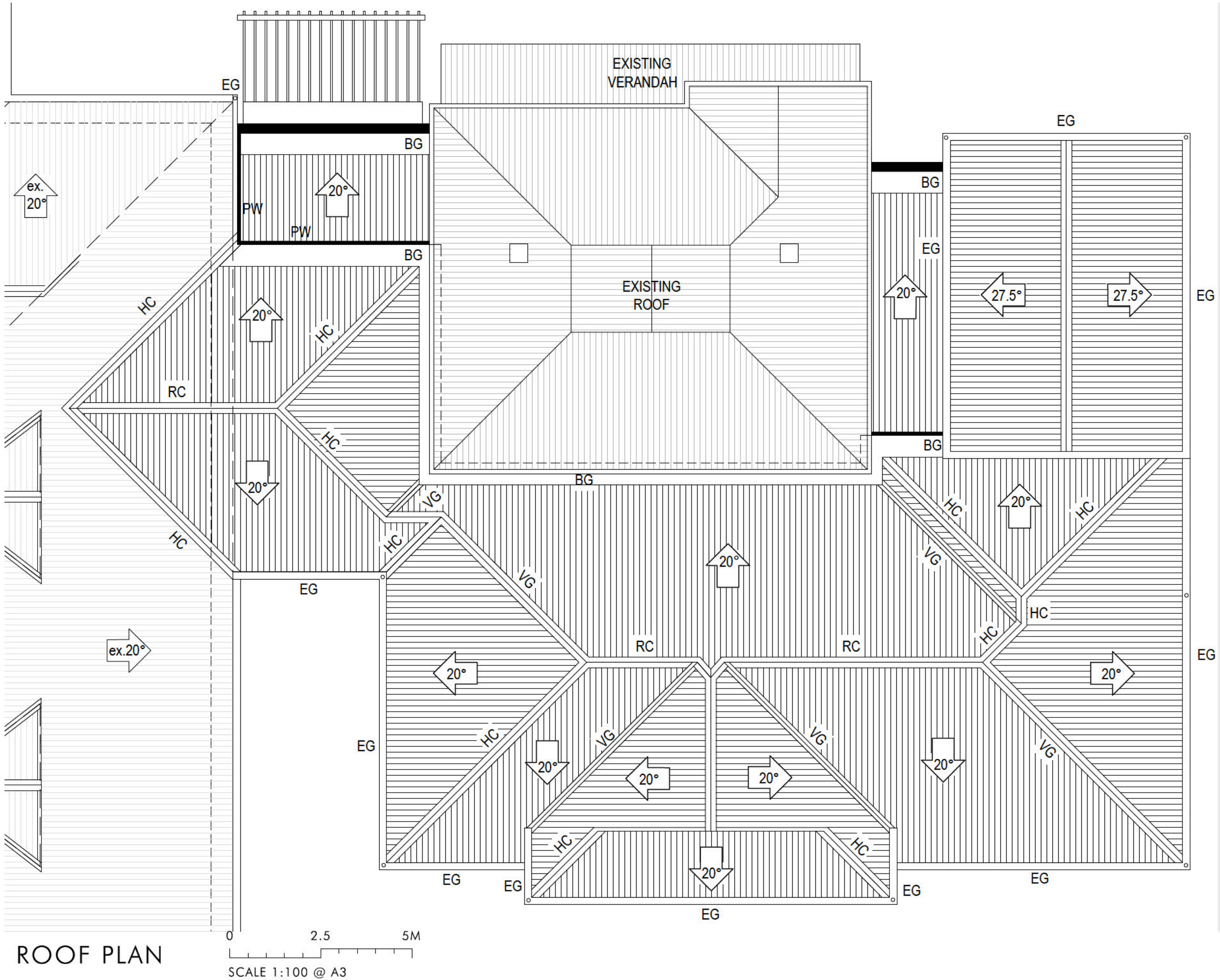


# PLANNING ISSUE

Rev	Amendment	Date
-	-	-

## LEGEND

- ex. DENOTES EXISTING
- EG COLORBOND EAVES GUTTER TO MATCH EXISTING
- HP HIP CAPPING
- BC BARGE CAPPING
- RC RIDGE CAPPING
- VG VALLEY GUTTER
- DP DOWNPIPES
- BG BOX GUTTER (600x150min)
- PW PARAPET WALL
- SP SPREADER TO LOWER ROOF
- 20° ROOF PITCH AND DIRECTION
- COLORBOND CORRUGATED STEEL ROOF CLADDING TO MATCH EXIST NG



Project  
**ST LOUIS AGED CARE PARKSIDE**  
 21 FOSTER STREET, PARKSIDE

Drawing  
**ROOF PLAN**

Scale	1:100	Drawn	JT
Client	-		
Date	09/02/2022		
Job No.	202100069		
Dwg No.	<b>DA06</b>	Rev.	A
			A3 SHEET



## ELEVATIONS

0 2.5 5M

SCALE 1:100 @ A3

NOTE:  
LANDSCAPING OMITTED  
FOR CLARITY



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
ELEVATIONS

Scale	1:100	Drawn	JT
Client	-		
Date	09/02/2022		
Job No.	202100069		
Dwg No.	DA07	Rev.	A

A3 SHEET



# PLANNING ISSUE

Rev	Amendment	Date
-	-	-

SPLIT FACE SANDSTONE CLADDING TO  
MATCH FRONT OF EXISTING RESIDENCE

COLORBOND ROOF CLADDING @  
20° PITCH TO MATCH EXISTING

TOP ROOF LEVEL 6200

CEILING LEVEL 2700  
EAVES LEVEL 2400

GROUND FLOOR 0 (RL 60.06)

EXISTING AGED CARE FACILITY

## EAST ELEVATION

1:100

ALUMINIUM DOORS AND  
WINDOWS TO MATCH EXISTING

RED BRICK TO MATCH EXISTING

EXISTING RESIDENCE BEYOND

EXISTING RESIDENCE BEYOND

EXISTING HERITAGE FRONT

TOP ROOF LEVEL 6200

CEILING LEVEL 2700  
EAVES LEVEL 2400

GROUND FLOOR 0 (RL 60.06)

EXISTING AGED CARE FACILITY

NEW VERANDAH TO MATCH EXISTING

## WEST ELEVATION

1:100

## ELEVATIONS

0 2.5 5M

SCALE 1:100 @ A3



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
ELEVATIONS

Scale	1:100	Drawn	JT
Client	-		
Date	09/02/2022		
Job No.	202100069		
Dwg No.	DA08	Rev.	A
			A3 SHEET

NOTE:  
LANDSCAPING OMITTED  
FOR CLARITY

TOP EXIST. ROOF LEVEL 6800 cos.

TOP ROOF LEVEL 4700

CEILING LEVEL 3035 cos.  
CEILING LEVEL 2700

GROUND FLOOR 0 (RL 60.06)

BRICK VENEER WALL  
WITH INSULATION

ROOF SPACE

COLORBOND ROOF CLADDING  
@20° ON TIMBER TRUSS ROOF  
FRAMING

BOX GUTTER

EXIST. ROOF

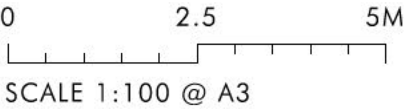
EXIST. WALL & FENESTRATION

EXIST. VERANDAH

RAFT FOOTING TO ENGINEER'S  
DESIGN

SECTION A  
1:100

SECTION



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

Drawing  
SECTION

Scale	1:100	Drawn	JT	
Client	-			
Date	09/02/2022			
Job No.	202100069			
Dwg No.	DA09	Rev.	A	A3 SHEET

NOTE:  
LANDSCAPING OMITTED  
FOR CLARITY




Rev	Amendment	Date
-	-	-



Project  
**ST LOUIS AGED CARE PARKSIDE**  
21 FOSTER STREET, PARKSIDE

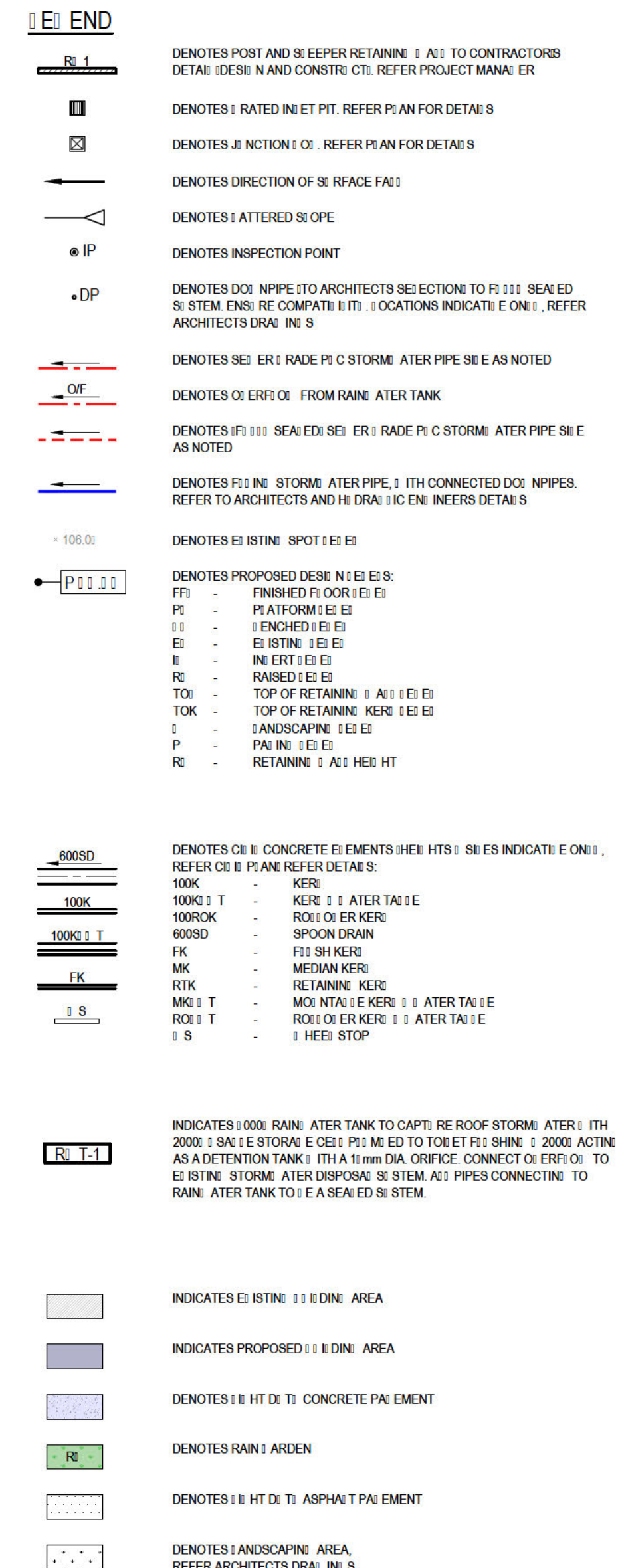
Drawing  
RENDERS

Scale	-	Drawn	JT	
Client	-			
Date	09/02/2022			
Job No.	202100069			
Dwg No.	<b>DA10</b>	Rev.	A	A3 SHEET



## **APPENDIX 3. STORMWATER**

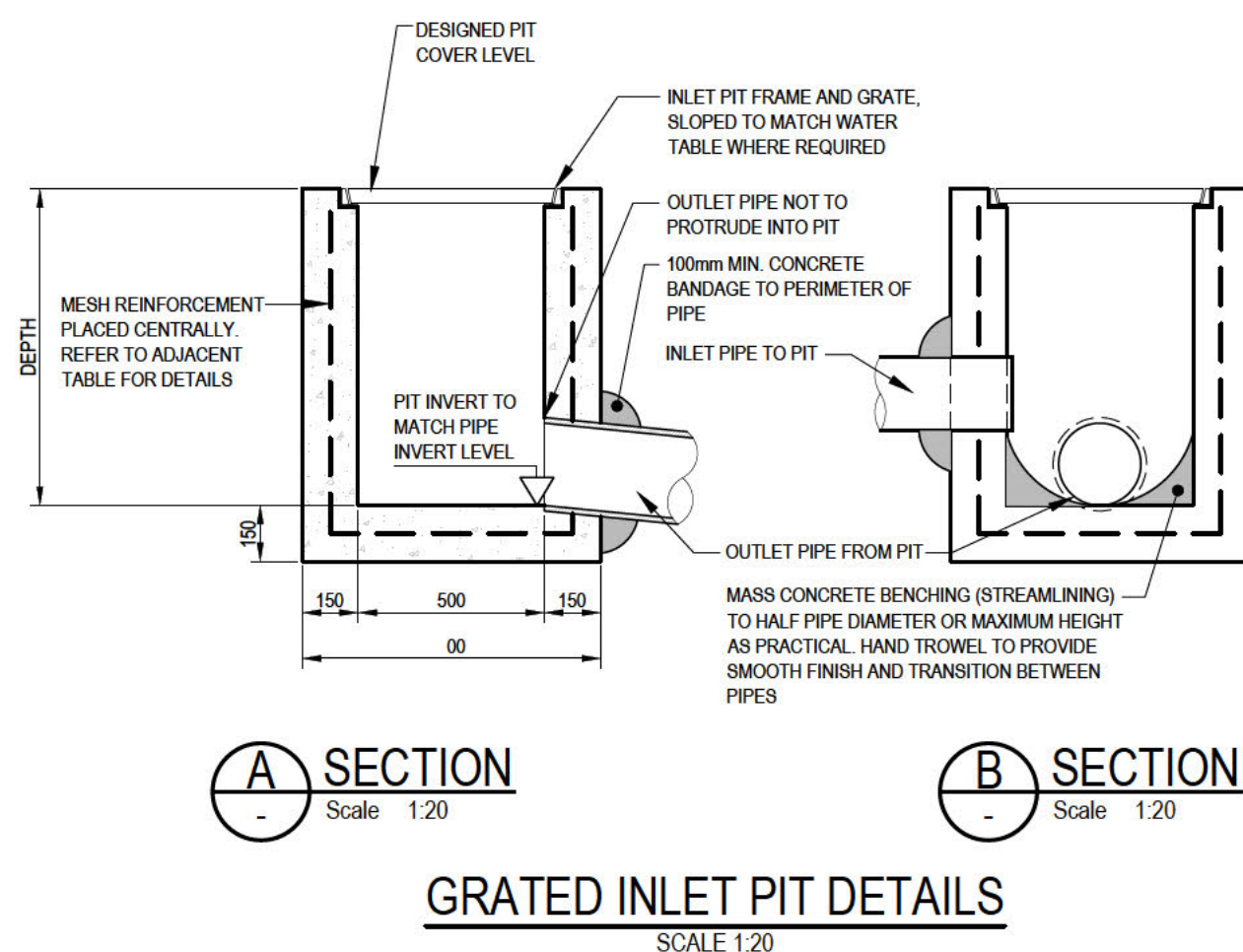
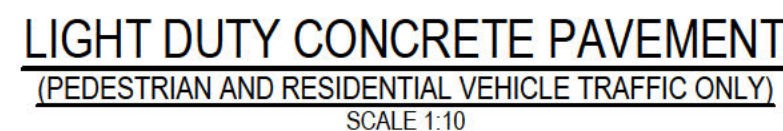




## SCA E 1:200

## 66





DEPTH	REINFORCEMENT
00-1000	SL72 MESH
1001-1300	SL82 MESH
1301-1600	SL92 MESH
1601-1900	SL102 MESH
>1900	DESIGN

APPROVAL ISSUE  
NOT FOR CONSTRUCTION



**JACK ADCOCK CONSULTING PTY. LTD**  
**STRUCTURAL & CIVIL ENGINEERING**  
**TELEPHONE:** +61 8 7226 2868  
**WEBSITE:** jackadcock.com.au  
**EMAIL:** admin@jackadcock.com.au

JAC220043-DRG- C002	ISSUE:	A
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## STORMWATER MANAGEMENT PLAN

Project No. JAC220043  
Project Name St. Louis Aged Care  
Site Address 21 Foster Street, Parkside SA

Architect On Architecture

Date 04 May 2022  
Prepared By WL

### Revisions

No.	Date	Author	Reviewed	Notes
A	04/05/2022	WL		Issued for approval

## 1. INTRODUCTION

Jack Adcock Consulting Pty Ltd has been engaged by On Architects on behalf of the site owner to prepare a stormwater management plan for the proposed development to be located at 21 Foster Street, Parkside SA.

The development is within the City of Unley.

This stormwater management plan outlines the design concept for the management of stormwater on the site, for planning approval purposes.

## 2. SITE DESCRIPTION

The site area is approximately 820 m<sup>2</sup> carpark and 830 m<sup>2</sup> extension.

The site in its current state, has a gentle gradient toward Foster Street and is 90% impervious, occupied by the floor of a previous building and vehicular paving and a small carpark. Refer to the below aerial photo.

The stormwater drainage system from the previous development drained to side entry pit on Foster street through the existing footpath.



**Aerial Photo of the Existing Site**



### **3. PROPOSED DEVELOPMENT**

With reference to the Architect's planning drawings, the proposed development consists of the following:

- New addition to aged care facilities
- New carpark with 11 parking spaces
- Landscaped frontage and at the back

### **4. DESIGN CRITERIA**

In accordance with Council requirements, the following fundamental design requirements have been considered:

1. Stormwater runoff generated from impervious areas (paving + roof) discharged from the site is to be less than pre-development conditions using 5 year ARI 45min duration.
2. Runoff calculations for pre-development flow calculations have been based on coefficients appropriate to the existing surface conditions.
3. Runoff from the site must satisfy EPA and DPTI quality requirements.
4. The proposed development must not adversely affect the surrounding environment and adjacent sites after construction is completed.
5. Stormwater runoff shall be managed by detaining water on site so as not to exceed the maximum allowable flow rates.
6. Maximum of 4 L/s discharge allowed per discharge point to the street watertable.
7. Raingardens have been used to collect surface runoff as much as possible.

### **5. WATER QUALITY**

In order to meet Council requirements, an ECOSOL GPT 4200 Gross Pollutant Trap is proposed for each carpark collection point.

### **6. STORMWATER CALCULATIONS**

Refer to attached pages.



## STORMWATER DETENTION DESIGN

page

### A. Design Rainfall Data System 2016 from Bureau of Meterology

Suburb = Unley

Latitude = -34.95054

Longitude = 138.62

Duration (mins)	Annual Exceedance Probability AEP (%)						
	63.2	50	20	10	5	2	1
5	51.2	58.1	81.8	100.0	120.0	149.0	173.0
10	37.2	42.2	59.6	72.8	87.0	108.0	125.0
15	29.9	34.0	48.0	58.7	70.1	86.8	101.0
20	25.4	28.8	40.7	49.8	59.5	73.7	85.8
25	22.3	25.3	35.6	43.6	52.1	64.6	75.2
30	19.9	22.6	31.9	39.0	46.6	57.8	67.4
45	15.5	17.6	24.7	30.2	36.1	44.8	52.3
60	12.9	14.6	20.5	25.0	29.9	37.2	43.4

### B. Pre-development and Post-development Area

	Pre-development	Post-development
$A_L$ , Land ( $m^2$ )	822.8	822.8
$A_r$ , Roof ( $m^2$ )	100.0	0.0
$A_i$ , impervious ( $m^2$ )	349.0	183.0
$A_p$ , pervious ( $m^2$ )	373.8	0.0



C - 1

page

### C. Equivalent Impervious Area

Run-off coefficients	Pre-development	Post-development
$C_r$ , roof	1.0	1.0
$C_i$ , impervious	0.9	0.9
$C_p$ , pervious	0.3	0.3
Equivalent run-off coefficient	0.64	0.20
$\Sigma CA$ , Equivalent Impervious Area ( $m^2$ )	526.2	164.7

### D. Design Flows and Detention Volume

*Pre-development* -  $Q_R = \Sigma CA * I_R / 3600$

Design ARI =	1	in	5	year
Design AEP =	20	%		
Design Duration =	45	minutes		
Rainfall Intensity, $I_R$ =	24.7	mm/hr		
Calculated flow rate, $Q_R$ =	3.6	L/s		
Design restricted flow rate, $Q_D$ =	3.6	L/s		

ARI	AEP (%)
1	63.2
1.4	50
5	20
10	10
20	5
50	2
100	1

$ARI = 1/(-\log_e(1-AEP))$



C - 2 *Post-development : separate into roof stormwater and surface stormwater detention* page

Design ARI = 1 in 20 year

Design AEP = 5 %

Post development - roof stormwater detention

Restricted flow = 0.0 L/s

Duration (min)	Rain intensity (mm/hr)	Flow rate (L/s)	Flow to detain (L/s)	Detention (L)
5	120.0	0.0	0.0	0
10	87.0	0.0	0.0	0
15	70.1	0.0	0.0	0
20	59.5	0.0	0.0	0
25	52.1	0.0	0.0	0
30	46.6	0.0	0.0	0
45	36.1	0.0	0.0	0
60	29.9	0.0	0.0	0
			<b>TOTAL</b>	<b>0</b>

Post development - surface stormwater detention

Restricted flow = 3.6 L/s

Duration (min)	Rain intensity (mm/hr)	Flow rate (L/s)	Flow to detain (L/s)	Detention (L)
5	120.0	5.5	1.9	563.8226667
10	87.0	4.0	0.4	221.7953333
15	70.1	3.2	0.0	0
20	59.5	2.7	0.0	0
25	52.1	2.4	0.0	0
30	46.6	2.1	0.0	0
45	36.1	1.7	0.0	0
60	29.9	1.4	0.0	0
			<b>TOTAL</b>	<b>563.8226667</b>





## STORMWATER DETENTION DESIGN

page

### A. Design Rainfall Data System 2016 from Bureau of Meterology

Suburb = Unley

Latitude = -34.95054

Longitude = 138.62

Duration (mins)	Annual Exceedance Probability AEP (%)						
	63.2	50	20	10	5	2	1
5	51.2	58.1	81.8	100.0	120.0	149.0	173.0
10	37.2	42.2	59.6	72.8	87.0	108.0	125.0
15	29.9	34.0	48.0	58.7	70.1	86.8	101.0
20	25.4	28.8	40.7	49.8	59.5	73.7	85.8
25	22.3	25.3	35.6	43.6	52.1	64.6	75.2
30	19.9	22.6	31.9	39.0	46.6	57.8	67.4
45	15.5	17.6	24.7	30.2	36.1	44.8	52.3
60	12.9	14.6	20.5	25.0	29.9	37.2	43.4

### B. Pre-development and Post-development Area

	Pre-development	Post-development
$A_L$ , Land ( $m^2$ )	821.0	821.0
$A_r$ , Roof ( $m^2$ )	257.0	536.0
$A_i$ , impervious ( $m^2$ )	194.0	91.0
$A_p$ , pervious ( $m^2$ )	370.0	195.0

C - 1

page

**C. Equivalent Impervious Area**

Run-off coefficients	Pre-development	Post-development
$C_r$ , roof	1.0	1.0
$C_i$ , impervious	0.9	0.9
$C_p$ , pervious	0.3	0.3
Equivalent run-off coefficient	0.66	0.82
$\Sigma CA$ , Equivalent Impervious Area ( $m^2$ )	542.6	676.4

**D. Design Flows and Detention Volume**

$$Pre-development - Q_R = \Sigma CA * I_R / 3600$$

Design ARI =	1	in	5	year
Design AEP =	20	%		
Design Duration =	45	minutes		
Rainfall Intensity, $I_R$ =	24.7	mm/hr		
Calculated flow rate, $Q_R$ =	3.7	L/s		
Design restricted flow rate, $Q_D$ =	3.7	L/s		

ARI	AEP (%)
1	63.2
1.4	50
5	20
10	10
20	5
50	2
100	1

$$ARI = 1/(-\log_e(1-AEP))$$





C - 2 *Post-development : separate into roof stormwater and surface stormwater detention* page

Design ARI = 1 in 5 year

Design AEP = 20 %

Post development - roof stormwater detention

Restricted flow = 0.5 L/s

Duration (min)	Rain intensity (mm/hr)	Flow rate (L/s)	Flow to detain (L/s)	Detention (L)
5	81.8	12.2	11.7	3503.733333
10	59.6	8.9	8.4	5024.266667
15	48.0	7.1	6.6	5982
20	40.7	6.1	5.6	6671.733333
25	35.6	5.3	4.8	7200.666667
30	31.9	4.7	4.2	7649.2
45	24.7	3.7	3.2	8579.4
60	20.5	3.1	2.6	9188
			<b>TOTAL</b>	<b>9188</b>

Post development - surface stormwater detention

Restricted flow = 3.2 L/s

Duration (min)	Rain intensity (mm/hr)	Flow rate (L/s)	Flow to detain (L/s)	Detention (L)
5	81.8	3.2	0.0	0
10	59.6	2.3	0.0	0
15	48.0	1.9	0.0	0
20	40.7	1.6	0.0	0
25	35.6	1.4	0.0	0
30	31.9	1.2	0.0	0
45	24.7	1.0	0.0	0
60	20.5	0.8	0.0	0
			<b>TOTAL</b>	<b>0</b>





## **APPENDIX 4. TRAFFIC AND PARKING**

JML/22-0044

6 May 2022

Mr Marc Duncan  
Future Urban  
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ADELAIDE SA 5000

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MFY Pty Ltd

ABN 79 102 630 759

Dear Marc,

**PROPOSED EXPANSION, ST LOUIS AGED CARE  
17-23 FOSTER STREET, PARKSIDE**

We refer to the proposal to expand an existing aged care facility (supported accommodation) in Foster Street Parkside.

The subject site comprises the existing facility and two adjacent dwellings as shown in Figure 1.



**Figure 1: Subject site**



It is proposed to incorporate the dwelling to the east into the facility to provide 10 additional beds.

The dwelling to the west of the facility will be retained, with the backyard being developed to provide additional parking for the expansion of the facility. It is proposed to construct 11 additional parking spaces in this area. An additional parking space for people with disabilities will also be constructed in front of this dwelling.

The new parking spaces will comply with the requirements of the relevant *Australian/New Zealand Standard, Parking Facilities Part 1: Off-street parking facilities (AS/NZS 2890.1:2004)*, in that:

- standard spaces will be 5.4 m long, or 4.8 m long overhanging low lying landscaping;
- spaces allocated to staff will be at least 2.4 m wide and spaces for visitor parking will be 2.6 m wide;
- the parking aisle will be at least 5.8 m wide;
- small car spaces will be at least 2.3 m wide and 5.0 m long; and
- all spaces and aisle will have 300 mm clearance to obstructions greater than kerb height such as walls and fences.

The existing internal driveway will be widened to at least 5.5 m, which will provide for two-way traffic flow for staff and visitors entering and exiting the site.

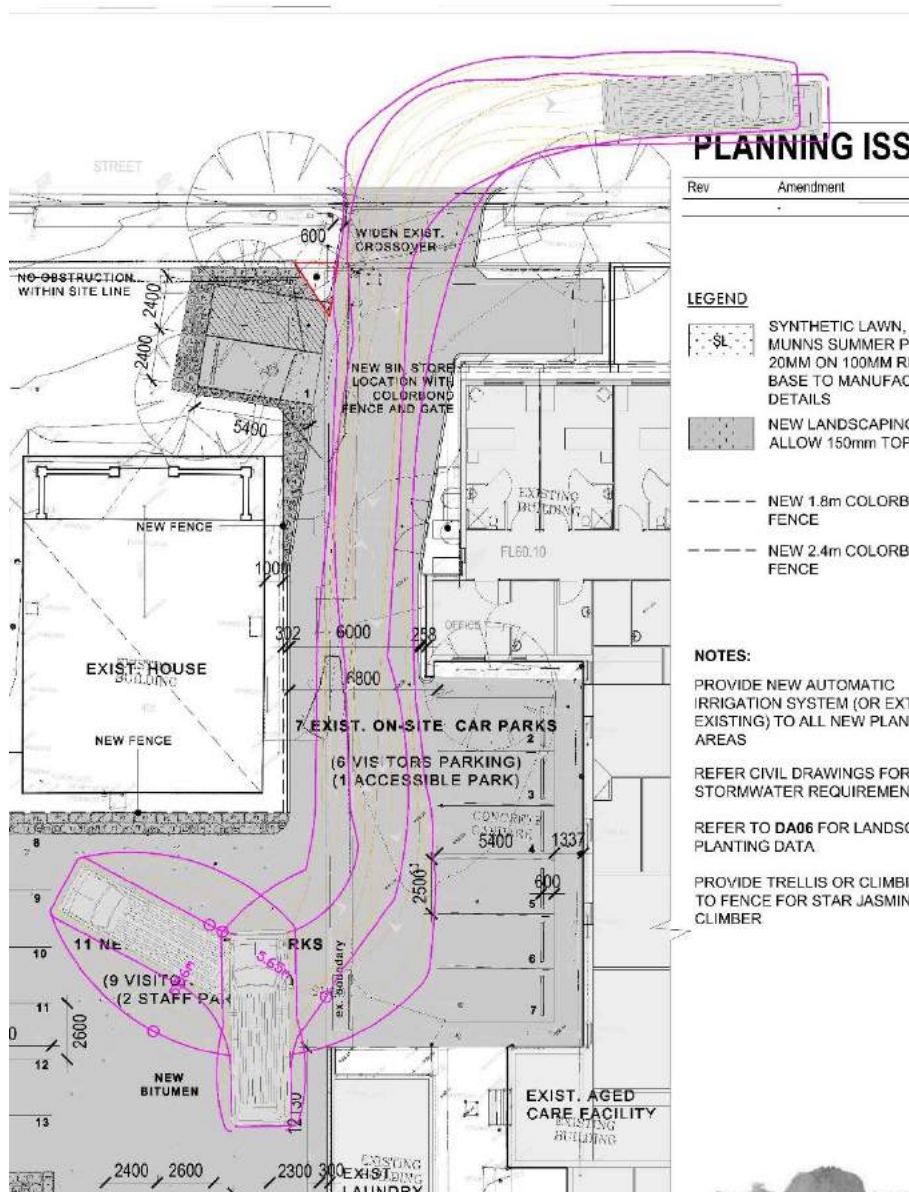
The new parking space for people with disabilities will comply with the requirements of the relevant *Australian/New Zealand Standard, Parking Facilities Part 6: Off-street parking for people with disabilities (AS/NZS 2890.6:2009)*, in that a 5.4 m long and 2.4 m wide parking spaces will be provided adjacent to a similarly sized shared zone.

The plans show wheel stops in the new parking spaces, but these would not be required for installation in 4.8 m long spaces which overhang landscaped areas, as the kerb has been brought forward to limit the vehicle path instead. A wheel stop should also not be provided in the parking space for people with disabilities, as this is non-compliant with the Standard. These changes to the plan can be readily made incorporated into the detailed design.

The aged care site currently has a driveway on Foster Street which provides access to parking on the site. The driveway and circulation roadway are one lane wide and there is not sufficient space on the site for manoeuvring to allow drivers of commercial vehicles to turn and therefore enter/exit in a forward direction.

As part of the proposed works, the existing driveway will be widened and angled at no greater than 20° from perpendicular to the road. A 2 m by 2.5 m wide triangle adjacent to the western side of the crossover will also be provided, kept clear of obstructions, in order to provide appropriate sightlines to pedestrians. This will provide simultaneous two-way access to the site and will maintain appropriate sightlines for drivers exiting the site.

The larger hardstand area at the rear of the dwelling at 23 Foster Street will allow drivers of commercial vehicles to turn on site, and thereby enter and exit in a forward direction, as shown in Figure 2.



**Figure 2: Swept path Medium Rigid Vehicle**

The subject site is located within an Established Neighbourhood Zone, and as such, is subject to the PlanSA *Planning and Design Code (P&DC)* parking rate for aged care facilities (supported accommodation) of 0.3 spaces per bed. The proposed 10-bed expansion will therefore require three additional spaces. The increase in parking on the site will provide these spaces, along with nine additional spaces. These extra spaces would support up to 27 additional beds on the site.

The proposal will result in only a small number of additional staff and visitors to the site and the additional traffic generated by these vehicles will be offset by the reduction in traffic associated with the repurposed use of the existing residential dwelling. As such, the proposal will have a negligible impact on the traffic volumes on the adjacent road network.





In summary, the proposed development will provide for improved access to and circulation within the site, and will provide additional parking to accommodate the small demand associated with the proposed 10-bed expansion. There will be a negligible traffic impact on the road network as a result of the works.

Please contact me should you require clarification or further information.

Yours sincerely,

**MFY PTY LTD**

A handwritten signature in black ink, appearing to read 'J Lovell'.

**JAYNE LOVELL**

Senior Associate



## **APPENDIX 5. ACOUSTIC**





# St Louis Aged Care Parkside

## Environmental Noise Assessment

27 April 2022

Reference ID: 20-3

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## Abbreviations

BMT	Base Metal Thickness
DO	Desired Outcome of the Code
DTS	Deemed to Satisfy criteria of the Code
EPA	South Australian Environment Protection Authority
PO	Performance Outcome of the Code
WHO	World Health Organization

## Glossary

<b>A-weighting</b>	A mathematical adjustment to the measured noise levels to represent the human response to sound. An <i>A-weighted noise level</i> is presented as dB(A).
<b>Ambient noise level</b>	The noise level associated with the environment in the absence of the activity under investigation.
<b>Characteristic</b>	A characteristic determined in accordance with the <i>Environment Protection (Noise) Policy 2007</i> (the Policy) to be fundamental to the nature and impact of the noise. For example, a noise source is deemed to exhibit a characteristic if it produces distinctive tonal, impulsive, low frequency or modulating features.
<b>Code</b>	<i>Planning and Design Code</i> Version 2022.6 dated 31 March 2022, PlanSA.
<b>Day</b>	A period defined by the <i>Environment Protection (Noise) Policy 2007</i> as between 7am and 10pm.
<b>EP Act</b>	<i>Environment Protection Act 1993</i>
<b>Equivalent noise level</b>	The A-weighted noise level which is equivalent to a noise level which varies over time. The descriptor is $L_{Aeq}$ and it is the A-weighted <i>source noise level (continuous)</i> referenced in the Policy. The $L_{Aeq}$ is also referenced as an average noise level in this assessment for simplicity.
<b>dB</b>	The logarithmic unit of measurement to define the magnitude of a fluctuating air pressure wave. Used as the unit for <i>sound or noise level</i> . An <i>A-weighted noise level</i> is presented as dB(A).
<b>Indicative Noise Level</b>	The noise level assigned by the Policy at a location to represent an impact on the acoustic amenity at that location. No further action is required to be taken under the <i>Environment Protection Act 1993</i> for noise levels which are lower than the Indicative Noise Level.
<b>Instantaneous maximum noise level</b>	The A-weighted noise level which is the instantaneous maximum over a period. The $L_{Amax}$ is the A-weighted instantaneous maximum noise level referenced in Clause 20(b)(ii) of the Policy.

<b>Night</b>	A period defined by the <i>Environment Protection (Noise) Policy 2007</i> as between 10pm and 7am.
<b>Noise</b>	An interchangeable term with sound but which is most often described as <i>unwanted sound</i> .
<b>Noise Sensitive Premises</b>	Premises that could be “noise-affected”. For the purposes of this assessment, the noise sensitive premises are dwellings.
<b>Policy</b>	The <i>Environment Protection (Noise) Policy 2007</i>
<b>Sound</b>	An activity or operation which generates a fluctuating air pressure wave. The ear drum can perceive both the frequency (pitch) and the magnitude (loudness) of the fluctuations to convert those waves to sound.
<b>Sound power level</b>	The amount of sound energy an activity produces for a given operation. The sound power level is a constant value for a given activity. The sound power level is analogous to the power rating on a light globe (which remains constant), whereas the lighting level in a space (sound pressure level in this analogy) will be influenced by the distance from the globe, shielding and different locations within the space.
<b>Sound pressure level</b>	The magnitude of sound (or noise) at a position. The sound pressure level can vary according to location relative to the noise source, and operational, meteorological and topographical influences.
<b>WHO Guidelines</b>	<i>Guidelines For Community Noise</i> Birgitta Berglund Thomas Lindvall Dietrich H Schwela London, United Kingdom, April 1999, World Health Organization.

# Executive Summary

---

The proposed development at 21 Foster Street, Parkside, for the St Louis Aged Care facility, comprises a new car parking and driveway arrangement in the western portion of the site and a new 10-bedroom addition in the eastern portion of the site.

Many of the noise sources remain unchanged by the proposed arrangement and only the new noise sources are considered further in this assessment.

The assessment process includes the prediction of noise levels based on measurements of deliveries at the site, established inputs for car parking and the operation of the mechanical services systems which are equivalent to those currently serving the facility. The predicted noise levels are compared against standards in the Planning and Design Code to provide an objective measure of adverse impacts on the amenity of an area.

In the circumstance where the noise levels need to be reduced to achieve the relevant requirements, this assessment provides the recommended control measures. The objective of the above process is to ensure the new activity at the facility does not adversely impact on the amenity of surrounding dwellings.

This assessment determines the proposed new activity can reasonably and practicably achieve the relevant standards through the implementation of the following:

- constructing a solid fence of a minimum height and construction between the new car park and the nearest dwellings
- restricting deliveries to the site to between the hours of 7am and 10pm
- maintaining the current location on the site for unloading deliveries
- ensuring delivery trucks turn off any roof mounted refrigeration systems before entering the site
- specifying staff carparks that will be used after 10pm or before 7am
- specifically locating mechanical plant away from the residential boundary
- reviewing the final mechanical services systems during the design stage of the project to ensure compliance with the *Environment Protection (Noise) Policy 2007*.



# Introduction

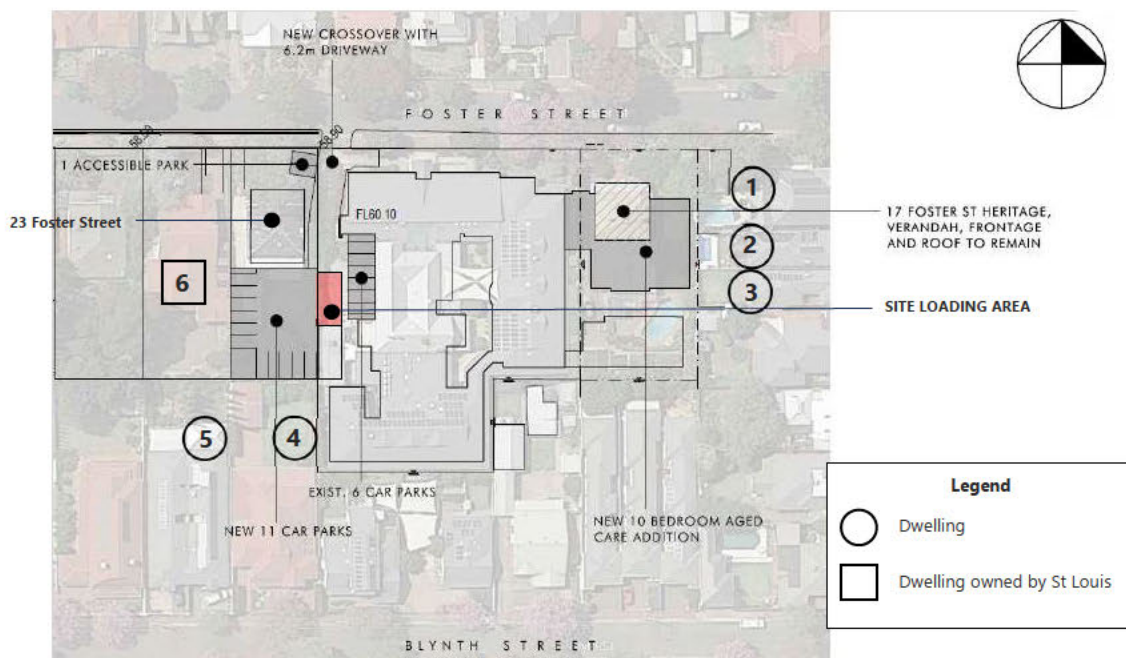
The proposed development at 21 Foster Street, Parkside for the St Louis Aged Care facility (the facility), comprises a new car parking and driveway arrangement in the western portion of the site and a new 10-bedroom addition in the eastern portion of the site.

Many of the noise sources remain unchanged by the proposed arrangement and only the new noise sources are considered further in this assessment. The changes at the facility which are the subject of this assessment comprise:

- vehicle movements in the new car parking area
- existing deliveries manoeuvring in the new car parking area
- operation of air conditioning and ventilation systems serving the new 10-bedroom addition.

The St Louis Aged Care facility owns the closest dwelling to the west. The proposed development at the facility and surrounding dwellings are shown in Figure 1 below.

Figure 1 The facility and surrounding dwellings



Source On Architecture Drawing 202100069 DA01 Rev A dated 9 February 2022

In addition to the site loading area shown in Figure 2, there is a designated loading zone on Foster Street adjacent the facility entry. It is understood the arrangement will reduce the need for the use of the loading zone on Foster Street, improving the acoustic amenity for those residents in and around the zone. The new western carpark also provides the opportunity for delivery vehicles to manoeuvre, removing the current need to reverse into or out of the full length of the driveway. This improves safety on the site and at the public crossover / entry onto Foster Street.

# Assessment Criteria

---

## The Code

The facility and the nearest sensitive premises (dwellings) are located within an *Established Neighbourhood Zone* of the *Planning and Design Code Version 2022.6* dated 31 March 2022 (the Code). The following provisions within the Code are considered relevant to the environmental noise assessment.

### Established Neighbourhood (Part 2 – Zone and Sub Zones)

#### *Performance Outcome PO 1.2*

Commercial activities improve community access to services are of a scale and type to *maintain residential amenity*.

#### *Performance Outcome PO 1.3*

Non-residential development sited and designed to *complement the residential character and amenity* of the neighbourhood.

### Interface between Land Uses (Part 4 – General Development Policies)

#### *Desired Outcome DO 1*

Development is located and designed to *mitigate adverse effects* on or from neighbouring and proximate land uses.

#### *Performance Outcome PO 1.2*

Development adjacent to a site containing a sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers is *designed to minimise adverse impacts*.

#### *Performance Outcome PO 2.1*

Non-residential development *does not unreasonably impact the amenity of sensitive receivers* (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having regard to:

- a) the nature of the development
- b) measures to mitigate off-site impacts
- c) the extent to which the development is desired in the zone
- d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land.

#### *Performance Outcome PO 4.1*

Development that emits noise (other than music) *does not unreasonably impact the amenity of sensitive receivers* (or lawfully approved sensitive receivers).

#### *Deemed to Satisfy Criteria DTS 4.1*

Noise that might affect sensitive receivers achieves the relevant Environment Protection (Noise) Policy criteria.

## The Policy

Deemed to Satisfy Criteria 4.1 references the *Environment Protection (Noise) Policy 2007* (the Policy).

The Policy was developed under the *Environment Protection Act 1993* (the EP Act). The EP Act incorporates a requirement to ensure the acoustic *amenity of a locality is not unreasonably interfered with*. The Policy provides a quantitative approach to satisfy this requirement underpinned by the World Health Organization's *Guidelines for Community Noise* (WHO Guidelines) as it relates to community annoyance and sleep disturbance.

Compliance with the Policy will satisfy Deemed to Satisfy criteria DTS 4.1 and is considered to also satisfy the subjective requirements of the Desired and Performance Outcomes in the Code (being the Established Neighbourhood PO 1.2 and 1.3 and the Interface between land uses DO 1, PO 1.2, PO 2.1 and PO 4.1).

The Policy establishes noise levels that apply at noise sensitive premises (dwellings) for both the day (7am to 10pm) and night (10pm to 7am the following day) periods. These noise levels vary according to the land use zoning in which the facility and the dwellings are located.

For new activity at the facility, the noise criteria that applies at existing dwellings not associated with St Louis is the *Indicative Noise Level* minus 5 dB(A), and an instantaneous maximum noise level during the night, as summarised below:

- an average noise level of 47 dB(A) during the day
- an average noise level of 40 dB(A) during the night
- an instantaneous maximum noise level of 60 dB(A) during the night.

The "average noise level" is an equivalent noise level over a default assessment period of 15 minutes.

Dwelling 6 (as identified in Figure 1) is owned by St Louis Aged Care. In the interests of conservatism, the assessment has considered this dwelling in the circumstance where it is sold in the future. The noise criteria that apply in this situation is the *Indicative Noise Level* provided by the Policy, as summarised below:

- an average noise level of 52 dB(A) during the day
- an average noise level of 45 dB(A) during the night.

The dwelling at 23 Foster Street is understood to be part of the facility and is not considered further in this assessment.

When predicting noise levels for comparison to the Policy, the predicted noise levels are to be adjusted (increased) where the activities exhibit "annoying" characteristics (dominant tonal, impulsive, low frequency content or modulation characteristics) in comparison to the surrounding ambient environment.



# Assessment

---

## The Policy

The Policy is utilised by this assessment to satisfy the Code requirements that relate to noise from new activity at the facility including:

- vehicle movements in the new car parking area
- existing deliveries manoeuvring in the new car parking area
- operation of air conditioning and ventilation systems serving the new 10-bedroom addition.

There are existing approved activities at the site which do not change in impact as a result of the development, including deliveries unloading on the site, parking in the existing on-site car parks and operation of existing plant and equipment. Existing approved activities which do not change in impact are not considered further in this assessment.

## Noise Data Inputs

The following noise inputs have been used for the assessment of the proposed operations over the default 15-minute period of the Policy:

- 6 vehicles using the carpark between 7am and 10pm with a sound power level of 81 dB(A) per arrival<sup>1</sup> (manoeuvring into the parking space, opening and closing doors and conversing)
- 4 staff vehicles using the staff carpark before 7am or after 10pm at shift change over with a sound power level of 81 dB(A) per arrival (as per above)
- A refrigerated vehicle manoeuvring in the new car park after 7am and prior to 10pm with a sound power level of 98 dB(A)<sup>2</sup> with the refrigeration system on and with a sound power level of 93 dB(A) with the refrigeration system off
- The application of a penalty of 5 dB(A) to the predicted noise levels in the vicinity of the new car park (due to modulation from vehicles)
- Each bedroom having an individual air conditioning condensing unit with a sound power level of 61 dB(A)
- a common exhaust system serving the extension with a sound power level of 75 dB(A)
- The dining and living room having an air conditioning condensing unit with a sound power level of 75 dB(A).

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<sup>1</sup> Sound power levels for passenger vehicle activity in accordance with the Association of Australasian Acoustical Consultants (AAAC) *Guideline for Child Care Centre Acoustic Assessment* Version 3.0

<sup>2</sup> Sound power levels (refrigeration system on and off) based on measurements of a PFD refrigerated fixed axle truck

## Predicted Noise Level

Noise predictions have been made and summarised in for the identified dwelling locations.

Table 1 Predicted Noise Levels dB(A)

Dwelling	Predicted cumulative noise level Car parking and plant operation (dB(A))			Compliance
	Day	Night		
	L <sub>Aeq</sub>	L <sub>Aeq</sub>	L <sub>Amax</sub>	
Criteria	47	40	60	
1	36	36	<40	Yes
2	34	34	<40	Yes
3	37	37	<40	Yes
4	47	37	57	Yes
5	45	36	55	Yes
Criteria	52	45	NA	
6	48	44	-	Yes

## Noise Reduction Measures

With reference to Table 1, the proposal will achieve the assessment criteria required to satisfy the Code subject to the following noise reduction measures:

- Ensure the extent of the fence depicted as blue in Figure 2 is a minimum of 2.4m in height (when measured from the finished car park level at the facility). The fence should be constructed from solid sheet steel with a BMT of 0.42mm, or an alternative material with the same or greater surface density. The fence should also be sealed airtight at all junctions, including with the ground, at the laundry building, and at the overlap of any sheets
- restrict deliveries at the facility to between the hours of 7am and 10pm
- maintain the current location on the site for unloading deliveries
- ensure delivery vehicles turn off any roof mounted refrigeration systems before entering the site (as currently occurs with the PFD foods truck when unloading in the street)
- only use carparks 8, 9, 10 or 11 for staff vehicles after 10pm or before 7am
- locate individual condensing units immediately outside of the rooms they serve and not on the eastern side of the addition
- locate any exhaust system discharges on the western half of the extension
- locate the dining and lounge condensing unit on the western side of the addition at ground level.

Figure 2 Noise Reduction Measures



Source Plan SA – SA Property & Planning Atlas, and On Architecture Drawing 202100069 DA01 Rev A dated 9 February 2022

## Mechanical Services

The mechanical plant has not yet been designed (as is common at the planning application stage of a project). Therefore, the type of system, its noise levels and the final siting is unknown. As a result, there are specific recommendations relating to the ventilation and air conditioning systems in the assessment to be completed during the stage of the project when this design aspect has been completed.

Based on the assessment to date, the air conditioning and ventilation system design can reasonably and practicably achieve the Code. It is recommended that a review of the final new mechanical services systems be made during the design stage of the project to ensure compliance with the *Environment Protection (Noise) Policy 2007*.



# Conclusion

---

The proposed development at 21 Foster Street, Parkside, for the St Louis Aged Care facility, comprises a new car parking and driveway arrangement and a new 10-bedroom addition.

Many of the noise sources remain unchanged by the proposed arrangement and only the new noise sources are considered in the assessment.

The environmental noise assessment considers the predicted noise levels from the proposed changes against standards established in accordance with the Planning and Design Code and the *Environment Protection (Noise) Policy 2007* to ensure the acoustic amenity of the surrounding sensitive premises (dwellings) is not adversely impacted.

This assessment determines the new activity at the facility can reasonably and practicably achieve the relevant standards through implementation of the following:

- constructing a solid fence of a minimum height and construction between the new car park and the nearest dwellings
- restricting deliveries to the site to between the hours of 7am and 10pm
- maintaining the current location on the site for unloading deliveries
- ensuring delivery trucks turn off any roof mounted refrigeration systems before entering the site
- specifying staff carparks that will be used after 10pm or before 7am
- specifically locating mechanical plant away from the residential boundary
- reviewing the final mechanical services systems during the design stage of the project to ensure compliance with the *Environment Protection (Noise) Policy 2007*.

Based on the above, the assessment concludes the proposed development will not adversely impact on the amenity of any dwelling in the locality and will provide a facility which will meet the relevant Planning and Design Code provisions.

# References

---

*Planning and Design Code* Version 2022.6 dated 31 March 2022, PlanSA

*Environment Protection (Noise) Policy 2007*, SA EPA

*Guidelines For The Use Of The Environment Protection (Noise) Policy 2007*, SA EPA June 2009

*On Architecture Drawings 202100069 DA Set Rev A dated 9 February 2022*

*Guidelines For Community Noise* Birgitta Berglund Thomas Lindvall Dietrich H Schwela London, United Kingdom, April 1999, World Health Organization

# Document Details

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## Distribution:

<b>Issue date</b>	27 April 2022
<b>Issued to</b>	Future Urban and St Louis Parkside Aged Care
<b>Description</b>	Environmental Noise Assessment

## Author:

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<b>Email</b>	██

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## **ATTACHMENT 2**

# Details of Representations

## Application Summary

Application ID	22015437
Proposal	Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)
Location	17 FOSTER ST PARKSIDE SA 5063, 23 FOSTER ST PARKSIDE SA 5063, 19-21 FOSTER ST PARKSIDE SA 5063

## Representations

### Representor 1 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	19/05/2022 01:05 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development

### Reasons

I am against this car park being built against my property because it will 1. Create excessive noise from the vehicles (especially delivery trucks). 2. Create excessive noise from the drivers and passengers loitering in the carpark. 3. Create pollution from car and truck exhausts. 4. The applicant states that delivering trucks will turn off any roof mounted refrigeration systems before entering the site. This is impossible to be policed. 5. All deliveries between 7am and 10 pm this is 15 hours per day when deliveries can be made. Firstly how do you police that these times which I don't agree with are adhered to. Aren't these times for delivery excessive. Parkside is a quiet neighborhood that's why I live here. 6. The car park will light up my back yard during the night. The constant head lights from staff and delivery vehicles. The fence will not be sufficient to stop the lighting up of my backyard. 7. The carpark will require security lighting for staff and residents. Therefore the car park will have lighting all night so this means my backyard will be in constant light all night. This is not acceptable. 8. Safety reasons. The car park will be open 24hours to the street. Therefore undesirables entering the car park will be able to access my house from the rear. This presents a problem of safety for me. I have gone through great lengths to secure the front of my house now the rear presents safety issues. 9. I have an issue with the fence. This fence will create shade problems for my garden especially my citrus trees. Also will my fence be replaced by this 2.4 meter fence or be erected next to my fence? The previous time the applicant St. Louis Nursing Home erected a fence and retaining wall on our border I was not notified. As we are neighbours I would like to be informed so that we can come to a mutual agreement. 10. The applicant states that they will only use car park 8 9 10 or 11 for staff vehicles after 10pm or before 7am. How do you know that this will be adhered to. 11. This car park next to my house will devalue my house which is exactly what St. Louis Nursing Home wants. I have made my backyard a pleasant place to be in. This car park will destroy ~~910~~ that. Car parks should be placed in the front of the property facing the street not in the backyard where you affect all

adjacent homes with excessive noise, exhaust pollution and constant lighting at night. Kind regards

## Attached Documents



## Representations

Representor 2 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	19/05/2022 06:39 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development
<b>Reasons</b> Just bringing to your attention that this car park development affects 2 adjacent properties 1, 38 Blyth St. Parkside owned [REDACTED]. 25 Foster St. Parkside owned [REDACTED]. Obviously there will be no objection to the car park from 25 Foster St. Parkside. This car park development is placed in this area so that it only affects [REDACTED] to mitigate objections to the development and to devalue my property. Kind regards [REDACTED]	

## Attached Documents

## Representations

Representor 3 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	20/05/2022 01:19 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns

### Reasons

The back of my property adjoins the side of 17 Foster St where the existing house will be demolished to build a 10 bed extension to St Louis. The trees and shrubs in 17 Foster St that currently overhang my fence will be removed as part of the demolition process. I have paid to have my back yard professionally netted to keep my cats in my yard so they are safe, not a nuisance to neighbours and not a risk to wildlife. This net is attached to my back fence where the shrubs and trees from 17 Foster Street that will be removed, overhang. My concern is that the net over my back yard may be damaged, cut, torn etc during the removal of shrubs and trees from 17 Foster Street. I am requesting the builders/developers to please be aware of the net attached to my back fence and to take great care not to damage it when working on the demolition and development. Thank you for your consideration.

## Attached Documents



## Representations

Representor 4 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	06/06/2022 04:00 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I support the development with some concerns

### Reasons

While we have no objections to the development in general there are a number of concerns that require consideration and addressing when approving the development, these are listed below; Traffic management is currently an issue along Foster Street with on-street parking of vehicles, delivery vans and trucks associated with St Louis Aged Care facility causing congestion and noise throughout the day. The additional 10 beds in the facility will result in an increase the traffic along Foster street, while the upgrade of the on-site parking is proposed it will not fully address the underlying issues and it would be beneficial to review the traffic management plan, in particular the delivery vans and trucks to reduce noise and congestion along the street, The development plan shows the Eastern side of the building is 1.8 meters from the boundary which will be quite imposing for all residents backing onto the St Louis Aged Care facility along Kenilworth Road. There is limited or no detail on the external lighting and any air conditioning along the Eastern side of the facility in the proposal. In addition, the site plan has a pathway between the building and eastern boundary with access to the eastern side of the facility, the recently added addition at the back of 17 Foster St, and the front of the property, this suggests the pathway will be used as a thoroughfare and access point for the eastern side of the building. The development application states that "Non-residential development does not unreasonably impact the amenity of sensitive receivers", as such, we request that any lighting is placed as near to the ground as feasible so there is no overflow to the adjoining properties. That no air conditioning services be placed along the eastern side of the building due to the close proximity to the adjoining properties. That the buildings access on the eastern and associated pathway is limited to emergency access only to reduce the day to day noise along the eastern side of the facility. Associated with the impact of the development on eastern side is the proposed fencing affecting the properties on Kenilworth Rd backing onto the St Louis Aged Care facility. The plans only indicate that a "1.8m ColorBond Fence to match existing on retaining wall to Engineers details", no other details are provided. A quick inspection of the existing fencing suggests that there are a number of different types of fencing with various heights along the eastern boundary, with some concrete footings and walls on the boundary line that have built over the years. Our property on [REDACTED] has a garage facing Foster St and along the boundary to 17 Foster street and behind that is a part of an old garage/structure that appears to be built on the boundary. The walls of the old garage are approximately 2.5 meters in height and surround an in-ground swimming pool that was constructed about 20 years ago. Our preference is to retain the existing fencing between our property and 17 Foster St. as any change will result in reworking the entire pools surrounds. We request that all the residence on Kenilworth Road with properties impacted by the fencing be consulted as a group and to consider the impact on each property and reach an agreement with regard to the fence design and construction to the satisfaction of all parties and reduce any visual and noise issues as a result of the development. The prevailing request for the proposed development is to retain a residential feel and to complement the existing street scape of the surrounding area. ?



## Attached Documents

ScivicGrou22060811411-3049150.pdf

## REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

*Planning, Development and Infrastructure Act 2016*

**Applicant:** St Louis Aged Care C/- Future Urban

**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

**Zone/Sub-zone/Overlay:** Established Neighbourhood Zone

**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

My name\*:

My phone number:

My postal address\*:

My email

*\* Indicates mandatory information*

My position is:

☐

I support the development

☒

I support the development with some **major** concerns, many seeking conditional requirements for development application approval.

☐

I oppose the development

## Executive Summary

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

## Summary of Foster Street Resident Recommendations

1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.
2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.
3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.
4. Foster Street residents request St Louis:
  - a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;
  - b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;
  - c. ensure no noise occurs at the development site before 7am, 7 days a week.
5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.
6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the existing facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.
7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.
8. Foster Street residents request that:
  - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and



- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for the new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.
9. Given that on-site space is proposed to be available, Foster Street residents therefore request that Council enforce all trucks/vehicle deliveries including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.
10. Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.
11. Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.
12. Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development
13. Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.
14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.
15. Foster Street residents remain significantly concerned about on-street parking implications from this development application and wish this concern noted by council for future deliberations regarding on-street parking and related traffic management issues.
16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.
17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.
18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.
19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to
-

establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.

The major concerns are detailed below and seek a range of key conditions and actions before such development is approved by Council.

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## **LAND USE**

### **Future Land Use**

- The planning documents refer to the provision of residential aged care as being suitable for the Established Neighbourhood Zone and that such use is compatible with the zone.
  - This is accepted at the size and scale of the current 45 bedroom facility.
  - However, it is noted that in a recent 2021 newsletter St Louis stated
    - *[St Louis] .."will be able to substantially expand St Louis nursing home located at 21 Foster Street Parkside. St Louis owns 5,000 square metres of land adjacent the nursing home [ie houses at 23,25,& 27 Foster Street on the western side], obtained over a number of years for precisely this purpose."*
  - Such a development would not be of the scale or form that the current service is.
  - While it is understood that the Development Assessment Panel (DAP) must limit its consideration to the application before them, residents are seeking transparency from St Louis of their intentions with respect to these houses.
  - It is noted that the houses west of the facility have not been kept in good repair, their gardens are significantly run down when compared to the rest of the street and the overall street scape is detrimental and impacts on residents' house values as a result.
  - Lack of clarity for the future of the houses on the western side causes significant concern for Foster Street residents.
- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**

### **Monthly Progress Meetings During Construction Phase and Preliminary Design Consultation**

- It is understood that St Louis have consulted widely with the Nursing Home residents and families regarding the design and build of the proposed new development.
  - No such consultation has been afforded to Foster Street residents even though there will be significant long term and short-term impacts on the street from this development.
  - It is also acknowledged that the construction phase of the development, if approved, will impact significantly on residents of Foster Street and surrounds. An appropriate communication process over this construction phase is sought as a priority.
- 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
- 3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.**
-



#### **Period of Building and capital development related to this current development application**

- Based on the 2019-20 development work, and should this development application progress, Foster Street residents are concerned about the additional noise, trucks, inappropriate and congested parking and start times, which from previous experience often commenced well before 7am.
- During the previous build in 2019-20 there was no formal process to raise concerns with the builder or St Louis management with sufficient delegation and authority.

#### **4. Foster Street residents request St Louis:**

- a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;***
- b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;***
- c. ensure no noise occurs at the development site before 7am, 7 days a week.***

#### **5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.**

### **BUILT FORM AND DESIGN**

- This development will add an additional 10 residential aged care beds to the eastern side of the property – taking the 'nursing home' as St Louis refers to the facility, up to 55 beds.
  - It is noted that the Royal Commission into Aged Care recommended that future aged care services have a small home approach.
  - It is recognised that the new addition, by keeping the required façade of the existing building will go some way to meeting that objective.
  - Our concern however is that the fencing and plantings around the new accommodation should provide a different and varied look from the existing fencing and plantings in the current 45 bed facility.
  - It is noted that the plan (DAO5) advises the fencing will match the existing St Louis fencing.
  - We are concerned that this will create an increased 'institutional' look to the new accommodation by having almost uniform fencing along the whole length of the existing aged care facility. This contrasts with the rest of Foster Street where fencing varies between each neighbour, therefore creating variety and interest along the street.
  - We feel it prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment.
- 6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the current facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.**
- 7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.**



### **Privacy**

- Residents of 20, 22 and 24 Foster Street, as the most immediately impacted residences, are concerned that the new development, particularly with the location of its proposed pergola for meetings and sitting outside will directly overlook their homes.
- 8. Foster Street residents request that:**
  - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and***
  - b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for this new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.***

### **INTERFACE BETWEEN LAND USES**

- The planning document states that consideration has been made to ensure the minimisation of any adverse impacts on the amenity of sensitive receivers.
- The Plan states that noise will be minimised by
  - a. constructing a 2.4 metre acoustic fence (understood to be a Wallmark construction with foam filler) on the boundary of 23 Foster Street adjacent to the new car park.
  - b. restricting deliveries to the site between the hours of 7am and 10pm
  - c. turn off refrigeration before entering the site; and
  - d. locating mechanical plant away from the residential boundaries.

### **Delivery Vehicles**

- At present residents of Foster Street are confronted with daily large commercial vehicles parked on the street with refrigeration noise sometimes for up to 20 minutes at very early hours of the morning, and other vehicles during the day. Early morning times in particular has often been against regulations and has been raised by various neighbours over many years.
- Of note, neighbours advise there continues to be a delivery that occurs regularly between 3-4am. By example, on the morning of 1 June 2022, the delivery vehicle pulled up at 3.15am. The vehicle has its engine off and glides to its delivery point which is close to 30 Foster Street. It then moves off as quietly as possible but still wakes people on the street. This is a clear breach of regulations.
- It is acknowledged that the new plans include dedicated on-site space so that commercial vehicles can unload within St Louis grounds which is not possible currently. The development application advises that this will reduce the volume of trucks/vehicles using the street for deliveries. Truck parking and unloading on the road restricts traffic flow and compromises street safety.
- The emptying of the large industrial site rubbish bin has to be carried out on the street as the truck cannot manage the activity fully on site. This blocks the street for a period of time or limits traffic to a narrow passageway.

9. ***Given that on-site space is proposed to be available, Foster Street Residents therefore request that Council enforce all trucks/vehicle deliveries, including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.***
10. ***Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.***
- It is acknowledged that on-site deliveries may cause noise problems for some residents of Blyth Street.
11. ***Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.***

#### **Air Conditioning and Plant Noise**

- At present the large air conditioner plant on the St Louis premises can be heard as a constant hum for houses on the other side of the street.
  - The noise consultant contracted by St Louis, has indicated that the placement of individual air conditioners for each new bedroom of the new development will address any noise concerns and that there will be no worsening of the existing noise.
12. ***Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development.***

#### **Light spill**

- Some street residents have been affected by light overspill, due to the installation of lighting presumably installed for safety reasons. Recent prompt intervention by the site manager minimised neighbourhood overspill from a poorly directed new LED replacement installation that pointed light at bedrooms. There is now an opportunity to review all current and proposed lighting for neighbourhood overspill. Luminaires that spill light in all directions are wasteful and compromise neighbourhood night time amenity and sleeping patterns. Most inappropriate lighting installations can be remedied by carefully considering what needs to be illuminated and installing energy efficient, properly shielded light fixtures for the purpose.
13. ***Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.***

## **TRAFFIC MANAGEMENT**

- The Plan states that the proposed addition does not result in unreasonable traffic implications, and further states that the new development will, in fact, assist in improving current access.

### **Delivery Vehicles**

- This issue is addressed above –

***14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.***

### **On-Street Parking**

- Parking on Foster Street is already significantly impacted by the St Louis staff and visitors, and made worse by the constant flow of commercial vehicles loading and unloading on Foster Street.
- Access and egress from our houses is often impacted by cars parked over our driveways. This is a serious safety concern when staff and visitors park too close to driveways compromising vision, turning and collision potential.
- There is little respect for Foster Street residents by poor or selfish parking. For example, during hot weather, some cars occupy two spaces for one car in order to park under a shade tree, thus limiting parking for everyone. This often forces another person to park inappropriately across or too close to a resident's driveway.
- The volume of on-street parking also impacts access for residents' weekly bins pickup, forcing residents to place bins on the actual street bitumen to ensure cars do not restrict access by waste management trucks.
- Discussion among neighbours indicate that this has been an issue for many years, and evidenced in the previous 2019 development application submissions
- At present there are only 6 designated car parks on the sites.
- It is noted that documents from URPS (18 October 2019) in support of the 2019 St Louis application states that
- "... the existing land has an existing imbedded shortfall of 8 on-site car parking spaces".
- This 2022 application increases on-site parking by 11 – to a total of 16 – this is only just sufficient to meet the need of the 55 beds on the site based on this parking algorithm (pending approval).
- Although meeting planning requirements for parking, Foster Street residents consider the pressure on the street will remain – with residents still having to take the unreasonable pressure of staff and visitors.

***15. Foster St residents remain significantly concerned about on-street parking implications from this development application and wish this concern be noted by council for future deliberations regarding on-street parking and related traffic management issues.***



### **Proposed disability car parking**

- As part of the solution for the lack of parking, the proposed disability car park at the front of 23 Foster Street is a significant concern.
- Acknowledging that there is a need for such car parking, residents are concerned that its positioning at the front of 23 Foster Street will significantly impact on the character and feel of the street.
- In particular, using the site at No 23 will indicate that the house is not a residential house, but part of a commercial/ institutional facility.

***16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.***

## **LANDSCAPING AND OPEN SPACE**

- The Plan indicates that 'additional open spaces and landscaping' is included in the proposal for the addition 10 beds.
- Foster Street residents are concerned that the new development appears to be removing existing established trees and only leaving small shrubs. This will detract significantly from the amenity of the site and will create privacy concerns for the residents across the road from the development.

***17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.***

## **OTHER SIGNIFICANT MATTERS**

### **Future planning for Foster Street and maintenance of amenity**

- We understand that DAP only addresses development applications as they are received, and with a focus only on building design and on-site parking. This means that traffic and parking impacts on surrounding residents of such development applications are not addressed.
  - Residents of Foster Street are effectively "bookended " by residential support facilities – St Louis at the eastern end and a residential disability facility at the western end of the street.
  - Both these facilities, as they currently stand, have significant impact on parking and traffic. The issues relating to St Louis have been detailed above. Issues regarding the disability facility include similar and significant on-street parking and problems with access to Foster Street from Windsor Street as a result.
  - It is apparent that both facilities have plans for future expansion – St Louis has advised in its recent newsletter regarding its masterplan and the further (undetermined) development some time in the future of its additional available 5000sqm beyond the current application, and the disability service with its adjoining large spare block. The further development of these properties will significantly exacerbate what is already a major concern for traffic management and parking along the full length of Foster Street.
  - We advise that we will consult all residents on Foster Street as a matter of priority with a view to presenting a petition to Council about the major impacts on traffic and parking on the street with
-

shared recommendations for the way forward. It is recognised that the resulting actions will need to be considered within the Council's overall budget and forward capital works program.

- 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.***
  - 19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.***
- 

*[attach additional pages as needed]*

Note: In order for this submission to be valid, it must:

- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
  - Click here to enter text. *[list any accepted or deemed-to-satisfy elements of the development]*.

I: ☒ wish to be heard in support of my submission\*  
☐ do not wish to be heard in support of my submission

By: ☐ appearing personally  
☒ being represented by the following person

*\*You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*

Signature:

Date: 8 June 2022

Return Address:

or

Email:

or

Complete online submission:

[plan.sa.gov.au/have\\_your\\_say/notified\\_developments/current\\_notified\\_developments](http://plan.sa.gov.au/have_your_say/notified_developments/current_notified_developments)



## Representations

Representor 5 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	08/06/2022 12:14 PM
Submission Source	Over Counter
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I support the development with some concerns
<b>Reasons</b> see submission	

## Attached Documents

ScivicGrou22060811370-3048599.pdf

## REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

*Planning, Development and Infrastructure Act 2016*

**Applicant:** St Louis Aged Care C/- Future Urban

**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

**Zone/Sub-zone/Overlay:** Established Neighbourhood Zone

**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

My name\*:

1. [REDACTED]

for and on behalf of

2. [REDACTED]  
3. [REDACTED]  
4. [REDACTED]  
5. [REDACTED]  
6. [REDACTED]  
7. [REDACTED]  
8. [REDACTED]  
9. [REDACTED]  
10. [REDACTED]  
11. [REDACTED]  
12. [REDACTED]

My phone number: [REDACTED]

My postal address\*: [REDACTED]

My email: [REDACTED]

*\* Indicates mandatory information*

My position is: ☐ I support the development  
☒ I support the development with some major concerns, many seeking conditional requirements for development application approval.

- 8 JUN 2022



Government of South Australia  
Attorney-General's Department

## **Executive Summary**

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

## **Summary of Foster Street Resident Recommendations**

- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**
- 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
- 3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.**
- 4. Foster Street residents request St Louis:**
  - a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;**
  - b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;**
  - c. ensure no noise occurs at the development site before 7am, 7 days a week.**
- 5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.**
- 6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the existing facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.**
- 7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.**
- 8. Foster Street residents request that:**
  - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and**



- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for the new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.**
  - 9. Given that on-site space is proposed to be available, Foster Street residents therefore request that Council enforce all trucks/vehicle deliveries including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.**
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  - 12. Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development**
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  - 16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.**
  - 17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.**
  - 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.**
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establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.

The major concerns are detailed below and seek a range of key conditions and actions before such development is approved by Council.

---

## **LAND USE**

### **Future Land Use**

- The planning documents refer to the provision of residential aged care as being suitable for the Established Neighbourhood Zone and that such use is compatible with the zone.
  - This is accepted at the size and scale of the current 45 bedroom facility.
  - However, it is noted that in a recent 2021 newsletter St Louis stated
    - *[St Louis] .."will be able to substantially expand St Louis nursing home located at 21 Foster Street Parkside. St Louis owns 5,000 square metres of land adjacent the nursing home [ie houses at 23,25,& 27 Foster Street on the western side], obtained over a number of years for precisely this purpose."*
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  - It is noted that the houses west of the facility have not been kept in good repair, their gardens are significantly run down when compared to the rest of the street and the overall street scape is detrimental and impacts on residents' house values as a result.
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- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**

### **Monthly Progress Meetings During Construction Phase and Preliminary Design Consultation**

- It is understood that St Louis have consulted widely with the Nursing Home residents and families regarding the design and build of the proposed new development.
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  - It is also acknowledged that the construction phase of the development, if approved, will impact significantly on residents of Foster Street and surrounds. An appropriate communication process over this construction phase is sought as a priority.
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#### **Period of Building and capital development related to this current development application**

- Based on the 2019-20 development work, and should this development application progress, Foster Street residents are concerned about the additional noise, trucks, inappropriate and congested parking and start times, which from previous experience often commenced well before 7am.
- During the previous build in 2019-20 there was no formal process to raise concerns with the builder or St Louis management with sufficient delegation and authority.

#### **4. Foster Street residents request St Louis:**

- a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;*
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#### **5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.**

### **BUILT FORM AND DESIGN**

- This development will add an additional 10 residential aged care beds to the eastern side of the property – taking the 'nursing home' as St Louis refers to the facility, up to 55 beds.
  - It is noted that the Royal Commission into Aged Care recommended that future aged care services have a small home approach.
  - It is recognised that the new addition, by keeping the required façade of the existing building will go some way to meeting that objective.
  - Our concern however is that the fencing and plantings around the new accommodation should provide a different and varied look from the existing fencing and plantings in the current 45 bed facility.
  - It is noted that the plan (DAO5) advises the fencing will match the existing St Louis fencing.
  - We are concerned that this will create an increased 'institutional' look to the new accommodation by having almost uniform fencing along the whole length of the existing aged care facility. This contrasts with the rest of Foster Street where fencing varies between each neighbour, therefore creating variety and interest along the street.
  - We feel it prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment.
- 6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the current facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.**
- 7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.**



### Privacy

- Residents of 20, 22 and 24 Foster Street, as the most immediately impacted residences, are concerned that the new development, particularly with the location of its proposed pergola for meetings and sitting outside will directly overlook their homes.
- 8. Foster Street residents request that:**
  - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and**
  - b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for this new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.**

## INTERFACE BETWEEN LAND USES

- The planning document states that consideration has been made to ensure the minimisation of any adverse impacts on the amenity of sensitive receivers.
- The Plan states that noise will be minimised by
  - a. constructing a 2.4 metre acoustic fence (understood to be a Wallmark construction with foam filler) on the boundary of 23 Foster Street adjacent to the new car park.
  - b. restricting deliveries to the site between the hours of 7am and 10pm
  - c. turn off refrigeration before entering the site; and
  - d. locating mechanical plant away from the residential boundaries.

### Delivery Vehicles

- At present residents of Foster Street are confronted with daily large commercial vehicles parked on the street with refrigeration noise sometimes for up to 20 minutes at very early hours of the morning, and other vehicles during the day. Early morning times in particular has often been against regulations and has been raised by various neighbours over many years.
- Of note, neighbours advise there continues to be a delivery that occurs regularly between 3-4am. By example, on the morning of 1 June 2022, the delivery vehicle pulled up at 3.15am. The vehicle has its engine off and glides to its delivery point which is close to 30 Foster Street. It then moves off as quietly as possible but still wakes people on the street. This is a clear breach of regulations.
- It is acknowledged that the new plans include dedicated on-site space so that commercial vehicles can unload within St Louis grounds which is not possible currently. The development application advises that this will reduce the volume of trucks/vehicles using the street for deliveries. Truck parking and unloading on the road restricts traffic flow and compromises street safety.
- The emptying of the large industrial site rubbish bin has to be carried out on the street as the truck cannot manage the activity fully on site. This blocks the street for a period of time or limits traffic to a narrow passageway.

9. ***Given that on-site space is proposed to be available, Foster Street Residents therefore request that Council enforce all trucks/vehicle deliveries, including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.***
10. ***Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.***
- It is acknowledged that on-site deliveries may cause noise problems for some residents of Blyth Street.
11. ***Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.***

#### **Air Conditioning and Plant Noise**

- At present the large air conditioner plant on the St Louis premises can be heard as a constant hum for houses on the other side of the street.
  - The noise consultant contracted by St Louis, has indicated that the placement of individual air conditioners for each new bedroom of the new development will address any noise concerns and that there will be no worsening of the existing noise.
12. ***Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development.***

#### **Light spill**

- Some street residents have been affected by light overspill, due to the installation of lighting presumably installed for safety reasons. Recent prompt intervention by the site manager minimised neighbourhood overspill from a poorly directed new LED replacement installation that pointed light at bedrooms. There is now an opportunity to review all current and proposed lighting for neighbourhood overspill. Luminaires that spill light in all directions are wasteful and compromise neighbourhood night time amenity and sleeping patterns. Most inappropriate lighting installations can be remedied by carefully considering what needs to be illuminated and installing energy efficient, properly shielded light fixtures for the purpose.
13. ***Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.***



## **TRAFFIC MANAGEMENT**

- The Plan states that the proposed addition does not result in unreasonable traffic implications, and further states that the new development will, in fact, assist in improving current access.

### **Delivery Vehicles**

- This issue is addressed above –

***14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.***

### **On-Street Parking**

- Parking on Foster Street is already significantly impacted by the St Louis staff and visitors, and made worse by the constant flow of commercial vehicles loading and unloading on Foster Street.
- Access and egress from our houses is often impacted by cars parked over our driveways. This is a serious safety concern when staff and visitors park too close to driveways compromising vision, turning and collision potential.
- There is little respect for Foster Street residents by poor or selfish parking. For example, during hot weather, some cars occupy two spaces for one car in order to park under a shade tree, thus limiting parking for everyone. This often forces another person to park inappropriately across or too close to a resident's driveway.
- The volume of on-street parking also impacts access for residents' weekly bins pickup, forcing residents to place bins on the actual street bitumen to ensure cars do not restrict access by waste management trucks.
- Discussion among neighbours indicate that this has been an issue for many years, and evidenced in the previous 2019 development application submissions
- At present there are only 6 designated car parks on the sites.
- It is noted that documents from URPS (18 October 2019) in support of the 2019 St Louis application states that
- "... the existing land has an existing imbedded shortfall of 8 on-site car parking spaces".
- This 2022 application increases on-site parking by 11 – to a total of 16 – this is only just sufficient to meet the need of the 55 beds on the site based on this parking algorithm (pending approval).
- Although meeting planning requirements for parking, Foster Street residents consider the pressure on the street will remain – with residents still having to take the unreasonable pressure of staff and visitors.

***15. Foster St residents remain significantly concerned about on-street parking implications from this development application and wish this concern be noted by council for future deliberations regarding on-street parking and related traffic management issues.***



### **Proposed disability car parking**

- As part of the solution for the lack of parking, the proposed disability car park at the front of 23 Foster Street is a significant concern.
- Acknowledging that there is a need for such car parking, residents are concerned that its positioning at the front of 23 Foster Street will significantly impact on the character and feel of the street.
- In particular, using the site at No 23 will indicate that the house is not a residential house, but part of a commercial/ institutional facility.

***16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.***

## **LANDSCAPING AND OPEN SPACE**

- The Plan indicates that ‘additional open spaces and landscaping’ is included in the proposal for the addition 10 beds.
- Foster Street residents are concerned that the new development appears to be removing existing established trees and only leaving small shrubs. This will detract significantly from the amenity of the site and will create privacy concerns for the residents across the road from the development.

***17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.***

## **OTHER SIGNIFICANT MATTERS**

### **Future planning for Foster Street and maintenance of amenity**

- We understand that DAP only addresses development applications as they are received, and with a focus only on building design and on-site parking. This means that traffic and parking impacts on surrounding residents of such development applications are not addressed.
- Residents of Foster Street are effectively “bookended” by residential support facilities – St Louis at the eastern end and a residential disability facility at the western end of the street.
- Both these facilities, as they currently stand, have significant impact on parking and traffic. The issues relating to St Louis have been detailed above. Issues regarding the disability facility include similar and significant on-street parking and problems with access to Foster Street from Windsor Street as a result.
- It is apparent that both facilities have plans for future expansion – St Louis has advised in its recent newsletter regarding its masterplan and the further (undetermined) development some time in the future of its additional available 5000sqm beyond the current application, and the disability service with its adjoining large spare block. The further development of these properties will significantly exacerbate what is already a major concern for traffic management and parking along the full length of Foster Street.
- We advise that we will consult all residents on Foster Street as a matter of priority with a view to presenting a petition to Council about the major impacts on traffic and parking on the street with

shared recommendations for the way forward. It is recognised that the resulting actions will need to be considered within the Council's overall budget and forward capital works program.

- 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.***
  - 19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.***
- 

*[attach additional pages as needed]*

Note: In order for this submission to be valid, it must:

- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
  - Click here to enter text. *[list any accepted or deemed-to-satisfy elements of the development]*.

I: ☒ wish to be heard in support of my submission\*  
☐ do not wish to be heard in support of my submission

By: ☒ appearing personally  
☐ being represented by the following person: .....

*\*You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*



Complete online submission:

[plan.sa.gov.au/have\\_your\\_say/notified\\_developments/current\\_notified\\_developments](http://plan.sa.gov.au/have_your_say/notified_developments/current_notified_developments)



## Representations

Representor 6 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	08/06/2022 12:20 PM
Submission Source	Over Counter
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
<b>Reasons</b> see submission	

## Attached Documents

ScivicGrou22060811380-3048709.pdf

## REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

*Planning, Development and Infrastructure Act 2016*

**Applicant:** St Louis Aged Care C/- Future Urban

**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

**Zone/Sub-zone/Overlay:** Established Neighbourhood Zone

**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

My name\*:

My phone number:

My postal address\*:

My email:

*\* Indicates mandatory information*

My position is:

☐

I support the development

☒

I support the development with some **major** concerns, many seeking conditional requirements for development application approval.

☐

I oppose the development

## **Executive Summary**

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

## **Summary of Foster Street Resident Recommendations**

- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**
  - 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
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- At present the large air conditioner plant on the St Louis premises can be heard as a constant hum for houses on the other side of the street.
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- Some street residents have been affected by light overspill, due to the installation of lighting presumably installed for safety reasons. Recent prompt intervention by the site manager minimised neighbourhood overspill from a poorly directed new LED replacement installation that pointed light at bedrooms. There is now an opportunity to review all current and proposed lighting for neighbourhood overspill. Luminaires that spill light in all directions are wasteful and compromise neighbourhood night time amenity and sleeping patterns. Most inappropriate lighting installations can be remedied by carefully considering what needs to be illuminated and installing energy efficient, properly shielded light fixtures for the purpose.
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## **TRAFFIC MANAGEMENT**

- The Plan states that the proposed addition does not result in unreasonable traffic implications, and further states that the new development will, in fact, assist in improving current access.

### **Delivery Vehicles**

- This issue is addressed above –

***14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.***

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- Parking on Foster Street is already significantly impacted by the St Louis staff and visitors, and made worse by the constant flow of commercial vehicles loading and unloading on Foster Street.
- Access and egress from our houses is often impacted by cars parked over our driveways. This is a serious safety concern when staff and visitors park too close to driveways compromising vision, turning and collision potential.
- There is little respect for Foster Street residents by poor or selfish parking. For example, during hot weather, some cars occupy two spaces for one car in order to park under a shade tree, thus limiting parking for everyone. This often forces another person to park inappropriately across or too close to a resident's driveway.
- The volume of on-street parking also impacts access for residents' weekly bins pickup, forcing residents to place bins on the actual street bitumen to ensure cars do not restrict access by waste management trucks.
- Discussion among neighbours indicate that this has been an issue for many years, and evidenced in the previous 2019 development application submissions
- At present there are only 6 designated car parks on the sites.
- It is noted that documents from URPS (18 October 2019) in support of the 2019 St Louis application states that
- "... the existing land has an existing imbedded shortfall of 8 on-site car parking spaces".
- This 2022 application increases on-site parking by 11 – to a total of 16 – this is only just sufficient to meet the need of the 55 beds on the site based on this parking algorithm (pending approval).
- Although meeting planning requirements for parking, Foster Street residents consider the pressure on the street will remain – with residents still having to take the unreasonable pressure of staff and visitors.

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### Proposed disability car parking

- As part of the solution for the lack of parking, the proposed disability car park at the front of 23 Foster Street is a significant concern.
- Acknowledging that there is a need for such car parking, residents are concerned that its positioning at the front of 23 Foster Street will significantly impact on the character and feel of the street.
- In particular, using the site at No 23 will indicate that the house is not a residential house, but part of a commercial/ institutional facility.

**16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.**

## LANDSCAPING AND OPEN SPACE

- The Plan indicates that 'additional open spaces and landscaping' is included in the proposal for the addition 10 beds.
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**17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.**

## OTHER SIGNIFICANT MATTERS

### Future planning for Foster Street and maintenance of amenity

- We understand that DAP only addresses development applications as they are received, and with a focus only on building design and on-site parking. This means that traffic and parking impacts on surrounding residents of such development applications are not addressed.
- Residents of Foster Street are effectively "bookended " by residential support facilities – St Louis at the eastern end and a residential disability facility at the western end of the street.
- Both these facilities, as they currently stand, have significant impact on parking and traffic. The issues relating to St Louis have been detailed above. Issues regarding the disability facility include similar and significant on-street parking and problems with access to Foster Street from Windsor Street as a result.
- It is apparent that both facilities have plans for future expansion – St Louis has advised in its recent newsletter regarding its masterplan and the further (undetermined) development some time in the future of its additional available 5000sqm beyond the current application, and the disability service with its adjoining large spare block. The further development of these properties will significantly exacerbate what is already a major concern for traffic management and parking along the full length of Foster Street.
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- 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.***
  - 19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.***
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*[attach additional pages as needed]*

Note: In order for this submission to be valid, it must:

- be in writing; and
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- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
  - Click here to enter text. *[list any accepted or deemed-to-satisfy elements of the development]*.

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☒ do not wish to be heard in support of my submission

By: ☐ appearing personally  
☒ being represented by the following person:

*\*You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*

Signature:

Date: 8 June 2022

Return Address:

or

Email:

or

Complete online submission:

[plan.sa.gov.au/have\\_your\\_say/notified\\_developments/current\\_notified\\_developments](http://plan.sa.gov.au/have_your_say/notified_developments/current_notified_developments)



## Representations

Representor 7 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	08/06/2022 12:23 PM
Submission Source	Over Counter
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I support the development with some concerns
<b>Reasons</b> see submission	

## Attached Documents

ScivicGrou22060811381-3048763.pdf

## REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

*Planning, Development and Infrastructure Act 2016*

**Applicant:** St Louis Aged Care C/- Future Urban

**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

**Zone/Sub-zone/Overlay:** Established Neighbourhood Zone

**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

My name\*: [REDACTED]

My phone number: [REDACTED]

My postal address\*: [REDACTED]

My email: [REDACTED]

*\* Indicates mandatory information*

My position is: ☐ I support the development

☒ I support the development with some **major** concerns, many seeking conditional requirements for development application approval.

☐ I oppose the development

## Executive Summary

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

## Summary of Foster Street Resident Recommendations

1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.
  2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.
  3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.
  4. Foster Street residents request St Louis:
    - a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;
    - b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;
    - c. ensure no noise occurs at the development site before 7am, 7 days a week.
  5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.
  6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the existing facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.
  7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.
  8. Foster Street residents request that:
    - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and
-



- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for the new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.
9. Given that on-site space is proposed to be available, Foster Street residents therefore request that Council enforce all trucks/vehicle deliveries including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.
10. Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.
11. Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.
12. Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development
13. Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.
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15. Foster Street residents remain significantly concerned about on-street parking implications from this development application and wish this concern noted by council for future deliberations regarding on-street parking and related traffic management issues.
16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.
17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.
18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.
19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to
-

establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.

The major concerns are detailed below and seek a range of key conditions and actions before such development is approved by Council.

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## **LAND USE**

### **Future Land Use**

- The planning documents refer to the provision of residential aged care as being suitable for the Established Neighbourhood Zone and that such use is compatible with the zone.
  - This is accepted at the size and scale of the current 45 bedroom facility.
  - However, it is noted that in a recent 2021 newsletter St Louis stated
    - *[St Louis] .."will be able to substantially expand St Louis nursing home located at 21 Foster Street Parkside. St Louis owns 5,000 square metres of land adjacent the nursing home [ie houses at 23,25,& 27 Foster Street on the western side], obtained over a number of years for precisely this purpose."*
  - Such a development would not be of the scale or form that the current service is.
  - While it is understood that the Development Assessment Panel (DAP) must limit its consideration to the application before them, residents are seeking transparency from St Louis of their intentions with respect to these houses.
  - It is noted that the houses west of the facility have not been kept in good repair, their gardens are significantly run down when compared to the rest of the street and the overall street scape is detrimental and impacts on residents' house values as a result.
  - Lack of clarity for the future of the houses on the western side causes significant concern for Foster Street residents.
- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**

### **Monthly Progress Meetings During Construction Phase and Preliminary Design Consultation**

- It is understood that St Louis have consulted widely with the Nursing Home residents and families regarding the design and build of the proposed new development.
  - No such consultation has been afforded to Foster Street residents even though there will be significant long term and short-term impacts on the street from this development.
  - It is also acknowledged that the construction phase of the development, if approved, will impact significantly on residents of Foster Street and surrounds. An appropriate communication process over this construction phase is sought as a priority.
- 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
- 3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.**
-



#### **Period of Building and capital development related to this current development application**

- Based on the 2019-20 development work, and should this development application progress, Foster Street residents are concerned about the additional noise, trucks, inappropriate and congested parking and start times, which from previous experience often commenced well before 7am.
- During the previous build in 2019-20 there was no formal process to raise concerns with the builder or St Louis management with sufficient delegation and authority.

#### **4. Foster Street residents request St Louis:**

- a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;*
- b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;*
- c. ensure no noise occurs at the development site before 7am, 7 days a week.*

#### **5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.**

### **BUILT FORM AND DESIGN**

- This development will add an additional 10 residential aged care beds to the eastern side of the property – taking the 'nursing home' as St Louis refers to the facility, up to 55 beds.
  - It is noted that the Royal Commission into Aged Care recommended that future aged care services have a small home approach.
  - It is recognised that the new addition, by keeping the required façade of the existing building will go some way to meeting that objective.
  - Our concern however is that the fencing and plantings around the new accommodation should provide a different and varied look from the existing fencing and plantings in the current 45 bed facility.
  - It is noted that the plan (DAO5) advises the fencing will match the existing St Louis fencing.
  - We are concerned that this will create an increased 'institutional' look to the new accommodation by having almost uniform fencing along the whole length of the existing aged care facility. This contrasts with the rest of Foster Street where fencing varies between each neighbour, therefore creating variety and interest along the street.
  - We feel it prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment.
- 6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the current facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.**
- 7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.**



### Privacy

- Residents of 20, 22 and 24 Foster Street, as the most immediately impacted residences, are concerned that the new development, particularly with the location of its proposed pergola for meetings and sitting outside will directly overlook their homes.
- 8. ***Foster Street residents request that:***
  - a. ***fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and***
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## INTERFACE BETWEEN LAND USES

- The planning document states that consideration has been made to ensure the minimisation of any adverse impacts on the amenity of sensitive receivers.
- The Plan states that noise will be minimised by
  - a. constructing a 2.4 metre acoustic fence (understood to be a Wallmark construction with foam filler) on the boundary of 23 Foster Street adjacent to the new car park.
  - b. restricting deliveries to the site between the hours of 7am and 10pm
  - c. turn off refrigeration before entering the site; and
  - d. locating mechanical plant away from the residential boundaries.

### Delivery Vehicles

- At present residents of Foster Street are confronted with daily large commercial vehicles parked on the street with refrigeration noise sometimes for up to 20 minutes at very early hours of the morning, and other vehicles during the day. Early morning times in particular has often been against regulations and has been raised by various neighbours over many years.
  - Of note, neighbours advise there continues to be a delivery that occurs regularly between 3-4am. By example, on the morning of 1 June 2022, the delivery vehicle pulled up at 3.15am. The vehicle has its engine off and glides to its delivery point which is close to 30 Foster Street. It then moves off as quietly as possible but still wakes people on the street. This is a clear breach of regulations.
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By: ☐ appearing personally  
☒ being represented by the following person: .

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Signature

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Return Address:

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Complete online submission:

[plan.sa.gov.au/have\\_your\\_say/notified\\_developments/current\\_notified\\_developments](http://plan.sa.gov.au/have_your_say/notified_developments/current_notified_developments)



## Representations

Representor 8 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	
Email Address	
Submission Date	08/06/2022 12:27 PM
Submission Source	Over Counter
Late Submission	No
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*Planning, Development and Infrastructure Act 2016*

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**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

**Zone/Sub-zone/Overlay:** Established Neighbourhood Zone

**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

My name\*:

My phone number:

My postal address\*:

My email:

*\* Indicates mandatory information*

My position is:

☐

I support the development

☒

I support the development with some **major** concerns, many seeking conditional requirements for development application approval.

☐

I oppose the development

## Executive Summary

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

## Summary of Foster Street Resident Recommendations

1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.
2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.
3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.
4. Foster Street residents request St Louis:
  - a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;
  - b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;
  - c. ensure no noise occurs at the development site before 7am, 7 days a week.
5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.
6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the existing facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.
7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.
8. Foster Street residents request that:
  - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and



establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.

**The major concerns are detailed below and seek a range of key conditions and actions before such development is approved by Council.**

---

## **LAND USE**

### **Future Land Use**

- The planning documents refer to the provision of residential aged care as being suitable for the Established Neighbourhood Zone and that such use is compatible with the zone.
  - This is accepted at the size and scale of the current 45 bedroom facility.
  - However, it is noted that in a recent 2021 newsletter St Louis stated
    - *[St Louis] .."will be able to substantially expand St Louis nursing home located at 21 Foster Street Parkside. St Louis owns 5,000 square metres of land adjacent the nursing home [ie houses at 23,25,& 27 Foster Street on the western side], obtained over a number of years for precisely this purpose."*
  - Such a development would not be of the scale or form that the current service is.
  - While it is understood that the Development Assessment Panel (DAP) must limit its consideration to the application before them, residents are seeking transparency from St Louis of their intentions with respect to these houses.
  - It is noted that the houses west of the facility have not been kept in good repair, their gardens are significantly run down when compared to the rest of the street and the overall street scape is detrimental and impacts on residents' house values as a result.
  - Lack of clarity for the future of the houses on the western side causes significant concern for Foster Street residents.
- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**

### **Monthly Progress Meetings During Construction Phase and Preliminary Design Consultation**

- It is understood that St Louis have consulted widely with the Nursing Home residents and families regarding the design and build of the proposed new development.
  - No such consultation has been afforded to Foster Street residents even though there will be significant long term and short-term impacts on the street from this development.
  - It is also acknowledged that the construction phase of the development, if approved, will impact significantly on residents of Foster Street and surrounds. An appropriate communication process over this construction phase is sought as a priority.
- 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
- 3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.**
-

### **Privacy**

- Residents of 20, 22 and 24 Foster Street, as the most immediately impacted residences, are concerned that the new development, particularly with the location of its proposed pergola for meetings and sitting outside will directly overlook their homes.
- 8. Foster Street residents request that:**
  - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and***
  - b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for this new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.***

## **INTERFACE BETWEEN LAND USES**

- The planning document states that consideration has been made to ensure the minimisation of any adverse impacts on the amenity of sensitive receivers.
- The Plan states that noise will be minimised by
  - a. constructing a 2.4 metre acoustic fence (understood to be a Wallmark construction with foam filler) on the boundary of 23 Foster Street adjacent to the new car park.
  - b. restricting deliveries to the site between the hours of 7am and 10pm
  - c. turn off refrigeration before entering the site; and
  - d. locating mechanical plant away from the residential boundaries.

### **Delivery Vehicles**

- At present residents of Foster Street are confronted with daily large commercial vehicles parked on the street with refrigeration noise sometimes for up to 20 minutes at very early hours of the morning, and other vehicles during the day. Early morning times in particular has often been against regulations and has been raised by various neighbours over many years.
- Of note, neighbours advise there continues to be a delivery that occurs regularly between 3-4am. By example, on the morning of 1 June 2022, the delivery vehicle pulled up at 3.15am. The vehicle has its engine off and glides to its delivery point which is close to 30 Foster Street. It then moves off as quietly as possible but still wakes people on the street. This is a clear breach of regulations.
- It is acknowledged that the new plans include dedicated on-site space so that commercial vehicles can unload within St Louis grounds which is not possible currently. The development application advises that this will reduce the volume of trucks/vehicles using the street for deliveries. Truck parking and unloading on the road restricts traffic flow and compromises street safety.
- The emptying of the large industrial site rubbish bin has to be carried out on the street as the truck cannot manage the activity fully on site. This blocks the street for a period of time or limits traffic to a narrow passageway.



## **TRAFFIC MANAGEMENT**

- The Plan states that the proposed addition does not result in unreasonable traffic implications, and further states that the new development will, in fact, assist in improving current access.

### **Delivery Vehicles**

- This issue is addressed above –

***14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.***

### **On-Street Parking**

- Parking on Foster Street is already significantly impacted by the St Louis staff and visitors, and made worse by the constant flow of commercial vehicles loading and unloading on Foster Street.
  - Access and egress from our houses is often impacted by cars parked over our driveways. This is a serious safety concern when staff and visitors park too close to driveways compromising vision, turning and collision potential.
  - There is little respect for Foster Street residents by poor or selfish parking. For example, during hot weather, some cars occupy two spaces for one car in order to park under a shade tree, thus limiting parking for everyone. This often forces another person to park inappropriately across or too close to a resident's driveway.
  - The volume of on-street parking also impacts access for residents' weekly bins pickup, forcing residents to place bins on the actual street bitumen to ensure cars do not restrict access by waste management trucks.
  - Discussion among neighbours indicate that this has been an issue for many years, and evidenced in the previous 2019 development application submissions
  - At present there are only 6 designated car parks on the sites.
  - It is noted that documents from URPS (18 October 2019) in support of the 2019 St Louis application states that
  - "... the existing land has an existing imbedded shortfall of 8 on-site car parking spaces".
  - This 2022 application increases on-site parking by 11 – to a total of 16 – this is only just sufficient to meet the need of the 55 beds on the site based on this parking algorithm (pending approval).
  - Although meeting planning requirements for parking, Foster Street residents consider the pressure on the street will remain – with residents still having to take the unreasonable pressure of staff and visitors.
- 15. Foster St residents remain significantly concerned about on-street parking implications from this development application and wish this concern be noted by council for future deliberations regarding on-street parking and related traffic management issues.***



shared recommendations for the way forward. It is recognised that the resulting actions will need to be considered within the Council's overall budget and forward capital works program.

- 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.***
  - 19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.***
- 

*[attach additional pages as needed]*

Note: In order for this submission to be valid, it must:

- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
  - Click here to enter text. *[list any accepted or deemed-to-satisfy elements of the development]*.

I: ☐ wish to be heard in support of my submission\*  
☒ do not wish to be heard in support of my submission

By: ☐ appearing personally  
☒ being represented by the following person:

*\*You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*

Signature:

Date: 8 June 2022

Return Address:

or

Email:

or

Complete online submission:

[plan.sa.gov.au/have\\_your\\_say/notified\\_developments/current\\_notified\\_developments](http://plan.sa.gov.au/have_your_say/notified_developments/current_notified_developments)

## Representations

Representor 9 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	08/06/2022 12:30 PM
Submission Source	Over Counter
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
<b>Reasons</b> see submission	

## Attached Documents

ScivicGrou22060811390-3048874.pdf



## REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

*Planning, Development and Infrastructure Act 2016*

**Applicant:** St Louis Aged Care C/- Future Urban

**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

**Zone/Sub-zone/Overlay:** Established Neighbourhood Zone

**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

My name\*:

My phone number:

My postal address\*:

My email:

*\* Indicates mandatory information*

My position is: ☐ I support the development

☒ I support the development with some **major** concerns, many seeking conditional requirements for development application approval.

☐ I oppose the development

## **Executive Summary**

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

## **Summary of Foster Street Resident Recommendations**

- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**
  - 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
  - 3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.**
  - 4. Foster Street residents request St Louis:**
    - a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;**
    - b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;**
    - c. ensure no noise occurs at the development site before 7am, 7 days a week.**
  - 5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.**
  - 6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the existing facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.**
  - 7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.**
  - 8. Foster Street residents request that:**
    - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and**
-

- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for the new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.
9. Given that on-site space is proposed to be available, Foster Street residents therefore request that Council enforce all trucks/vehicle deliveries including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.
10. Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.
11. Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.
12. Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development
13. Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.
14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.
15. Foster Street residents remain significantly concerned about on-street parking implications from this development application and wish this concern noted by council for future deliberations regarding on-street parking and related traffic management issues.
16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.
17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.
18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.
19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to



establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.

**The major concerns are detailed below and seek a range of key conditions and actions before such development is approved by Council.**

---

## **LAND USE**

### **Future Land Use**

- The planning documents refer to the provision of residential aged care as being suitable for the Established Neighbourhood Zone and that such use is compatible with the zone.
  - This is accepted at the size and scale of the current 45 bedroom facility.
  - However, it is noted that in a recent 2021 newsletter St Louis stated
    - *[St Louis] .."will be able to substantially expand St Louis nursing home located at 21 Foster Street Parkside. St Louis owns 5,000 square metres of land adjacent the nursing home [ie houses at 23,25,& 27 Foster Street on the western side], obtained over a number of years for precisely this purpose."*
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  - While it is understood that the Development Assessment Panel (DAP) must limit its consideration to the application before them, residents are seeking transparency from St Louis of their intentions with respect to these houses.
  - It is noted that the houses west of the facility have not been kept in good repair, their gardens are significantly run down when compared to the rest of the street and the overall street scape is detrimental and impacts on residents' house values as a result.
  - Lack of clarity for the future of the houses on the western side causes significant concern for Foster Street residents.
- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**

### **Monthly Progress Meetings During Construction Phase and Preliminary Design Consultation**

- It is understood that St Louis have consulted widely with the Nursing Home residents and families regarding the design and build of the proposed new development.
  - No such consultation has been afforded to Foster Street residents even though there will be significant long term and short-term impacts on the street from this development.
  - It is also acknowledged that the construction phase of the development, if approved, will impact significantly on residents of Foster Street and surrounds. An appropriate communication process over this construction phase is sought as a priority.
- 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
- 3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.**
-

#### **Period of Building and capital development related to this current development application**

- Based on the 2019-20 development work, and should this development application progress, Foster Street residents are concerned about the additional noise, trucks, inappropriate and congested parking and start times, which from previous experience often commenced well before 7am.
- During the previous build in 2019-20 there was no formal process to raise concerns with the builder or St Louis management with sufficient delegation and authority.

#### **4. Foster Street residents request St Louis:**

- a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;*
- b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;*
- c. ensure no noise occurs at the development site before 7am, 7 days a week.*

#### **5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.**

### **BUILT FORM AND DESIGN**

- This development will add an additional 10 residential aged care beds to the eastern side of the property – taking the 'nursing home' as St Louis refers to the facility, up to 55 beds.
  - It is noted that the Royal Commission into Aged Care recommended that future aged care services have a small home approach.
  - It is recognised that the new addition, by keeping the required façade of the existing building will go some way to meeting that objective.
  - Our concern however is that the fencing and plantings around the new accommodation should provide a different and varied look from the existing fencing and plantings in the current 45 bed facility.
  - It is noted that the plan (DAO5) advises the fencing will match the existing St Louis fencing.
  - We are concerned that this will create an increased 'institutional' look to the new accommodation by having almost uniform fencing along the whole length of the existing aged care facility. This contrasts with the rest of Foster Street where fencing varies between each neighbour, therefore creating variety and interest along the street.
  - We feel it prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment.
- 6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the current facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.**
- 7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.**
-



### **Privacy**

- Residents of 20, 22 and 24 Foster Street, as the most immediately impacted residences, are concerned that the new development, particularly with the location of its proposed pergola for meetings and sitting outside will directly overlook their homes.
- 8. Foster Street residents request that:**
  - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and**
  - b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for this new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.**

### **INTERFACE BETWEEN LAND USES**

- The planning document states that consideration has been made to ensure the minimisation of any adverse impacts on the amenity of sensitive receivers.
- The Plan states that noise will be minimised by
  - a. constructing a 2.4 metre acoustic fence (understood to be a Wallmark construction with foam filler) on the boundary of 23 Foster Street adjacent to the new car park.
  - b. restricting deliveries to the site between the hours of 7am and 10pm
  - c. turn off refrigeration before entering the site; and
  - d. locating mechanical plant away from the residential boundaries.

### **Delivery Vehicles**

- At present residents of Foster Street are confronted with daily large commercial vehicles parked on the street with refrigeration noise sometimes for up to 20 minutes at very early hours of the morning, and other vehicles during the day. Early morning times in particular has often been against regulations and has been raised by various neighbours over many years.
- Of note, neighbours advise there continues to be a delivery that occurs regularly between 3-4am. By example, on the morning of 1 June 2022, the delivery vehicle pulled up at 3.15am. The vehicle has its engine off and glides to its delivery point which is close to 30 Foster Street. It then moves off as quietly as possible but still wakes people on the street. This is a clear breach of regulations.
- It is acknowledged that the new plans include dedicated on-site space so that commercial vehicles can unload within St Louis grounds which is not possible currently. The development application advises that this will reduce the volume of trucks/vehicles using the street for deliveries. Truck parking and unloading on the road restricts traffic flow and compromises street safety.
- The emptying of the large industrial site rubbish bin has to be carried out on the street as the truck cannot manage the activity fully on site. This blocks the street for a period of time or limits traffic to a narrow passageway.



9. ***Given that on-site space is proposed to be available, Foster Street Residents therefore request that Council enforce all trucks/vehicle deliveries, including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.***
10. ***Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.***
- It is acknowledged that on-site deliveries may cause noise problems for some residents of Blyth Street.
11. ***Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.***

#### **Air Conditioning and Plant Noise**

- At present the large air conditioner plant on the St Louis premises can be heard as a constant hum for houses on the other side of the street.
  - The noise consultant contracted by St Louis, has indicated that the placement of individual air conditioners for each new bedroom of the new development will address any noise concerns and that there will be no worsening of the existing noise.
12. ***Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development.***

#### **Light spill**

- Some street residents have been affected by light overspill, due to the installation of lighting presumably installed for safety reasons. Recent prompt intervention by the site manager minimised neighbourhood overspill from a poorly directed new LED replacement installation that pointed light at bedrooms. There is now an opportunity to review all current and proposed lighting for neighbourhood overspill. Luminaires that spill light in all directions are wasteful and compromise neighbourhood night time amenity and sleeping patterns. Most inappropriate lighting installations can be remedied by carefully considering what needs to be illuminated and installing energy efficient, properly shielded light fixtures for the purpose.
13. ***Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.***

## **TRAFFIC MANAGEMENT**

- The Plan states that the proposed addition does not result in unreasonable traffic implications, and further states that the new development will, in fact, assist in improving current access.

### **Delivery Vehicles**

- This issue is addressed above –

***14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.***

### **On-Street Parking**

- Parking on Foster Street is already significantly impacted by the St Louis staff and visitors, and made worse by the constant flow of commercial vehicles loading and unloading on Foster Street.
  - Access and egress from our houses is often impacted by cars parked over our driveways. This is a serious safety concern when staff and visitors park too close to driveways compromising vision, turning and collision potential.
  - There is little respect for Foster Street residents by poor or selfish parking. For example, during hot weather, some cars occupy two spaces for one car in order to park under a shade tree, thus limiting parking for everyone. This often forces another person to park inappropriately across or too close to a resident's driveway.
  - The volume of on-street parking also impacts access for residents' weekly bins pickup, forcing residents to place bins on the actual street bitumen to ensure cars do not restrict access by waste management trucks.
  - Discussion among neighbours indicate that this has been an issue for many years, and evidenced in the previous 2019 development application submissions
  - At present there are only 6 designated car parks on the sites.
  - It is noted that documents from URPS (18 October 2019) in support of the 2019 St Louis application states that
  - "... the existing land has an existing imbedded shortfall of 8 on-site car parking spaces".
  - This 2022 application increases on-site parking by 11 – to a total of 16 – this is only just sufficient to meet the need of the 55 beds on the site based on this parking algorithm (pending approval).
  - Although meeting planning requirements for parking, Foster Street residents consider the pressure on the street will remain – with residents still having to take the unreasonable pressure of staff and visitors.
- 15. Foster St residents remain significantly concerned about on-street parking implications from this development application and wish this concern be noted by council for future deliberations regarding on-street parking and related traffic management issues.***



### Proposed disability car parking

- As part of the solution for the lack of parking, the proposed disability car park at the front of 23 Foster Street is a significant concern.
- Acknowledging that there is a need for such car parking, residents are concerned that its positioning at the front of 23 Foster Street will significantly impact on the character and feel of the street.
- In particular, using the site at No 23 will indicate that the house is not a residential house, but part of a commercial/ institutional facility.

***16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.***

## LANDSCAPING AND OPEN SPACE

- The Plan indicates that 'additional open spaces and landscaping' is included in the proposal for the addition 10 beds.
- Foster Street residents are concerned that the new development appears to be removing existing established trees and only leaving small shrubs. This will detract significantly from the amenity of the site and will create privacy concerns for the residents across the road from the development.

***17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.***

## OTHER SIGNIFICANT MATTERS

### Future planning for Foster Street and maintenance of amenity

- We understand that DAP only addresses development applications as they are received, and with a focus only on building design and on-site parking. This means that traffic and parking impacts on surrounding residents of such development applications are not addressed.
  - Residents of Foster Street are effectively "bookended " by residential support facilities – St Louis at the eastern end and a residential disability facility at the western end of the street.
  - Both these facilities, as they currently stand, have significant impact on parking and traffic. The issues relating to St Louis have been detailed above. Issues regarding the disability facility include similar and significant on-street parking and problems with access to Foster Street from Windsor Street as a result.
  - It is apparent that both facilities have plans for future expansion – St Louis has advised in its recent newsletter regarding its masterplan and the further (undetermined) development some time in the future of its additional available 5000sqm beyond the current application, and the disability service with its adjoining large spare block. The further development of these properties will significantly exacerbate what is already a major concern for traffic management and parking along the full length of Foster Street.
  - We advise that we will consult all residents on Foster Street as a matter of priority with a view to presenting a petition to Council about the major impacts on traffic and parking on the street with
-



shared recommendations for the way forward. It is recognised that the resulting actions will need to be considered within the Council's overall budget and forward capital works program.

- 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.***
  - 19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.***
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*[attach additional pages as needed]*

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- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
  - Click here to enter text. *[list any accepted or deemed-to-satisfy elements of the development]*.

I: ☐ wish to be heard in support of my submission\*  
☒ do not wish to be heard in support of my submission

By: ☐ appearing personally  
☒ being represented by the following person

*\*You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*

Signature:

Date: 8 June 2022

Return Address:

or

Email:

or

Complete online submission:

[plan.sa.gov.au/have\\_your\\_say/notified\\_developments/current\\_notified\\_developments](https://plan.sa.gov.au/have_your_say/notified_developments/current_notified_developments)

## Representations

Representor 10 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	08/06/2022 12:33 PM
Submission Source	Over Counter
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
<b>Reasons</b> see submission	

## Attached Documents

ScivicGrou22060811391-3048906.pdf



## REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

*Planning, Development and Infrastructure Act 2016*

**Applicant:** St Louis Aged Care C/- Future Urban

**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

**Zone/Sub-zone/Overlay:** Established Neighbourhood Zone

**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

My name\*:

My phone number:

My postal address\*:

My email:

*\* Indicates mandatory information*

My position is: ☐ I support the development

☒ I support the development with some **major** concerns, many seeking conditional requirements for development application approval.

☐ I oppose the development

## **Executive Summary**

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

## **Summary of Foster Street Resident Recommendations**

1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.
  2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.
  3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.
  4. Foster Street residents request St Louis:
    - a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;
    - b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;
    - c. ensure no noise occurs at the development site before 7am, 7 days a week.
  5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.
  6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the existing facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.
  7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.
  8. Foster Street residents request that:
    - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and
-

- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for the new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.
9. Given that on-site space is proposed to be available, Foster Street residents therefore request that Council enforce all trucks/vehicle deliveries including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.
10. Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.
11. Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.
12. Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development
13. Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.
14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.
15. Foster Street residents remain significantly concerned about on-street parking implications from this development application and wish this concern noted by council for future deliberations regarding on-street parking and related traffic management issues.
16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.
17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.
18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.
19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to
-



establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.

The major concerns are detailed below and seek a range of key conditions and actions before such development is approved by Council.

---

## **LAND USE**

### **Future Land Use**

- The planning documents refer to the provision of residential aged care as being suitable for the Established Neighbourhood Zone and that such use is compatible with the zone.
  - This is accepted at the size and scale of the current 45 bedroom facility.
  - However, it is noted that in a recent 2021 newsletter St Louis stated
    - *[St Louis] .."will be able to substantially expand St Louis nursing home located at 21 Foster Street Parkside. St Louis owns 5,000 square metres of land adjacent the nursing home [ie houses at 23,25,& 27 Foster Street on the western side], obtained over a number of years for precisely this purpose."*
  - Such a development would not be of the scale or form that the current service is.
  - While it is understood that the Development Assessment Panel (DAP) must limit its consideration to the application before them, residents are seeking transparency from St Louis of their intentions with respect to these houses.
  - It is noted that the houses west of the facility have not been kept in good repair, their gardens are significantly run down when compared to the rest of the street and the overall street scape is detrimental and impacts on residents' house values as a result.
  - Lack of clarity for the future of the houses on the western side causes significant concern for Foster Street residents.
- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**

### **Monthly Progress Meetings During Construction Phase and Preliminary Design Consultation**

- It is understood that St Louis have consulted widely with the Nursing Home residents and families regarding the design and build of the proposed new development.
  - No such consultation has been afforded to Foster Street residents even though there will be significant long term and short-term impacts on the street from this development.
  - It is also acknowledged that the construction phase of the development, if approved, will impact significantly on residents of Foster Street and surrounds. An appropriate communication process over this construction phase is sought as a priority.
- 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
- 3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.**
-

#### **Period of Building and capital development related to this current development application**

- Based on the 2019-20 development work, and should this development application progress, Foster Street residents are concerned about the additional noise, trucks, inappropriate and congested parking and start times, which from previous experience often commenced well before 7am.
- During the previous build in 2019-20 there was no formal process to raise concerns with the builder or St Louis management with sufficient delegation and authority.

#### **4. Foster Street residents request St Louis:**

- a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;*
- b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;*
- c. ensure no noise occurs at the development site before 7am, 7 days a week.*

#### **5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.**

### **BUILT FORM AND DESIGN**

- This development will add an additional 10 residential aged care beds to the eastern side of the property – taking the 'nursing home' as St Louis refers to the facility, up to 55 beds.
  - It is noted that the Royal Commission into Aged Care recommended that future aged care services have a small home approach.
  - It is recognised that the new addition, by keeping the required façade of the existing building will go some way to meeting that objective.
  - Our concern however is that the fencing and plantings around the new accommodation should provide a different and varied look from the existing fencing and plantings in the current 45 bed facility.
  - It is noted that the plan (DAO5) advises the fencing will match the existing St Louis fencing.
  - We are concerned that this will create an increased 'institutional' look to the new accommodation by having almost uniform fencing along the whole length of the existing aged care facility. This contrasts with the rest of Foster Street where fencing varies between each neighbour, therefore creating variety and interest along the street.
  - We feel it prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment.
- 6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the current facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.**
- 7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.**



### Privacy

- Residents of 20, 22 and 24 Foster Street, as the most immediately impacted residences, are concerned that the new development, particularly with the location of its proposed pergola for meetings and sitting outside will directly overlook their homes.

#### **8. Foster Street residents request that:**

- a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and*
- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for this new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.*

## **INTERFACE BETWEEN LAND USES**

- The planning document states that consideration has been made to ensure the minimisation of any adverse impacts on the amenity of sensitive receivers.
- The Plan states that noise will be minimised by
  - a. constructing a 2.4 metre acoustic fence (understood to be a Wallmark construction with foam filler) on the boundary of 23 Foster Street adjacent to the new car park.
  - b. restricting deliveries to the site between the hours of 7am and 10pm
  - c. turn off refrigeration before entering the site; and
  - d. locating mechanical plant away from the residential boundaries.

### Delivery Vehicles

- At present residents of Foster Street are confronted with daily large commercial vehicles parked on the street with refrigeration noise sometimes for up to 20 minutes at very early hours of the morning, and other vehicles during the day. Early morning times in particular has often been against regulations and has been raised by various neighbours over many years.
- Of note, neighbours advise there continues to be a delivery that occurs regularly between 3-4am. By example, on the morning of 1 June 2022, the delivery vehicle pulled up at 3.15am. The vehicle has its engine off and glides to its delivery point which is close to 30 Foster Street. It then moves off as quietly as possible but still wakes people on the street. This is a clear breach of regulations.
- It is acknowledged that the new plans include dedicated on-site space so that commercial vehicles can unload within St Louis grounds which is not possible currently. The development application advises that this will reduce the volume of trucks/vehicles using the street for deliveries. Truck parking and unloading on the road restricts traffic flow and compromises street safety.
- The emptying of the large industrial site rubbish bin has to be carried out on the street as the truck cannot manage the activity fully on site. This blocks the street for a period of time or limits traffic to a narrow passageway.



9. ***Given that on-site space is proposed to be available, Foster Street Residents therefore request that Council enforce all trucks/vehicle deliveries, including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.***
10. ***Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.***
- It is acknowledged that on-site deliveries may cause noise problems for some residents of Blyth Street.
11. ***Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.***

#### **Air Conditioning and Plant Noise**

- At present the large air conditioner plant on the St Louis premises can be heard as a constant hum for houses on the other side of the street.
  - The noise consultant contracted by St Louis, has indicated that the placement of individual air conditioners for each new bedroom of the new development will address any noise concerns and that there will be no worsening of the existing noise.
12. ***Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development.***

#### **Light spill**

- Some street residents have been affected by light overspill, due to the installation of lighting presumably installed for safety reasons. Recent prompt intervention by the site manager minimised neighbourhood overspill from a poorly directed new LED replacement installation that pointed light at bedrooms. There is now an opportunity to review all current and proposed lighting for neighbourhood overspill. Luminaires that spill light in all directions are wasteful and compromise neighbourhood night time amenity and sleeping patterns. Most inappropriate lighting installations can be remedied by carefully considering what needs to be illuminated and installing energy efficient, properly shielded light fixtures for the purpose.
13. ***Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.***

## **TRAFFIC MANAGEMENT**

- The Plan states that the proposed addition does not result in unreasonable traffic implications, and further states that the new development will, in fact, assist in improving current access.

### **Delivery Vehicles**

- This issue is addressed above –

***14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.***

### **On-Street Parking**

- Parking on Foster Street is already significantly impacted by the St Louis staff and visitors, and made worse by the constant flow of commercial vehicles loading and unloading on Foster Street.
  - Access and egress from our houses is often impacted by cars parked over our driveways. This is a serious safety concern when staff and visitors park too close to driveways compromising vision, turning and collision potential.
  - There is little respect for Foster Street residents by poor or selfish parking. For example, during hot weather, some cars occupy two spaces for one car in order to park under a shade tree, thus limiting parking for everyone. This often forces another person to park inappropriately across or too close to a resident's driveway.
  - The volume of on-street parking also impacts access for residents' weekly bins pickup, forcing residents to place bins on the actual street bitumen to ensure cars do not restrict access by waste management trucks.
  - Discussion among neighbours indicate that this has been an issue for many years, and evidenced in the previous 2019 development application submissions
  - At present there are only 6 designated car parks on the sites.
  - It is noted that documents from URPS (18 October 2019) in support of the 2019 St Louis application states that
  - "... the existing land has an existing imbedded shortfall of 8 on-site car parking spaces".
  - This 2022 application increases on-site parking by 11 – to a total of 16 – this is only just sufficient to meet the need of the 55 beds on the site based on this parking algorithm (pending approval).
  - Although meeting planning requirements for parking, Foster Street residents consider the pressure on the street will remain – with residents still having to take the unreasonable pressure of staff and visitors.
- 15. Foster St residents remain significantly concerned about on-street parking implications from this development application and wish this concern be noted by council for future deliberations regarding on-street parking and related traffic management issues.***



### **Proposed disability car parking**

- As part of the solution for the lack of parking, the proposed disability car park at the front of 23 Foster Street is a significant concern.
- Acknowledging that there is a need for such car parking, residents are concerned that its positioning at the front of 23 Foster Street will significantly impact on the character and feel of the street.
- In particular, using the site at No 23 will indicate that the house is not a residential house, but part of a commercial/ institutional facility.

***16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.***

## **LANDSCAPING AND OPEN SPACE**

- The Plan indicates that 'additional open spaces and landscaping' is included in the proposal for the addition 10 beds.
- Foster Street residents are concerned that the new development appears to be removing existing established trees and only leaving small shrubs. This will detract significantly from the amenity of the site and will create privacy concerns for the residents across the road from the development.

***17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.***

## **OTHER SIGNIFICANT MATTERS**

### **Future planning for Foster Street and maintenance of amenity**

- We understand that DAP only addresses development applications as they are received, and with a focus only on building design and on-site parking. This means that traffic and parking impacts on surrounding residents of such development applications are not addressed.
- Residents of Foster Street are effectively "bookended " by residential support facilities – St Louis at the eastern end and a residential disability facility at the western end of the street.
- Both these facilities, as they currently stand, have significant impact on parking and traffic. The issues relating to St Louis have been detailed above. Issues regarding the disability facility include similar and significant on-street parking and problems with access to Foster Street from Windsor Street as a result.
- It is apparent that both facilities have plans for future expansion – St Louis has advised in its recent newsletter regarding its masterplan and the further (undetermined) development some time in the future of its additional available 5000sqm beyond the current application, and the disability service with its adjoining large spare block. The further development of these properties will significantly exacerbate what is already a major concern for traffic management and parking along the full length of Foster Street.
- We advise that we will consult all residents on Foster Street as a matter of priority with a view to presenting a petition to Council about the major impacts on traffic and parking on the street with



shared recommendations for the way forward. It is recognised that the resulting actions will need to be considered within the Council's overall budget and forward capital works program.

- 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.***
  - 19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.***
- 

*[attach additional pages as needed]*

Note: In order for this submission to be valid, it must:

- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
  - Click here to enter text. *[list any accepted or deemed-to-satisfy elements of the development]*.

I: ☐ wish to be heard in support of my submission\*  
☒ do not wish to be heard in support of my submission

By: ☐ appearing personally  
☒ being represented by the following person:

*\*You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*

Signature

Return A

Email:

Complete online submission:

[plan.sa.gov.au/have\\_your\\_say/notified\\_developments/current\\_notified\\_developments](http://plan.sa.gov.au/have_your_say/notified_developments/current_notified_developments)

## Representations

Representor 11

Name	
Address	
Phone Number	
Email Address	
Submission Date	08/06/2022 12:36 PM
Submission Source	Over Counter
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
<b>Reasons</b> see submission	

## Attached Documents

ScivicGrou22060811392-3048928.pdf



## REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

*Planning, Development and Infrastructure Act 2016*

**Applicant:** St Louis Aged Care C/- Future Urban

**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

**Zone/Sub-zone/Overlay:** Established Neighbourhood Zone

**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

*\* Indicates mandatory information*

My position is: ☐ I support the development

☒ I support the development with some **major** concerns, many seeking conditional requirements for development application approval.

☐ I oppose the development

## Executive Summary

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

### Summary of Foster Street Resident Recommendations

1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.
  2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.
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    - a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;
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- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for the new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.
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-



establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.

The major concerns are detailed below and seek a range of key conditions and actions before such development is approved by Council.

---

## **LAND USE**

### **Future Land Use**

- The planning documents refer to the provision of residential aged care as being suitable for the Established Neighbourhood Zone and that such use is compatible with the zone.
  - This is accepted at the size and scale of the current 45 bedroom facility.
  - However, it is noted that in a recent 2021 newsletter St Louis stated
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### **Monthly Progress Meetings During Construction Phase and Preliminary Design Consultation**

- It is understood that St Louis have consulted widely with the Nursing Home residents and families regarding the design and build of the proposed new development.
  - No such consultation has been afforded to Foster Street residents even though there will be significant long term and short-term impacts on the street from this development.
  - It is also acknowledged that the construction phase of the development, if approved, will impact significantly on residents of Foster Street and surrounds. An appropriate communication process over this construction phase is sought as a priority.
- 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
- 3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.**
-

#### **Period of Building and capital development related to this current development application**

- Based on the 2019-20 development work, and should this development application progress, Foster Street residents are concerned about the additional noise, trucks, inappropriate and congested parking and start times, which from previous experience often commenced well before 7am.
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#### **4. Foster Street residents request St Louis:**

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  - It is recognised that the new addition, by keeping the required façade of the existing building will go some way to meeting that objective.
  - Our concern however is that the fencing and plantings around the new accommodation should provide a different and varied look from the existing fencing and plantings in the current 45 bed facility.
  - It is noted that the plan (DAO5) advises the fencing will match the existing St Louis fencing.
  - We are concerned that this will create an increased 'institutional' look to the new accommodation by having almost uniform fencing along the whole length of the existing aged care facility. This contrasts with the rest of Foster Street where fencing varies between each neighbour, therefore creating variety and interest along the street.
  - We feel it prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment.
- 6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the current facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.**
- 7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.**
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### Privacy

- Residents of 20, 22 and 24 Foster Street, as the most immediately impacted residences, are concerned that the new development, particularly with the location of its proposed pergola for meetings and sitting outside will directly overlook their homes.
- 8. Foster Street residents request that:**
  - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and***
  - b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for this new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.***

## INTERFACE BETWEEN LAND USES

- The planning document states that consideration has been made to ensure the minimisation of any adverse impacts on the amenity of sensitive receivers.
- The Plan states that noise will be minimised by
  - a. constructing a 2.4 metre acoustic fence (understood to be a Wallmark construction with foam filler) on the boundary of 23 Foster Street adjacent to the new car park.
  - b. restricting deliveries to the site between the hours of 7am and 10pm
  - c. turn off refrigeration before entering the site; and
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- At present residents of Foster Street are confronted with daily large commercial vehicles parked on the street with refrigeration noise sometimes for up to 20 minutes at very early hours of the morning, and other vehicles during the day. Early morning times in particular has often been against regulations and has been raised by various neighbours over many years.
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- The emptying of the large industrial site rubbish bin has to be carried out on the street as the truck cannot manage the activity fully on site. This blocks the street for a period of time or limits traffic to a narrow passageway.



9. ***Given that on-site space is proposed to be available, Foster Street Residents therefore request that Council enforce all trucks/vehicle deliveries, including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.***
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### **Delivery Vehicles**

- This issue is addressed above –

***14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.***

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  - Access and egress from our houses is often impacted by cars parked over our driveways. This is a serious safety concern when staff and visitors park too close to driveways compromising vision, turning and collision potential.
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  - The volume of on-street parking also impacts access for residents' weekly bins pickup, forcing residents to place bins on the actual street bitumen to ensure cars do not restrict access by waste management trucks.
  - Discussion among neighbours indicate that this has been an issue for many years, and evidenced in the previous 2019 development application submissions
  - At present there are only 6 designated car parks on the sites.
  - It is noted that documents from URPS (18 October 2019) in support of the 2019 St Louis application states that
  - "... the existing land has an existing imbedded shortfall of 8 on-site car parking spaces".
  - This 2022 application increases on-site parking by 11 – to a total of 16 – this is only just sufficient to meet the need of the 55 beds on the site based on this parking algorithm (pending approval).
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- 15. Foster St residents remain significantly concerned about on-street parking implications from this development application and wish this concern be noted by council for future deliberations regarding on-street parking and related traffic management issues.***



### **Proposed disability car parking**

- As part of the solution for the lack of parking, the proposed disability car park at the front of 23 Foster Street is a significant concern.
- Acknowledging that there is a need for such car parking, residents are concerned that its positioning at the front of 23 Foster Street will significantly impact on the character and feel of the street.
- In particular, using the site at No 23 will indicate that the house is not a residential house, but part of a commercial/ institutional facility.

***16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.***

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- The Plan indicates that 'additional open spaces and landscaping' is included in the proposal for the addition 10 beds.
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***17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.***

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- We understand that DAP only addresses development applications as they are received, and with a focus only on building design and on-site parking. This means that traffic and parking impacts on surrounding residents of such development applications are not addressed.
  - Residents of Foster Street are effectively "bookended " by residential support facilities – St Louis at the eastern end and a residential disability facility at the western end of the street.
  - Both these facilities, as they currently stand, have significant impact on parking and traffic. The issues relating to St Louis have been detailed above. Issues regarding the disability facility include similar and significant on-street parking and problems with access to Foster Street from Windsor Street as a result.
  - It is apparent that both facilities have plans for future expansion – St Louis has advised in its recent newsletter regarding its masterplan and the further (undetermined) development some time in the future of its additional available 5000sqm beyond the current application, and the disability service with its adjoining large spare block. The further development of these properties will significantly exacerbate what is already a major concern for traffic management and parking along the full length of Foster Street.
  - We advise that we will consult all residents on Foster Street as a matter of priority with a view to presenting a petition to Council about the major impacts on traffic and parking on the street with
-



shared recommendations for the way forward. It is recognised that the resulting actions will need to be considered within the Council's overall budget and forward capital works program.

- 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.***
  - 19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.***
- 

*[attach additional pages as needed]*

Note: In order for this submission to be valid, it must:

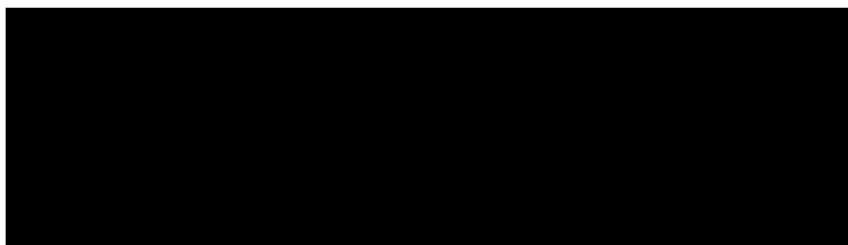
- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
  - Click here to enter text. *[list any accepted or deemed-to-satisfy elements of the development]*.

I: ☐ wish to be heard in support of my submission\*  
☒ do not wish to be heard in support of my submission

By: ☐ appearing personally  
☒ being represented by the following person:



*\*You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*



Date: 8 June 2022

or

Email:

or

Complete online submission:

[plan.sa.gov.au/have\\_your\\_say/notified\\_developments/current\\_notified\\_developments](https://plan.sa.gov.au/have_your_say/notified_developments/current_notified_developments)

## Representations

Representor 12 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	08/06/2022 12:39 PM
Submission Source	Over Counter
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
<b>Reasons</b> see submission	

## Attached Documents

ScivicGrou22060811400-3048973.pdf



## REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

*Planning, Development and Infrastructure Act 2016*

**Applicant:** St Louis Aged Care C/- Future Urban

**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

**Zone/Sub-zone/Overlay:** Established Neighbourhood Zone

**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

My name\*:

My phone number:

My postal address\*:

My email:

*\* Indicates mandatory information*

My position is:

☐

I support the development

☒

I support the development with some **major** concerns, many seeking conditional requirements for development application approval.

☐

I oppose the development

## Executive Summary

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

## Summary of Foster Street Resident Recommendations

1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.
2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.
3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.
4. Foster Street residents request St Louis:
  - a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;
  - b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;
  - c. ensure no noise occurs at the development site before 7am, 7 days a week.
5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.
6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the existing facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.
7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.
8. Foster Street residents request that:
  - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and

- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for the new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.
9. Given that on-site space is proposed to be available, Foster Street residents therefore request that Council enforce all trucks/vehicle deliveries including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.
10. Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.
11. Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.
12. Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development
13. Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.
14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.
15. Foster Street residents remain significantly concerned about on-street parking implications from this development application and wish this concern noted by council for future deliberations regarding on-street parking and related traffic management issues.
16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.
17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.
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establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.

The major concerns are detailed below and seek a range of key conditions and actions before such development is approved by Council.

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## **LAND USE**

### **Future Land Use**

- The planning documents refer to the provision of residential aged care as being suitable for the Established Neighbourhood Zone and that such use is compatible with the zone.
  - This is accepted at the size and scale of the current 45 bedroom facility.
  - However, it is noted that in a recent 2021 newsletter St Louis stated
    - *[St Louis] .."will be able to substantially expand St Louis nursing home located at 21 Foster Street Parkside. St Louis owns 5,000 square metres of land adjacent the nursing home [ie houses at 23,25,& 27 Foster Street on the western side], obtained over a number of years for precisely this purpose."*
  - Such a development would not be of the scale or form that the current service is.
  - While it is understood that the Development Assessment Panel (DAP) must limit its consideration to the application before them, residents are seeking transparency from St Louis of their intentions with respect to these houses.
  - It is noted that the houses west of the facility have not been kept in good repair, their gardens are significantly run down when compared to the rest of the street and the overall street scape is detrimental and impacts on residents' house values as a result.
  - Lack of clarity for the future of the houses on the western side causes significant concern for Foster Street residents.
- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**

### **Monthly Progress Meetings During Construction Phase and Preliminary Design Consultation**

- It is understood that St Louis have consulted widely with the Nursing Home residents and families regarding the design and build of the proposed new development.
  - No such consultation has been afforded to Foster Street residents even though there will be significant long term and short-term impacts on the street from this development.
  - It is also acknowledged that the construction phase of the development, if approved, will impact significantly on residents of Foster Street and surrounds. An appropriate communication process over this construction phase is sought as a priority.
- 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
- 3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.**
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#### **Period of Building and capital development related to this current development application**

- Based on the 2019-20 development work, and should this development application progress, Foster Street residents are concerned about the additional noise, trucks, inappropriate and congested parking and start times, which from previous experience often commenced well before 7am.
- During the previous build in 2019-20 there was no formal process to raise concerns with the builder or St Louis management with sufficient delegation and authority.

#### **4. Foster Street residents request St Louis:**

- a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;*
- b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;*
- c. ensure no noise occurs at the development site before 7am, 7 days a week.*

#### **5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.**

### **BUILT FORM AND DESIGN**

- This development will add an additional 10 residential aged care beds to the eastern side of the property – taking the 'nursing home' as St Louis refers to the facility, up to 55 beds.
- It is noted that the Royal Commission into Aged Care recommended that future aged care services have a small home approach.
- It is recognised that the new addition, by keeping the required façade of the existing building will go some way to meeting that objective.
- Our concern however is that the fencing and plantings around the new accommodation should provide a different and varied look from the existing fencing and plantings in the current 45 bed facility.
- It is noted that the plan (DAO5) advises the fencing will match the existing St Louis fencing.
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  - It is apparent that both facilities have plans for future expansion – St Louis has advised in its recent newsletter regarding its masterplan and the further (undetermined) development some time in the future of its additional available 5000sqm beyond the current application, and the disability service with its adjoining large spare block. The further development of these properties will significantly exacerbate what is already a major concern for traffic management and parking along the full length of Foster Street.
  - We advise that we will consult all residents on Foster Street as a matter of priority with a view to presenting a petition to Council about the major impacts on traffic and parking on the street with
-



shared recommendations for the way forward. It is recognised that the resulting actions will need to be considered within the Council's overall budget and forward capital works program.

- 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.***
  - 19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.***
- 

*[attach additional pages as needed]*

Note: In order for this submission to be valid, it must:

- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
  - Click here to enter text. *[list any accepted or deemed-to-satisfy elements of the development]*.

I: ☐ wish to be heard in support of my submission\*  
☒ do not wish to be heard in support of my submission

By: ☐ appearing personally  
☒ being represented by the following person:

*\*You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*

Signature:

Date: 8 June 2022

Return Address:

or

Email:

or

Complete online submission:

[plan.sa.gov.au/have\\_your\\_say/notified\\_developments/current\\_notified\\_developments](http://plan.sa.gov.au/have_your_say/notified_developments/current_notified_developments)

## Representations

Representor 13 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	08/06/2022 12:41 PM
Submission Source	Over Counter
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
<b>Reasons</b> see submission	

## Attached Documents

ScivicGrou22060811401-3049006.pdf



## REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

*Planning, Development and Infrastructure Act 2016*

**Applicant:** St Louis Aged Care C/- Future Urban

**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

**Zone/Sub-zone/Overlay:** Established Neighbourhood Zone

**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

My position is: ☐ I support the development

☒ I support the development with some **major** concerns, many seeking conditional requirements for development application approval.

☐ I oppose the development

## Executive Summary

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

## Summary of Foster Street Resident Recommendations

1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.
  2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.
  3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.
  4. Foster Street residents request St Louis:
    - a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;
    - b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;
    - c. ensure no noise occurs at the development site before 7am, 7 days a week.
  5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.
  6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the existing facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.
  7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.
  8. Foster Street residents request that:
    - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and
-

- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for the new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.
9. Given that on-site space is proposed to be available, Foster Street residents therefore request that Council enforce all trucks/vehicle deliveries including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.
10. Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.
11. Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.
12. Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development
13. Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.
14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.
15. Foster Street residents remain significantly concerned about on-street parking implications from this development application and wish this concern noted by council for future deliberations regarding on-street parking and related traffic management issues.
16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.
17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.
18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.
19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to
-



establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.

The major concerns are detailed below and seek a range of key conditions and actions before such development is approved by Council.

---

## **LAND USE**

### **Future Land Use**

- The planning documents refer to the provision of residential aged care as being suitable for the Established Neighbourhood Zone and that such use is compatible with the zone.
  - This is accepted at the size and scale of the current 45 bedroom facility.
  - However, it is noted that in a recent 2021 newsletter St Louis stated
    - *[St Louis] .."will be able to substantially expand St Louis nursing home located at 21 Foster Street Parkside. St Louis owns 5,000 square metres of land adjacent the nursing home [ie houses at 23,25,& 27 Foster Street on the western side], obtained over a number of years for precisely this purpose."*
  - Such a development would not be of the scale or form that the current service is.
  - While it is understood that the Development Assessment Panel (DAP) must limit its consideration to the application before them, residents are seeking transparency from St Louis of their intentions with respect to these houses.
  - It is noted that the houses west of the facility have not been kept in good repair, their gardens are significantly run down when compared to the rest of the street and the overall street scape is detrimental and impacts on residents' house values as a result.
  - Lack of clarity for the future of the houses on the western side causes significant concern for Foster Street residents.
- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**

### **Monthly Progress Meetings During Construction Phase and Preliminary Design Consultation**

- It is understood that St Louis have consulted widely with the Nursing Home residents and families regarding the design and build of the proposed new development.
  - No such consultation has been afforded to Foster Street residents even though there will be significant long term and short-term impacts on the street from this development.
  - It is also acknowledged that the construction phase of the development, if approved, will impact significantly on residents of Foster Street and surrounds. An appropriate communication process over this construction phase is sought as a priority.
- 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
- 3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.**
-

#### **Period of Building and capital development related to this current development application**

- Based on the 2019-20 development work, and should this development application progress, Foster Street residents are concerned about the additional noise, trucks, inappropriate and congested parking and start times, which from previous experience often commenced well before 7am.
- During the previous build in 2019-20 there was no formal process to raise concerns with the builder or St Louis management with sufficient delegation and authority.

#### **4. Foster Street residents request St Louis:**

- a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;*
- b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;*
- c. ensure no noise occurs at the development site before 7am, 7 days a week.*

#### **5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.**

### **BUILT FORM AND DESIGN**

- This development will add an additional 10 residential aged care beds to the eastern side of the property – taking the 'nursing home' as St Louis refers to the facility, up to 55 beds.
  - It is noted that the Royal Commission into Aged Care recommended that future aged care services have a small home approach.
  - It is recognised that the new addition, by keeping the required façade of the existing building will go some way to meeting that objective.
  - Our concern however is that the fencing and plantings around the new accommodation should provide a different and varied look from the existing fencing and plantings in the current 45 bed facility.
  - It is noted that the plan (DAO5) advises the fencing will match the existing St Louis fencing.
  - We are concerned that this will create an increased 'institutional' look to the new accommodation by having almost uniform fencing along the whole length of the existing aged care facility. This contrasts with the rest of Foster Street where fencing varies between each neighbour, therefore creating variety and interest along the street.
  - We feel it prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment.
- 6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the current facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.**
- 7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.**
-



### **Privacy**

- Residents of 20, 22 and 24 Foster Street, as the most immediately impacted residences, are concerned that the new development, particularly with the location of its proposed pergola for meetings and sitting outside will directly overlook their homes.
- 8. Foster Street residents request that:**
  - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and***
  - b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for this new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.***

### **INTERFACE BETWEEN LAND USES**

- The planning document states that consideration has been made to ensure the minimisation of any adverse impacts on the amenity of sensitive receivers.
- The Plan states that noise will be minimised by
  - a. constructing a 2.4 metre acoustic fence (understood to be a Wallmark construction with foam filler) on the boundary of 23 Foster Street adjacent to the new car park.
  - b. restricting deliveries to the site between the hours of 7am and 10pm
  - c. turn off refrigeration before entering the site; and
  - d. locating mechanical plant away from the residential boundaries.

### **Delivery Vehicles**

- At present residents of Foster Street are confronted with daily large commercial vehicles parked on the street with refrigeration noise sometimes for up to 20 minutes at very early hours of the morning, and other vehicles during the day. Early morning times in particular has often been against regulations and has been raised by various neighbours over many years.
- Of note, neighbours advise there continues to be a delivery that occurs regularly between 3-4am. By example, on the morning of 1 June 2022, the delivery vehicle pulled up at 3.15am. The vehicle has its engine off and glides to its delivery point which is close to 30 Foster Street. It then moves off as quietly as possible but still wakes people on the street. This is a clear breach of regulations.
- It is acknowledged that the new plans include dedicated on-site space so that commercial vehicles can unload within St Louis grounds which is not possible currently. The development application advises that this will reduce the volume of trucks/vehicles using the street for deliveries. Truck parking and unloading on the road restricts traffic flow and compromises street safety.
- The emptying of the large industrial site rubbish bin has to be carried out on the street as the truck cannot manage the activity fully on site. This blocks the street for a period of time or limits traffic to a narrow passageway.



9. ***Given that on-site space is proposed to be available, Foster Street Residents therefore request that Council enforce all trucks/vehicle deliveries, including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.***
10. ***Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.***
- It is acknowledged that on-site deliveries may cause noise problems for some residents of Blyth Street.
11. ***Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.***

#### **Air Conditioning and Plant Noise**

- At present the large air conditioner plant on the St Louis premises can be heard as a constant hum for houses on the other side of the street.
  - The noise consultant contracted by St Louis, has indicated that the placement of individual air conditioners for each new bedroom of the new development will address any noise concerns and that there will be no worsening of the existing noise.
12. ***Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development.***

#### **Light spill**

- Some street residents have been affected by light overspill, due to the installation of lighting presumably installed for safety reasons. Recent prompt intervention by the site manager minimised neighbourhood overspill from a poorly directed new LED replacement installation that pointed light at bedrooms. There is now an opportunity to review all current and proposed lighting for neighbourhood overspill. Luminaires that spill light in all directions are wasteful and compromise neighbourhood night time amenity and sleeping patterns. Most inappropriate lighting installations can be remedied by carefully considering what needs to be illuminated and installing energy efficient, properly shielded light fixtures for the purpose.
13. ***Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.***

## **TRAFFIC MANAGEMENT**

- The Plan states that the proposed addition does not result in unreasonable traffic implications, and further states that the new development will, in fact, assist in improving current access.

### **Delivery Vehicles**

- This issue is addressed above –

***14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.***

### **On-Street Parking**

- Parking on Foster Street is already significantly impacted by the St Louis staff and visitors, and made worse by the constant flow of commercial vehicles loading and unloading on Foster Street.
  - Access and egress from our houses is often impacted by cars parked over our driveways. This is a serious safety concern when staff and visitors park too close to driveways compromising vision, turning and collision potential.
  - There is little respect for Foster Street residents by poor or selfish parking. For example, during hot weather, some cars occupy two spaces for one car in order to park under a shade tree, thus limiting parking for everyone. This often forces another person to park inappropriately across or too close to a resident's driveway.
  - The volume of on-street parking also impacts access for residents' weekly bins pickup, forcing residents to place bins on the actual street bitumen to ensure cars do not restrict access by waste management trucks.
  - Discussion among neighbours indicate that this has been an issue for many years, and evidenced in the previous 2019 development application submissions
  - At present there are only 6 designated car parks on the sites.
  - It is noted that documents from URPS (18 October 2019) in support of the 2019 St Louis application states that
  - "... the existing land has an existing imbedded shortfall of 8 on-site car parking spaces".
  - This 2022 application increases on-site parking by 11 – to a total of 16 – this is only just sufficient to meet the need of the 55 beds on the site based on this parking algorithm (pending approval).
  - Although meeting planning requirements for parking, Foster Street residents consider the pressure on the street will remain – with residents still having to take the unreasonable pressure of staff and visitors.
- 15. Foster St residents remain significantly concerned about on-street parking implications from this development application and wish this concern be noted by council for future deliberations regarding on-street parking and related traffic management issues.***



### **Proposed disability car parking**

- As part of the solution for the lack of parking, the proposed disability car park at the front of 23 Foster Street is a significant concern.
- Acknowledging that there is a need for such car parking, residents are concerned that its positioning at the front of 23 Foster Street will significantly impact on the character and feel of the street.
- In particular, using the site at No 23 will indicate that the house is not a residential house, but part of a commercial/ institutional facility.

***16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.***

## **LANDSCAPING AND OPEN SPACE**

- The Plan indicates that 'additional open spaces and landscaping' is included in the proposal for the addition 10 beds.
- Foster Street residents are concerned that the new development appears to be removing existing established trees and only leaving small shrubs. This will detract significantly from the amenity of the site and will create privacy concerns for the residents across the road from the development.

***17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.***

## **OTHER SIGNIFICANT MATTERS**

### **Future planning for Foster Street and maintenance of amenity**

- We understand that DAP only addresses development applications as they are received, and with a focus only on building design and on-site parking. This means that traffic and parking impacts on surrounding residents of such development applications are not addressed.
- Residents of Foster Street are effectively "bookended " by residential support facilities – St Louis at the eastern end and a residential disability facility at the western end of the street.
- Both these facilities, as they currently stand, have significant impact on parking and traffic. The issues relating to St Louis have been detailed above. Issues regarding the disability facility include similar and significant on-street parking and problems with access to Foster Street from Windsor Street as a result.
- It is apparent that both facilities have plans for future expansion – St Louis has advised in its recent newsletter regarding its masterplan and the further (undetermined) development some time in the future of its additional available 5000sqm beyond the current application, and the disability service with its adjoining large spare block. The further development of these properties will significantly exacerbate what is already a major concern for traffic management and parking along the full length of Foster Street.
- We advise that we will consult all residents on Foster Street as a matter of priority with a view to presenting a petition to Council about the major impacts on traffic and parking on the street with



shared recommendations for the way forward. It is recognised that the resulting actions will need to be considered within the Council's overall budget and forward capital works program.

- 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.***
  - 19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.***
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*[attach additional pages as needed]*

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## Representations

Representor 14 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	08/06/2022 12:45 PM
Submission Source	Over Counter
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
<b>Reasons</b> see submission	

## Attached Documents

ScivicGrou22060811410-3049110.pdf



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*Planning, Development and Infrastructure Act 2016*

**Applicant:** St Louis Aged Care C/- Future Urban

**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

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**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

My position is: ☐ I support the development

☒ I support the development with some **major** concerns, many seeking conditional requirements for development application approval.

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## **Executive Summary**

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

## **Summary of Foster Street Resident Recommendations**

- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**
- 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
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- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for the new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.
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-



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The major concerns are detailed below and seek a range of key conditions and actions before such development is approved by Council.

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### **Future Land Use**

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### **Monthly Progress Meetings During Construction Phase and Preliminary Design Consultation**

- It is understood that St Louis have consulted widely with the Nursing Home residents and families regarding the design and build of the proposed new development.
  - No such consultation has been afforded to Foster Street residents even though there will be significant long term and short-term impacts on the street from this development.
  - It is also acknowledged that the construction phase of the development, if approved, will impact significantly on residents of Foster Street and surrounds. An appropriate communication process over this construction phase is sought as a priority.
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-

#### **Period of Building and capital development related to this current development application**

- Based on the 2019-20 development work, and should this development application progress, Foster Street residents are concerned about the additional noise, trucks, inappropriate and congested parking and start times, which from previous experience often commenced well before 7am.
- During the previous build in 2019-20 there was no formal process to raise concerns with the builder or St Louis management with sufficient delegation and authority.

#### **4. Foster Street residents request St Louis:**

- a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;***
- b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;***
- c. ensure no noise occurs at the development site before 7am, 7 days a week.***

#### **5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.**

### **BUILT FORM AND DESIGN**

- This development will add an additional 10 residential aged care beds to the eastern side of the property – taking the 'nursing home' as St Louis refers to the facility, up to 55 beds.
  - It is noted that the Royal Commission into Aged Care recommended that future aged care services have a small home approach.
  - It is recognised that the new addition, by keeping the required façade of the existing building will go some way to meeting that objective.
  - Our concern however is that the fencing and plantings around the new accommodation should provide a different and varied look from the existing fencing and plantings in the current 45 bed facility.
  - It is noted that the plan (DAO5) advises the fencing will match the existing St Louis fencing.
  - We are concerned that this will create an increased 'institutional' look to the new accommodation by having almost uniform fencing along the whole length of the existing aged care facility. This contrasts with the rest of Foster Street where fencing varies between each neighbour, therefore creating variety and interest along the street.
  - We feel it prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment.
- 6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the current facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.**
- 7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.**



### **Privacy**

- Residents of 20, 22 and 24 Foster Street, as the most immediately impacted residences, are concerned that the new development, particularly with the location of its proposed pergola for meetings and sitting outside will directly overlook their homes.

#### **8. Foster Street residents request that:**

- a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and*
- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for this new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.*

### **INTERFACE BETWEEN LAND USES**

- The planning document states that consideration has been made to ensure the minimisation of any adverse impacts on the amenity of sensitive receivers.
- The Plan states that noise will be minimised by
  - a. constructing a 2.4 metre acoustic fence (understood to be a Wallmark construction with foam filler) on the boundary of 23 Foster Street adjacent to the new car park.
  - b. restricting deliveries to the site between the hours of 7am and 10pm
  - c. turn off refrigeration before entering the site; and
  - d. locating mechanical plant away from the residential boundaries.

### **Delivery Vehicles**

- At present residents of Foster Street are confronted with daily large commercial vehicles parked on the street with refrigeration noise sometimes for up to 20 minutes at very early hours of the morning, and other vehicles during the day. Early morning times in particular has often been against regulations and has been raised by various neighbours over many years.
- Of note, neighbours advise there continues to be a delivery that occurs regularly between 3-4am. By example, on the morning of 1 June 2022, the delivery vehicle pulled up at 3.15am. The vehicle has its engine off and glides to its delivery point which is close to 30 Foster Street. It then moves off as quietly as possible but still wakes people on the street. This is a clear breach of regulations.
- It is acknowledged that the new plans include dedicated on-site space so that commercial vehicles can unload within St Louis grounds which is not possible currently. The development application advises that this will reduce the volume of trucks/vehicles using the street for deliveries. Truck parking and unloading on the road restricts traffic flow and compromises street safety.
- The emptying of the large industrial site rubbish bin has to be carried out on the street as the truck cannot manage the activity fully on site. This blocks the street for a period of time or limits traffic to a narrow passageway.



9. ***Given that on-site space is proposed to be available, Foster Street Residents therefore request that Council enforce all trucks/vehicle deliveries, including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.***
10. ***Foster Street residents request that some form of barrier/vegetation or other device be included on the street so that large commercial vehicles are no longer able to park on the street.***
- It is acknowledged that on-site deliveries may cause noise problems for some residents of Blyth Street.
11. ***Foster Street residents request that Council ensures the development application abides by installing acoustic fencing for the proposed additional on-site carpark which provides for suitable noise reduction to affected neighbouring residences and that vehicles have their refrigeration machines turned off at all times for the full duration of their delivery period.***

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12. ***Foster Street residents request that no additional noise from air conditioning or any other enhancements to plant machinery as a result of the new development application be made as a clear condition of the approval for this development.***

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- Some street residents have been affected by light overspill, due to the installation of lighting presumably installed for safety reasons. Recent prompt intervention by the site manager minimised neighbourhood overspill from a poorly directed new LED replacement installation that pointed light at bedrooms. There is now an opportunity to review all current and proposed lighting for neighbourhood overspill. Luminaires that spill light in all directions are wasteful and compromise neighbourhood night time amenity and sleeping patterns. Most inappropriate lighting installations can be remedied by carefully considering what needs to be illuminated and installing energy efficient, properly shielded light fixtures for the purpose.
13. ***Foster Street residents request that current and proposed lighting for the new developments be assessed for inappropriate neighbourhood environmental overspill. This should be a clear condition of the approval for this development.***

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- The Plan states that the proposed addition does not result in unreasonable traffic implications, and further states that the new development will, in fact, assist in improving current access.

### **Delivery Vehicles**

- This issue is addressed above –

***14. Foster Street residents request that delivery and other commercial vehicles are banned from parking on the street and are required to use the new cross over and on-site car parking area at all times as a condition of development approval.***

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- Parking on Foster Street is already significantly impacted by the St Louis staff and visitors, and made worse by the constant flow of commercial vehicles loading and unloading on Foster Street.
  - Access and egress from our houses is often impacted by cars parked over our driveways. This is a serious safety concern when staff and visitors park too close to driveways compromising vision, turning and collision potential.
  - There is little respect for Foster Street residents by poor or selfish parking. For example, during hot weather, some cars occupy two spaces for one car in order to park under a shade tree, thus limiting parking for everyone. This often forces another person to park inappropriately across or too close to a resident's driveway.
  - The volume of on-street parking also impacts access for residents' weekly bins pickup, forcing residents to place bins on the actual street bitumen to ensure cars do not restrict access by waste management trucks.
  - Discussion among neighbours indicate that this has been an issue for many years, and evidenced in the previous 2019 development application submissions
  - At present there are only 6 designated car parks on the sites.
  - It is noted that documents from URPS (18 October 2019) in support of the 2019 St Louis application states that
  - "... the existing land has an existing imbedded shortfall of 8 on-site car parking spaces".
  - This 2022 application increases on-site parking by 11 – to a total of 16 – this is only just sufficient to meet the need of the 55 beds on the site based on this parking algorithm (pending approval).
  - Although meeting planning requirements for parking, Foster Street residents consider the pressure on the street will remain – with residents still having to take the unreasonable pressure of staff and visitors.
- 15. Foster St residents remain significantly concerned about on-street parking implications from this development application and wish this concern be noted by council for future deliberations regarding on-street parking and related traffic management issues.***



### Proposed disability car parking

- As part of the solution for the lack of parking, the proposed disability car park at the front of 23 Foster Street is a significant concern.
- Acknowledging that there is a need for such car parking, residents are concerned that its positioning at the front of 23 Foster Street will significantly impact on the character and feel of the street.
- In particular, using the site at No 23 will indicate that the house is not a residential house, but part of a commercial/ institutional facility.

**16. Foster Street residents request that the disability car park be placed internally in another area within the St Louis site as a condition for the development approval. If this can not be achieved after detailed consideration by Council, then some other strategy, agreed by residents, be employed to fully mask the car park from the street frontage.**

## LANDSCAPING AND OPEN SPACE

- The Plan indicates that 'additional open spaces and landscaping' is included in the proposal for the addition 10 beds.
- Foster Street residents are concerned that the new development appears to be removing existing established trees and only leaving small shrubs. This will detract significantly from the amenity of the site and will create privacy concerns for the residents across the road from the development.

**17. Foster Street residents request that this development include gardens involving trees and significant shrubbery height in the development of 17 Foster Street.**

## OTHER SIGNIFICANT MATTERS

### Future planning for Foster Street and maintenance of amenity

- We understand that DAP only addresses development applications as they are received, and with a focus only on building design and on-site parking. This means that traffic and parking impacts on surrounding residents of such development applications are not addressed.
  - Residents of Foster Street are effectively "bookended " by residential support facilities – St Louis at the eastern end and a residential disability facility at the western end of the street.
  - Both these facilities, as they currently stand, have significant impact on parking and traffic. The issues relating to St Louis have been detailed above. Issues regarding the disability facility include similar and significant on-street parking and problems with access to Foster Street from Windsor Street as a result.
  - It is apparent that both facilities have plans for future expansion – St Louis has advised in its recent newsletter regarding its masterplan and the further (undetermined) development some time in the future of its additional available 5000sqm beyond the current application, and the disability service with its adjoining large spare block. The further development of these properties will significantly exacerbate what is already a major concern for traffic management and parking along the full length of Foster Street.
  - We advise that we will consult all residents on Foster Street as a matter of priority with a view to presenting a petition to Council about the major impacts on traffic and parking on the street with
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shared recommendations for the way forward. It is recognised that the resulting actions will need to be considered within the Council's overall budget and forward capital works program.

- 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.***
  - 19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.***
- 

*[attach additional pages as needed]*

Note: In order for this submission to be valid, it must:

- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
  - Click here to enter text. *[list any accepted or deemed-to-satisfy elements of the development]*.

I: ☐ wish to be heard in support of my submission\*  
☒ do not wish to be heard in support of my submission

By: ☐ appearing personally  
☒ being represented by the following person:

*\*You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*

Complete online submission:

[plan.sa.gov.au/have\\_your\\_say/notified\\_developments/current\\_notified\\_developments](http://plan.sa.gov.au/have_your_say/notified_developments/current_notified_developments)

## Representations

Representor 15 - [REDACTED]

Name	[REDACTED]
Address	[REDACTED]
Phone Number	[REDACTED]
Email Address	[REDACTED]
Submission Date	08/06/2022 12:49 PM
Submission Source	Over Counter
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
<b>Reasons</b> see submission	

## Attached Documents

ScivicGrou22060811412-3049205.pdf



## REPRESENTATION ON APPLICATION – PERFORMANCE ASSESSED DEVELOPMENT

*Planning, Development and Infrastructure Act 2016*

**Applicant:** St Louis Aged Care C/- Future Urban

**Development Number:** 22015437

**Nature of Development:** Alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing (2.4m high)

**Zone/Sub-zone/Overlay:** Established Neighbourhood Zone

**Subject Land:** 17 Foster Street, Parkside; 23 Foster Street Parkside; 19-21 Foster Street Parkside

**Contact Officer:** City of Unley Mark Troncone

**Phone Number:**

**Close Date:** 8 June 2022

My position is:

☐ I support the development

☒ I support the development with some major concerns, many seeking conditional requirements for development application approval.

☐ I oppose the development

## Executive Summary

**Foster Street residents' key message** is that it is prudent and responsible to ensure the proposed development sits comfortably and seamlessly into a predominantly residential environment and that noise, traffic, parking, privacy and street scape impacts from this proposed development, as detailed in this representation, are adequately addressed and conditional for the development application to be approved.

## Summary of Foster Street Resident Recommendations

1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.
2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.
3. Foster Street residents request that the first such monthly progress meeting be used for a preliminary design consultation prior to final design and any building commencing.
4. Foster Street residents request St Louis:
  - a. nominate a direct contact point for residents with an appropriate level of authority and delegation to contact as issues arise during the construction phase so that they are addressed and resolved within 24 hours;
  - b. make provision on St Louis property to accommodate additional trade workers' cars to alleviate expected additional parking problems during the development phase;
  - c. ensure no noise occurs at the development site before 7am, 7 days a week.
5. Foster Street residents request that Council schedule regular parking inspections during this building period, if approved.
6. Foster Street residents request that the new fencing for #17 Foster Street be no less than the height of the existing tubular fencing of the existing facility and be significantly different from the existing St Louis frontage to achieve the perception of another residential dwelling on Foster Street, not an extension for a larger institution.
7. Foster Street residents also request that St Louis consider changes to the current fencing to reduce the institutional look of the current street scape.
8. Foster Street residents request that:
  - a. fencing and plantings on the new development are created which addresses the privacy requirements for those homes directly opposite the proposed development (residents of 20, 22 and 24 Foster Street) while also providing St Louis residents with an ability to access the streetscape; and

- b. St Louis commit to working with residents of 20, 22 and 24 Foster Street to agree a suitable solution for the new fencing. One option is sections of solid Wallmark walls suitable for these residences' privacy and intermingled with sections of railings with high hedging and trees to shield from the street.
9. Given that on-site space is proposed to be available, Foster Street residents therefore request that Council enforce all trucks/vehicle deliveries including Cleanaway collection trucks, be on-site as a condition of approving the development. This is a non-negotiable requirement for all of those trucks/vehicles.
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establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.

The major concerns are detailed below and seek a range of key conditions and actions before such development is approved by Council.

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## **LAND USE**

### **Future Land Use**

- The planning documents refer to the provision of residential aged care as being suitable for the Established Neighbourhood Zone and that such use is compatible with the zone.
  - This is accepted at the size and scale of the current 45 bedroom facility.
  - However, it is noted that in a recent 2021 newsletter St Louis stated
    - *[St Louis] .."will be able to substantially expand St Louis nursing home located at 21 Foster Street Parkside. St Louis owns 5,000 square metres of land adjacent the nursing home [ie houses at 23,25,& 27 Foster Street on the western side], obtained over a number of years for precisely this purpose."*
  - Such a development would not be of the scale or form that the current service is.
  - While it is understood that the Development Assessment Panel (DAP) must limit its consideration to the application before them, residents are seeking transparency from St Louis of their intentions with respect to these houses.
  - It is noted that the houses west of the facility have not been kept in good repair, their gardens are significantly run down when compared to the rest of the street and the overall street scape is detrimental and impacts on residents' house values as a result.
  - Lack of clarity for the future of the houses on the western side causes significant concern for Foster Street residents.
- 1. Foster Street residents request that St Louis establish a regular and ongoing minuted forum to meet with Foster Street residents so that they can discuss any issues that may arise and provide St Louis with a forum to provide information on future plans. It is requested that this forum be held 6 monthly at a minimum.**

### **Monthly Progress Meetings During Construction Phase and Preliminary Design Consultation**

- It is understood that St Louis have consulted widely with the Nursing Home residents and families regarding the design and build of the proposed new development.
  - No such consultation has been afforded to Foster Street residents even though there will be significant long term and short-term impacts on the street from this development.
  - It is also acknowledged that the construction phase of the development, if approved, will impact significantly on residents of Foster Street and surrounds. An appropriate communication process over this construction phase is sought as a priority.
- 2. Foster Street residents request that in relation to this specific development application and if approved, building period, a regular monthly progress meeting occur between St Louis and Foster Street residents to discuss progress and any issues arising.**
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- We understand that DAP only addresses development applications as they are received, and with a focus only on building design and on-site parking. This means that traffic and parking impacts on surrounding residents of such development applications are not addressed.
  - Residents of Foster Street are effectively "bookended " by residential support facilities – St Louis at the eastern end and a residential disability facility at the western end of the street.
  - Both these facilities, as they currently stand, have significant impact on parking and traffic. The issues relating to St Louis have been detailed above. Issues regarding the disability facility include similar and significant on-street parking and problems with access to Foster Street from Windsor Street as a result.
  - It is apparent that both facilities have plans for future expansion – St Louis has advised in its recent newsletter regarding its masterplan and the further (undetermined) development some time in the future of its additional available 5000sqm beyond the current application, and the disability service with its adjoining large spare block. The further development of these properties will significantly exacerbate what is already a major concern for traffic management and parking along the full length of Foster Street.
  - We advise that we will consult all residents on Foster Street as a matter of priority with a view to presenting a petition to Council about the major impacts on traffic and parking on the street with
-



shared recommendations for the way forward. It is recognised that the resulting actions will need to be considered within the Council's overall budget and forward capital works program.

- 18. Foster Street residents request Council to note impending action by Foster Street residents to present a petition with recommendations to Council about management of traffic and on-street parking along the full length of Foster Street between Kenilworth and Windsor Roads.**
- 19. Foster Street residents request that Council actively engage with both St Louis and the disability service provider located at the western end of Foster Street near Windsor Road to establish a full understanding of their future development plans including their proposed strategies to address subsequent parking and traffic management impacts into the future.**

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*[attach additional pages as needed]*

Note: In order for this submission to be valid, it must:

- be in writing; and
- include the name and address of the person (or persons) who are making the representation; and
- set out the particular reasons why planning consent should be granted or refused; and
- comment only on the performance-based elements of the proposal, which does not include the:
  - Click here to enter text. *[list any accepted or deemed-to-satisfy elements of the development]*.

I: ☐ wish to be heard in support of my submission\*  
☒ do not wish to be heard in support of my submission

By: ☐ appearing personally  
☒ being represented by the following person:

*\*You may be contacted if you indicate that you wish to be heard by the relevant authority in support of your submission*

Complete online submission:

[plan.sa.gov.au/have\\_your\\_say/notified\\_developments/current\\_notified\\_developments](http://plan.sa.gov.au/have_your_say/notified_developments/current_notified_developments)

## **ATTACHMENT 3**



June 21, 2022

Mark Troncone  
City of Unley  
via the PlanSA Portal

Dear Mark,

## **RE: DA 22015437 (17-23 FOSTER STREET, PARKSIDE) – RESPONSE TO REPRESENTATIONS**

I refer to the proposed development application for alterations and additions to the existing Parkside St Louis Aged Care facility including ancillary car park and associated acoustic fencing.

Public notification has been undertaken and completed. The comments received from the representors during the public notification of the development application primarily relate to traffic and parking, noise, lighting, design and siting, delivery and waste collection, privacy, landscaping, fencing,

There is also a significant portion of matters raised by residents that are not planning related, including but not limited to, diminution of property values and construction impacts and as such, these will not be commented on.

Therefore, our response to each is set out below.

### **Design and Siting**

Comments were raised around the access door and setback on the eastern side. This door is required for emergency purposes and not for a thoroughfare. The side setback achieves Zone DPF 8.1, which recommends a side boundary setback of 1 metre.

The addition has been designed to retain the façade of the existing dwelling to ensure the streetscape character and amenity is not impacted by this proposal.

### **Amenity Impacts**

#### **Air pollution**

Concerns were raised over the exhaust and fumes generated from vehicles using the facility.

No evidence was provided to support these claims. It is noted that the ERD Court has provided guidance on many occasions to third party representors who wish to challenge a decision of local planning authorities to approve a development, with the decision of *Carey and Bourdon v DAC [1994] EDLR 233* being most instructive:

"..... an appellant should present a case of substance; ..... assertions should be supported by evidence amounting to more than a collection of presumptions by an unqualified observer..... Generally, it would not be enough to merely raise an issue without producing supporting evidence, particularly when the issue had been addressed by the developer as part of the development application."

No evidence has been put forward to support the assertions in relation to fumes or other odours.

## Noise

An acoustic expert was engaged to undertake an assessment to determine whether the proposed additions would create additional unreasonable noise impacts on adjoining properties. It was concluded that:

- Many of the noise sources remain unchanged by the proposed addition.
- The predicted noise levels from the proposal were assessed against both the Code and *Environment Protection (Noise) Policy 2007* and ensure the amenity of the surrounding sensitive receivers is not adversely impacted. This assessment determined that the proposal can reasonably and practicably achieve the standards through implementation of the following:
  - » acoustic fencing between car park and nearest dwellings;
  - » restrict deliveries to the site between the hours of 7:00am and 10:00pm, in line with EPA guidelines;
  - » ensuring delivery trucks turn off any roof mounted refrigeration systems before entering the site;
  - » specify staff carparks that will be used after 10:00pm or before 7:00am; and
  - » mechanical plant located away from the residential boundary.

The applicant is willing to abide by a condition of consent indicating that noise shall satisfy the EPA Noise Guidelines.

The applicant also confirms that whilst detailed design of services, including air-conditioning units has not been fully resolved, condensers will not be placed on the eastern boundary and will all be appropriately screened to ensure compliance with a condition of consent about compliance with the EPA Noise Guidelines.

## Light

No light poles are proposed and lighting that will be provided in the car park will be by way of low-level bollard lighting ensuring no light spill whatsoever.

Lighting, in its own right, does not constitute development. Even so, the applicant is willing to abide by a condition of consent, should it be forthcoming, to adhere to the Australian Standards relating to the obtrusive impacts of light spill.

## Privacy

Concerns were raised about the privacy impacts upon 20-24 Foster Street in relation to residents of St Louis sitting under the pergola. Firstly, this pergola is at the front of the proposed development, facing the street, and secondly, the properties in question are located across the road directly opposite the site. Looking into the front yard of properties occurs regularly as people walk along the street footpath. Views into properties from public spaces is not considered overlooking in a planning context.

Notwithstanding, the applicant is willing to look at reasonable options to provide further vegetation screening to the front elevation ensuring privacy without compromising the outlook of the residents of the aged care facility.

## **Traffic Management and Parking**

The vast majority of representations assert the proposal will have deleterious impacts upon traffic volume increase, movement and congestion.

It is noted that the representors have not provided any evidence to support these assertions. As noted above, if one is to make assertions about adverse impacts about a particular development proposal, independent evidence should be provided to support that view. It has not.

In addition, it does appear that the majority of representations appear to predominantly show their frustration at the current situation which occurs in relation to traffic congestion, parking and access within the entire area, rather than aimed specifically at the proposed development.

MFY Traffic Consultants have undertaken an assessment to confirm that the proposed addition does not result in unreasonable traffic implications, and actually assists in improving the current access arrangements and number of off-street car parks.

The car parking rate specified within the Code, for supported accommodation is 0.3 spaces per bed. Based on the centre's total capacity of 55 beds, the site has a theoretical demand of 17 parking spaces. 17 spaces are provided as a result of this proposed development, and also satisfying the Code. There will be no reliance for on-street parking as a result of the proposal. In fact, the proposal will ameliorate a current shortfall in car parking spaces at the development site.

## **Delivery and Waste Collection**

Deliveries are proposed to be collected on site and will adhere to the Environment Protection (Noise) Policy which deals with noise from vehicles limiting the hours to the least sensitive period of the day. Division 3 of the Policy requires rubbish collection to only occur between the hours of 9am and 7pm on Sundays or public holidays, and between 7am and 7pm on any other day. The proponent is willing to abide a condition of consent to this effect.

Waste collection will occur on street, as presently occurs in the dedicated loading bay to the front of the subject site. There will be no additional collections resulting from the proposed addition to the existing facility.

It is noted that this is to be a substantial improvement, given the site currently has waste and delivery collection undertaken on-street.

## **Fencing**

The front boundary steel fence has been selected to match in and continue with the existing front fencing of the St Louis Parkside Aged Care facility. The locality presents a vast range of fencing heights and styles, including but not limited to; Colorbond, brush, hedging, brick, wire and steel. As such, it is considered appropriate and not to impact upon the amenity of the streetscape.

Some representors also raised concerns about boundary fencing and impacts on vegetation and materials held upon their fences. It is confirmed the applicant will negotiate in good faith with any affected neighbours about boundary fencing in accordance with the requirements of the *Fences Act 1975*.



## **Other**

Construction impacts and perceived diminution in property values are not relevant to the planning assessment of the application.

Construction impacts are not a planning matter and can all be adequately dealt with by the requirement to submit a construction management plan via an appropriately worded condition of planning consent.

Property values are often raised in opposition to development proposals. The submission is (a) not relevant and (b) again, not supported by any empirical evidence.

As sought in a number of the representations, the applicant is willing to engage in a monthly forum meeting with Foster Street residents during the construction phase of the project to discuss construction impacts.

Finally, representations about ambulances “screaming” down Foster Street are refuted. My client has operated St Louis in Parkside for 75 years and are unaware of a time when an ambulance has operated a siren whilst in Foster Street. Ambulances generally only use their sirens in high traffic arterial roads and intersections and have no cause to operate a siren in residential streets.

I trust this adequately responds to the written representations received by Council.

I look forward to this matter being presented to the next available Council Assessment Panel meeting.

Yours sincerely,



**Marc Duncan**  
Director

## **ATTACHMENT 4**

Hi Mark,

I have reviewed the development plans, in conjunction with the traffic and parking assessment, for the proposed expansion of the existing aged care facility at 17-23 Foster Street, Parkside. Please see my comments below:

- Table 1 – General Off-Street car parking requirements from the Planning and Design Code indicates that for supported accommodation, a parking generation rate of 0.3 spaces per bed is applicable. Based on this rate, the proposed 10 bed addition will generate a requirement for 3 off-street parking spaces. Given it is proposed to provide 11 off-street parking spaces within an at-grade car park, this meets the planning scheme requirements and is considered acceptable.
- The traffic report prepared by MFY (dated 6 May 2022) indicates that all off-street parking facilities will be provided in accordance with relevant Australian standards (AS/NZS 2890.1:2004), which is considered acceptable including:
  - All parking spaces will be 4.8m long with 600mm overhanging low lying landscaping
  - All visitor spaces will be at least 2.6m wide
  - All staff spaces will be at least 2.4m wide
  - Parking aisle will be at least 5.8m wide
  - Small car space at least 2.3m wide (space will need to be signed and line marked as small car only).
  - All spaces adjacent obstructions will be widened by at least 300mm
- The traffic report indicates that a DDA parking space will be provided in accordance with Australian Standards, with 2.4m width, 5.4m length and adjacent shared area with similar dimensions. This is considered acceptable.
- The traffic report has indicated that the existing driveway crossover to Foster Street will be widened to cater for two-way traffic flow, allow commercial waste collection to occur on-site and provide a pedestrian sight triangle measuring 2m x 2.5m (in accordance with Australian Standards). A swept path assessment has been provided which indicates a MRV design vehicle is able to enter and exit the site in a forward's direction, this is considered acceptable.

If you have any questions about the above, please feel free to give me a call.

Cheers,

Jacob Avery

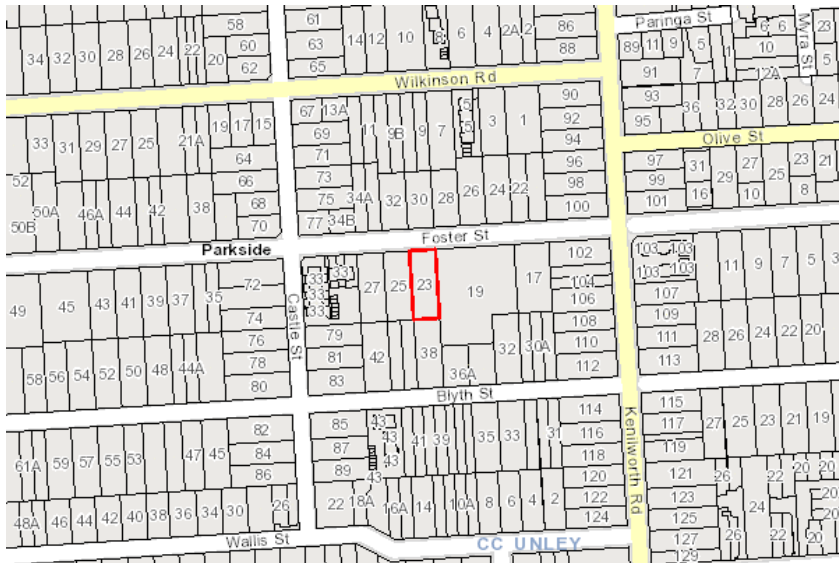
Transport Engineer



## **ATTACHMENT 5**

**23 FOSTER ST PARKSIDE SA 5063****Address:**Click to view a detailed interactive [SAILIS](#) in SAILIS

To view a detailed interactive property map in SAPPA click on the map below

**Property Zoning Details****Local Variation (TNV)**Maximum Building Height (Metres) (*Maximum building height is 5.7m*)Minimum Frontage (*Minimum frontage for a detached dwelling is 15m; semi-detached dwelling is 15m; row dwelling is 15m*)Minimum Site Area (*Minimum site area for a detached dwelling is 500 sqm; semi-detached dwelling is 500 sqm; row dwelling is 500 sqm*)Maximum Building Height (Levels) (*Maximum building height is 1 level*)Minimum Side Boundary Setback (*Minimum side boundary setback is 1m for the first building level; 3m for any second building level or higher*)Site Coverage (*Maximum site coverage is 50 per cent*)**Overlay**Airport Building Heights (Regulated) (*All structures over 45 metres*)Historic Area (*Un27*)

Prescribed Wells Area

Regulated and Significant Tree

Stormwater Management

Urban Tree Canopy

Water Resources

**Zone**

Established Neighbourhood

**Development Pathways**

## ■ Established Neighbourhood

## 1. Accepted Development

Means that the development type does not require planning consent (planning approval). Please ensure compliance with relevant land use and development controls in the Code.

- Air handling unit, air conditioning system or exhaust fan
- Brush fence
- Building work on railway land
- Internal building work
- Outbuilding

- Partial demolition of a building or structure
- Private bushfire shelter
- Shade sail
- Solar photovoltaic panels (roof mounted)
- Swimming pool or spa pool
- Verandah
- Water tank (above ground)
- Water tank (underground)

## 2. Code Assessed - Deemed to Satisfy

Means that the development type requires consent (planning approval). Please ensure compliance with relevant land use and development controls in the Code.

- Ancillary accommodation
- Carport
- Outbuilding
- Replacement building
- Temporary accommodation in an area affected by bushfire
- Verandah

## 3. Code Assessed - Performance Assessed

Performance Assessed development types listed below are those for which the Code identifies relevant policies. Additional development types that are not listed as Accepted, Deemed to Satisfy or Restricted default to a Performance assessed Pathway. Please contact your local council for more information.

- Ancillary accommodation
- Carport
- Demolition
- Detached dwelling
- Dwelling addition
- Fence
- Group dwelling
- Land division
- Outbuilding
- Residential flat building
- Retaining wall
- Row dwelling
- Semi-detached dwelling
- Tree-damaging activity
- Verandah

## 4. Impact Assessed - Restricted

Means that the development type requires approval. Classes of development that are classified as Restricted are listed in Table 4 of the relevant Zones.

Property Policy Information for above selection

## Part 2 - Zones and Sub Zones

### Established Neighbourhood Zone

#### Assessment Provisions (AP)

Desired Outcome	
DO 1	A neighbourhood that includes a range of housing types, with new buildings sympathetic to the predominant built form character and development patterns.
DO 2	Maintain the predominant streetscape character, having regard to key features such as roadside plantings, footpaths, front yards, and space between crossovers.



Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
<p>PO 1.1</p> <p>Predominantly residential development with complementary non-residential activities compatible with the established development pattern of the neighbourhood.</p>	<p>DTS/DPF 1.1</p> <p>Development comprises one or more of the following:</p> <ul style="list-style-type: none"> <li>(a) Ancillary accommodation</li> <li>(b) Community facility</li> <li>(c) Consulting room</li> <li>(d) Dwelling</li> <li>(e) Office</li> <li>(f) Recreation area</li> <li>(g) Shop.</li> </ul>
<p>PO 1.2</p> <p>Commercial activities improve community access to services are of a scale and type to maintain residential amenity.</p>	<p>DTS/DPF 1.2</p> <p>A shop, consulting room or office (or any combination thereof) satisfies any one of the following:</p> <ul style="list-style-type: none"> <li>(a) it is located on the same allotment and in conjunction with a dwelling where all the following are satisfied: <ul style="list-style-type: none"> <li>(i) does not exceed 30% of the total floor area of the associated dwelling (excluding any garage or carport) or 50m<sup>2</sup> gross leasable floor area, whichever is the lesser</li> <li>(ii) does not involve the display of goods in a window or about the dwelling or its curtilage</li> </ul> </li> <li>(b) it reinstates a former shop, consulting room or office in an existing building (or portion of a building) and satisfies one of the following: <ul style="list-style-type: none"> <li>(i) the building is a State or Local Heritage Place</li> <li>(ii) is in conjunction with a dwelling and there is no increase in the gross leasable floor area previously used for non-residential purposes</li> </ul> </li> <li>(c) is located more than 500m from an Activity Centre and satisfies one of the following: <ul style="list-style-type: none"> <li>(i) does not exceed 100m<sup>2</sup> gross leasable floor area (individually or combined, in a single building) where the site does not have a frontage to a State Maintained Road</li> <li>(ii) does not exceed 200m<sup>2</sup> gross leasable floor area (individually or combined, in a single building) where the site has a frontage to a State Maintained Road</li> </ul> </li> <li>(d) the development site abuts an Activity Centre and all the following are satisfied: <ul style="list-style-type: none"> <li>(i) it does not exceed 200m<sup>2</sup> gross leasable floor area (individually or combined, in a single building)</li> <li>(ii) the proposed development will not result in a</li> </ul> </li> </ul>

	combined gross leasable floor area (existing and proposed) of all shops, consulting rooms and offices that abut the Activity Centre in this zone exceeding the lesser of the following: <ul style="list-style-type: none"> <li>A. 50% of the existing gross leasable floor area within the Activity Centre</li> <li>B. 1000m<sup>2</sup>.</li> </ul>
PO 1.3  Non-residential development sited and designed to complement the residential character and amenity of the neighbourhood.	DTS/DPF 1.3  None are applicable.
PO 1.4  Non-residential development located and designed to improve community accessibility to services, primarily in the form of: <ul style="list-style-type: none"> <li>(a) small scale commercial uses such as offices, shops and consulting rooms</li> <li>(b) community services such as educational establishments, community centres, places of worship, pre-schools, childcare and other health and welfare services</li> <li>(c) services and facilities ancillary to the function or operation of supported accommodation or retirement facilities</li> <li>(d) open space and recreation facilities.</li> </ul>	DTS/DPF 1.4  None are applicable.
PO 1.5  Expansion of existing community services such as educational establishments, community facilities and pre-schools in a manner which complements the scale of development envisaged by the desired outcome for the neighbourhood.	DTS/DPF 1.5  Alteration of or addition to existing educational establishments, community facilities or pre-schools where all the following are satisfied: <ul style="list-style-type: none"> <li>(a) set back at least 3m from any boundary shared with a residential land use</li> <li>(b) building height not exceeding 1 building level</li> <li>(c) the total floor area of the building not exceeding 150% of the total floor area prior to the addition/alteration</li> <li>(d) off-street vehicular parking exists or will be provided in accordance with the rate(s) specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas to the nearest whole number.</li> </ul>
Site Dimensions and Land Division	
PO 2.1  Allotments/sites for residential purposes are of suitable size and dimension to accommodate the anticipated dwelling form and are compatible with the prevailing development pattern in the locality.	DTS/DPF 2.1  Development will not result in more than 1 dwelling on an existing allotment  or  Development involves the conversion of an existing dwelling into two or more dwellings and the existing dwelling retains its original external appearance to the public road  or  Allotments/sites for residential purposes accord with the

	<p>following:</p> <p>(a) site areas (or allotment areas in the case of land division) are not less than the following (average site area per dwelling, including common areas, applies for group dwellings or dwellings within a residential flat building):</p> <table><tr><th>Minimum Site Area</th></tr><tr><td>Minimum site area for a detached dwelling is 500 sqm; semi-detached dwelling is 500 sqm; row dwelling is 500 sqm</td></tr></table> <p>and</p> <p>(b) site frontages (or allotment frontages in the case of land division) are not less than:</p> <table><tr><th>Minimum Frontage</th></tr><tr><td>Minimum frontage for a detached dwelling is 15m; semi-detached dwelling is 15m; row dwelling is 15m</td></tr></table> <p>In relation to DTS/DPF 2.1, in instances where:</p> <p>(c) more than one value is returned in the same field, refer to the <i>Minimum Frontage Technical and Numeric Variation</i> layer or <i>Minimum Site Area Technical and Numeric Variation</i> layer in the SA planning database to determine the applicable value relevant to the site of the proposed development</p> <p>(d) no value is returned in (a) or (b) (i.e. there is a blank field or the relevant dwelling type is not listed), then none are applicable and the relevant development cannot be classified as deemed-to-satisfy.</p>	Minimum Site Area	Minimum site area for a detached dwelling is 500 sqm; semi-detached dwelling is 500 sqm; row dwelling is 500 sqm	Minimum Frontage	Minimum frontage for a detached dwelling is 15m; semi-detached dwelling is 15m; row dwelling is 15m
Minimum Site Area					
Minimum site area for a detached dwelling is 500 sqm; semi-detached dwelling is 500 sqm; row dwelling is 500 sqm					
Minimum Frontage					
Minimum frontage for a detached dwelling is 15m; semi-detached dwelling is 15m; row dwelling is 15m					
<p>PO 2.2</p> <p>Development creating new allotments/sites in conjunction with retention of an existing dwelling ensures the site of the existing dwelling remains fit for purpose.</p>	<p>DTS/DPF 2.2</p> <p>Where the site of a dwelling does not comprise an entire allotment:</p> <p>(a) the balance of the allotment accords with the requirements specified in Established Neighbourhood Zone DTS/DPF 2.1, with 10% reduction in minimum site area where located in a Character Area Overlay or Historic Area Overlay</p> <p>(b) if there is an existing dwelling on the allotment that will remain on the allotment after completion of the development it will not contravene:</p> <p>(i) private open space requirements specified in Design in Urban Areas Table 1 - Private Open Space</p> <p>(ii) car parking requirements specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas to the nearest whole number.</p>				
<p>Site coverage</p>					
<p>PO 3.1</p> <p>Building footprints are consistent with the character and pattern of the neighbourhood and provide sufficient space around buildings to limit visual impact, provide an attractive outlook and</p>	<p>DTS/DPF 3.1</p> <p>Development does not result in site coverage exceeding:</p> <table><tr><th>Site Coverage</th></tr></table>	Site Coverage			
Site Coverage					



access to light and ventilation.	<table><tr><td>Maximum site coverage is 50 per cent</td></tr></table> <p>In instances where:</p> <ul style="list-style-type: none"><li>(a) no value is returned (i.e. there is a blank field), then a maximum 50% site coverage applies</li><li>(b) more than one value is returned in the same field, refer to the Site Coverage Technical and Numeric Variation layer in the SA planning database to determine the applicable value relevant to the site of the proposed development.</li></ul>	Maximum site coverage is 50 per cent			
Maximum site coverage is 50 per cent					
Building Height					
<p>PO 4.1</p> <p>Buildings contribute to the prevailing character of the neighbourhood and complements the height of nearby buildings.</p>	<p>DTS/DPF 4.1</p> <p>Building height (excluding garages, carports and outbuildings) is no greater than:</p> <ul style="list-style-type: none"><li>(a) the following:</li></ul> <table><tr><td>Maximum Building Height (Metres)</td></tr><tr><td>Maximum building height is 5.7m</td></tr><tr><td>Maximum Building Height (Levels)</td></tr><tr><td>Maximum building height is 1 level</td></tr></table> <ul style="list-style-type: none"><li>(b) in all other cases (i.e. there are blank fields for both maximum building height (metres) and maximum building height (levels)) - 2 building levels up to a height of 9m.</li></ul> <p>In relation to DTS/DPF 4.1, in instances where:</p> <ul style="list-style-type: none"><li>(c) more than one value is returned in the same field, refer to the <i>Maximum Building Height (Levels) Technical and Numeric Variation layer or Maximum Building Height (Meters) Technical and Numeric Variation layer</i> in the SA planning database to determine the applicable value relevant to the site of the proposed development.</li><li>(d) only one value is returned for DTS/DPF 4.1(a) (i.e. there is one blank field), then the relevant height in metres or building levels applies with no criteria for the other.</li></ul>	Maximum Building Height (Metres)	Maximum building height is 5.7m	Maximum Building Height (Levels)	Maximum building height is 1 level
Maximum Building Height (Metres)					
Maximum building height is 5.7m					
Maximum Building Height (Levels)					
Maximum building height is 1 level					
<p>PO 4.2</p> <p>Additions and alterations do not adversely impact on the streetscape character.</p>	<p>DTS/DPF 4.2</p> <p>Additions and alterations:</p> <ul style="list-style-type: none"><li>(a) are fully contained within the roof space of a building with no external alterations made to the building elevation facing the primary street</li><li>or</li><li>(b) meet all of the following:<ul style="list-style-type: none"><li>(i) do not include any development forward of the front façade building line</li><li>(ii) where including a second or subsequent building level addition, does not project beyond a 45 degree angle measured from ground level at the building line of the existing building.</li></ul></li></ul>				
Primary Street Setback					

<p>PO 5.1</p> <p>Buildings are set back from primary street boundaries consistent with the existing streetscape.</p>	<p>DTS/DPF 5.1</p> <p>The building line of a building is set back from the primary street boundary:</p> <p>(a) at least the average setback to the building line of existing buildings on adjoining sites which face the same primary street (including those buildings that would adjoin the site if not separated by a public road or a vacant allotment)</p> <p>(b) where there is only one existing building on adjoining sites which face the same primary street (including those that would adjoin if not separated by a public road or a vacant allotment), not less than the setback to the building line of that building</p> <p>or</p> <p>(c) in all other cases, no DTS/DPF is applicable.</p>	
<p>Secondary Street Setback</p>		
<p>PO 6.1</p> <p>Buildings are set back from secondary street boundaries (not being a rear laneway) to maintain the established pattern of separation between buildings and public streets and reinforce streetscape character.</p>	<p>DTS/DPF 6.1</p> <p>Building walls are set back from the secondary street boundary (other than a rear laneway):</p> <p>(a) no less than:</p> <table><tr><td><p><b>Minimum Side Boundary Setback</b></p><p>Minimum side boundary setback is 1m for the first building level; 3m for any second building level or higher</p></td></tr></table> <p>or</p> <p>(b) 900mm, whichever is greater</p> <p>or</p> <p>(c) if a dwelling on any adjoining allotment is closer to the secondary street, the distance of that dwelling from the boundary with the secondary street.</p> <p>In instances where no value is returned in DTS/DPF 6.1(a) (i.e. there is a blank field), then it is taken that the value for DTS/DPF 6.1(a) is zero.</p>	<p><b>Minimum Side Boundary Setback</b></p> <p>Minimum side boundary setback is 1m for the first building level; 3m for any second building level or higher</p>
<p><b>Minimum Side Boundary Setback</b></p> <p>Minimum side boundary setback is 1m for the first building level; 3m for any second building level or higher</p>		
<p>Boundary Walls</p>		
<p>PO 7.1</p> <p>Dwelling boundary walls are limited in height and length to manage visual and overshadowing impacts on adjoining properties.</p>	<p>DTS/DPF 7.1</p> <p>Dwellings do not incorporate side boundary walls where a side boundary setback value is returned in (a) below:</p> <p>(a)</p> <table><tr><td><p><b>Minimum Side Boundary Setback</b></p><p>Minimum side boundary setback is 1m for the first building level; 3m for any second building level or higher</p></td></tr></table> <p>or</p> <p>(b) where no side boundary setback value is returned in (a) above, and except where the dwelling is located on a</p>	<p><b>Minimum Side Boundary Setback</b></p> <p>Minimum side boundary setback is 1m for the first building level; 3m for any second building level or higher</p>
<p><b>Minimum Side Boundary Setback</b></p> <p>Minimum side boundary setback is 1m for the first building level; 3m for any second building level or higher</p>		

	<p>central site within a row dwelling or terrace arrangement, side boundary walls occur only on one side boundary and satisfy (i) or (ii) below:</p> <p>(i) side boundary walls adjoin or abut a boundary wall of a building on adjoining land for the same or lesser length and height</p> <p>(ii) side boundary walls do not:</p> <p>A. exceed 3.2m in height from the lower of the natural or finished ground level</p> <p>B. exceed 8m in length</p> <p>C. when combined with other walls on the boundary of the subject development site, exceed a maximum 45% of the length of the boundary</p> <p>D. encroach within 3m of any other existing or proposed boundary walls on the subject land.</p>		
<p>PO 7.2</p> <p>Dwellings in a semi-detached, row or terrace arrangement maintain space between buildings consistent with a low density suburban streetscape character.</p>	<p>DTS/DPF 7.2</p> <p>Dwellings in a semi-detached, row or terrace arrangement are setback from side boundaries shared with allotments outside the development site at least the minimum distance identified in Established Neighbourhood Zone DTS/DPF 8.1.</p>		
<p>Side Boundary Setback</p>			
<p>PO 8.1</p> <p>Buildings are set back from side boundaries to provide:</p> <p>(a) separation between buildings in a way that complements the established character of the locality</p> <p>(b) access to natural light and ventilation for neighbours.</p>	<p>DTS/DPF 8.1</p> <p>Other than walls located on a side boundary in accordance with Established Neighbourhood Zone DTS/DPF 7.1, building walls are set back from the side boundary:</p> <p>(a) no less than:</p> <table><tr><td><p><b>Minimum Side Boundary Setback</b></p><p>Minimum side boundary setback is 1m for the first building level; 3m for any second building level or higher</p></td></tr><tr><td><p>(b) in all other cases (i.e. there is a blank field), then:</p><p>(i) at least 900mm where the wall is up to 3m</p><p>(ii) other than for a south facing wall, at least 900mm plus 1/3 of the wall height above 3m</p><p>(iii) at least 1.9m plus 1/3 of the wall height above 3m for south facing walls.</p></td></tr></table>	<p><b>Minimum Side Boundary Setback</b></p> <p>Minimum side boundary setback is 1m for the first building level; 3m for any second building level or higher</p>	<p>(b) in all other cases (i.e. there is a blank field), then:</p> <p>(i) at least 900mm where the wall is up to 3m</p> <p>(ii) other than for a south facing wall, at least 900mm plus 1/3 of the wall height above 3m</p> <p>(iii) at least 1.9m plus 1/3 of the wall height above 3m for south facing walls.</p>
<p><b>Minimum Side Boundary Setback</b></p> <p>Minimum side boundary setback is 1m for the first building level; 3m for any second building level or higher</p>			
<p>(b) in all other cases (i.e. there is a blank field), then:</p> <p>(i) at least 900mm where the wall is up to 3m</p> <p>(ii) other than for a south facing wall, at least 900mm plus 1/3 of the wall height above 3m</p> <p>(iii) at least 1.9m plus 1/3 of the wall height above 3m for south facing walls.</p>			
<p>Rear Boundary Setback</p>			
<p>PO 9.1</p> <p>Buildings are set back from rear boundaries to provide:</p> <p>(a) separation between dwellings in a way that complements the established character of the locality</p> <p>(b) access to natural light and ventilation for neighbours</p> <p>(c) private open space</p> <p>(d) space for landscaping and vegetation.</p>	<p>DTS/DPF 9.1</p> <p>Other than in relation to an access lane way, buildings are set back from the rear boundary at least:</p> <p>(a) 4m for the first building level</p> <p>(b) 6m for any second building level.</p>		
<p>Appearance</p>			



<p>PO 10.1</p> <p>Garages and carports are designed and sited to be discrete and not dominate the appearance of the associated dwelling when viewed from the street.</p>	<p>DTS/DPF 10.1</p> <p>Garages and carports facing a street (other than an access lane way):</p> <ul style="list-style-type: none"> <li>(a) are set back at least 0.5m behind the building line of the associated dwelling</li> <li>(b) are set back at least 5.5m from the boundary of the primary street</li> <li>(c) have a total garage door / opening width not exceeding 30% of the allotment or site frontage, to a maximum width of 7m.</li> </ul>
<p>PO 10.2</p> <p>The appearance of development as viewed from public roads is sympathetic to the wall height, roof forms and roof pitches of the predominant housing stock in the locality.</p>	<p>DTS/DPF 10.2</p> <p>None are applicable.</p>
Ancillary buildings and structures	
<p>PO 11.1</p> <p>Residential ancillary buildings and structures are sited and designed to not detract from the streetscape or appearance of buildings on the site or neighbouring properties.</p>	<p>DTS/DPF 11.1</p> <p>Ancillary buildings and structures:</p> <ul style="list-style-type: none"> <li>(a) are ancillary to a dwelling erected on the same site</li> <li>(b) have a floor area not exceeding 60m<sup>2</sup></li> <li>(c) are constructed, added to or altered so that they are situated at least <ul style="list-style-type: none"> <li>(i) 500mm behind the building line of the dwelling to which they are ancillary or</li> <li>(ii) 900mm from a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads)</li> </ul> </li> <li>(d) in the case of a garage or carport, the garage or carport: <ul style="list-style-type: none"> <li>(i) is set back at least 5.5m from the boundary of the primary street</li> <li>(ii) when facing a primary street or secondary street has a total door/opening not exceeding 7m or 30% of the site frontage (whichever is the lesser) when facing a primary street or secondary street</li> </ul> </li> <li>(e) if situated on a boundary (not being a boundary with a primary street or secondary street), a length not exceeding 8m unless: <ul style="list-style-type: none"> <li>(i) a longer wall or structure exists on the adjacent site and is situated on the same allotment boundary and</li> <li>(ii) the proposed wall or structure will be built along the same length of boundary as the existing adjacent wall or structure to the same or lesser extent</li> </ul> </li> <li>(f) if situated on a boundary of the allotment (not being a boundary with a primary street or secondary street), all walls or structures on the boundary not exceeding 45% of the length of that boundary</li> <li>(g) will not be located within 3m of any other wall along the same boundary unless on an adjacent site on that</li> </ul>

	<p>boundary there is an existing wall of a building that would be adjacent to or abut the proposed wall or structure</p> <p>(h) have a wall height or post height not exceeding 3m above natural ground level, and where located to the side of the associated dwelling, have a wall height or post height no higher than the wall height of the associated dwelling</p> <p>(i) have a roof height where no part of the roof is more than 5m above the natural ground level</p> <p>(j) if clad in sheet metal, are pre-colour treated or painted in a non-reflective colour.</p> <p>(k) retains a total area of soft landscaping in accordance with (i) or (ii), whichever is less:</p> <p>(i) a total area as determined by the following table:</p> <table border="1"> <thead> <tr> <th>Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m<sup>2</sup>)</th><th>Minimum percentage of site</th></tr> </thead> <tbody> <tr> <td>&lt;150</td><td>10%</td></tr> <tr> <td>150-200</td><td>15%</td></tr> <tr> <td>201-450</td><td>20%</td></tr> <tr> <td>&gt;450</td><td>25%</td></tr> </tbody> </table> <p>(ii) the amount of existing soft landscaping prior to the development occurring.</p>	Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m <sup>2</sup> )	Minimum percentage of site	<150	10%	150-200	15%	201-450	20%	>450	25%
Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m <sup>2</sup> )	Minimum percentage of site										
<150	10%										
150-200	15%										
201-450	20%										
>450	25%										
<p>PO 11.2</p> <p>Ancillary buildings and structures do not impede on-site functional requirements such as private open space provision, car parking requirements or result in over-development of the site.</p>	<p>DTS/DPF 11.2</p> <p>Ancillary buildings and structures do not result in:</p> <p>(a) less private open space than specified in Design in Urban Areas Table 1 - Private Open Space</p> <p>(b) less on-site car parking than specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.</p>										
Advertisements											
<p>PO 12.1</p> <p>Advertisements identify the associated business activity, and do not detract from the residential character of the locality.</p>	<p>DTS/DPF 12.1</p> <p>Advertisements relating to a lawful business activity associated with a residential use do not exceed 0.3m<sup>2</sup> and mounted flush with a wall or fence.</p>										

### Table 5 - Procedural Matters (PM) - Notification

The following table identifies, pursuant to section 107(6) of the *Planning, Development and Infrastructure Act 2016*, classes of performance assessed development that are excluded from notification. The table also identifies any exemptions to the placement of notices when notification is required.

#### Interpretation

Notification tables exclude the classes of development listed in Column A from notification provided that they do not fall within a corresponding exclusion prescribed in Column B.

Where a development or an element of a development falls within more than one class of development listed in Column A, it will be excluded from notification if it is excluded (in its entirety) under any of those classes of development. It need not be excluded under all applicable classes of development.

Where a development involves multiple performance assessed elements, all performance assessed elements will require notification (regardless of whether one or more elements are excluded in the applicable notification table) unless every performance assessed element of the application is excluded in the applicable notification table, in which case the application will not require notification.

Class of Development (Column A)	Exceptions (Column B)
1. Development which, in the opinion of the relevant authority, is of a minor nature only and will not unreasonably impact on the owners or occupiers of land in the locality of the site of the development.	None specified.
2. All development undertaken by: <ul style="list-style-type: none"> <li>(a) the South Australian Housing Trust either individually or jointly with other persons or bodies</li> <li>or</li> <li>(b) a provider registered under the Community Housing National Law participating in a program relating to the renewal of housing endorsed by the South Australian Housing Trust.</li> </ul>	Except development involving any of the following: <ul style="list-style-type: none"> <li>1. residential flat building(s) of 3 or more building levels</li> <li>2. the demolition of a State or Local Heritage Place</li> <li>3. the demolition of a building (except an ancillary building) in a Historic Area Overlay.</li> </ul>
3. Any development involving any of the following (or of any combination of any of the following): <ul style="list-style-type: none"> <li>(a) air handling unit, air conditioning system or exhaust fan</li> <li>(b) ancillary accommodation</li> <li>(c) building work on railway land</li> <li>(d) carport</li> <li>(e) deck</li> <li>(f) dwelling</li> <li>(g) dwelling addition</li> <li>(h) fence</li> <li>(i) outbuilding</li> <li>(j) pergola</li> <li>(k) private bushfire shelter</li> <li>(l) residential flat building</li> <li>(m) retaining wall</li> <li>(n) shade sail</li> <li>(o) solar photovoltaic panels (roof mounted)</li> <li>(p) swimming pool or spa pool</li> <li>(q) verandah</li> <li>(r) water tank.</li> </ul>	Except development that: <ul style="list-style-type: none"> <li>1. exceeds the maximum building height specified in Established Neighbourhood Zone DTS/DPF 4.1 or</li> <li>2. involves a building wall (or structure) that is proposed to be situated on (or abut) an allotment boundary (not being a boundary with a primary street or secondary street or an excluded boundary) and: <ul style="list-style-type: none"> <li>(a) the length of the proposed wall (or structure) exceeds 8m (other than where the proposed wall abuts an existing wall or structure of greater length on the adjoining allotment) or</li> <li>(b) the height of the proposed wall (or post height) exceeds 3.2m measured from the lower of the natural or finished ground level (other than where the proposed wall abuts an existing wall or structure of greater height on the adjoining allotment).</li> </ul> </li> </ul>
4. Any development involving any of the following (or of any combination of any of the following): <ul style="list-style-type: none"> <li>(a) consulting room</li> <li>(b) office</li> <li>(c) shop.</li> </ul>	Except development that: <ul style="list-style-type: none"> <li>1. does not satisfy Established Neighbourhood Zone DTS/DPF 1.2 or</li> <li>2. exceeds the maximum building height specified</li> </ul>



	<p>in Established Neighbourhood Zone DTS/DPF 4.1 or</p> <p>3. involves a building wall (or structure) that is proposed to be situated on (or abut) an allotment boundary (not being a boundary with a primary street or secondary street or an excluded boundary) and:</p> <ul style="list-style-type: none"> <li>(a) the length of the proposed wall (or structure) exceeds 8m (other than where the proposed wall abuts an existing wall or structure of greater length on the adjoining allotment) or</li> <li>(b) the height of the proposed wall (or post height) exceeds 3.2m measured from the lower of the natural or finished ground level (other than where the proposed wall abuts an existing wall or structure of greater height on the adjoining allotment).</li> </ul>
<p>5. Any of the following (or of any combination of any of the following):</p> <ul style="list-style-type: none"> <li>(a) internal building works</li> <li>(b) land division</li> <li>(c) recreation area</li> <li>(d) replacement building</li> <li>(e) temporary accommodation in an area affected by bushfire</li> <li>(f) tree damaging activity.</li> </ul>	None specified.
6. Demolition.	<p>Except any of the following:</p> <ul style="list-style-type: none"> <li>1. the demolition of a State or Local Heritage Place</li> <li>2. the demolition of a building (except an ancillary building) in a Historic Area Overlay.</li> </ul>
<b>Placement of Notices - Exemptions for Performance Assessed Development</b>	
None specified.	
<b>Placement of Notices - Exemptions for Restricted Development</b>	
None specified.	

## Part 3 - Overlays

### Airport Building Heights (Regulated) Overlay

#### Assessment Provisions (AP)

Desired Outcome	
DO 1	Management of potential impacts of buildings and generated emissions to maintain operational and safety requirements of registered and certified commercial and military airfields, airports, airstrips and helicopter landing

sites.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1  Building height does not pose a hazard to the operation of a certified or registered aerodrome.	DTS/DPF 1.1  Buildings are located outside the area identified as 'All structures' (no height limit is prescribed) and do not exceed the height specified in the Airport Building Heights (Regulated) Overlay which applies to the subject site as shown on the SA Property and Planning Atlas.  In instances where more than one value applies to the site, the lowest value relevant to the site of the proposed development is applicable.
PO 1.2  Exhaust stacks are designed and sited to minimise plume impacts on aircraft movements associated with a certified or registered aerodrome.	DTS/DPF 1.2  Development does not include exhaust stacks.

### Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Any of the following classes of development:  (a) building located in an area identified as 'All structures' (no height limit is prescribed) or will exceed the height specified in the <i>Airport Building Heights (Regulated) Overlay</i>  (b) building comprising exhaust stacks that generates plumes, or may cause plumes to be generated, above a height specified in the <i>Airport Building Heights (Regulated) Overlay</i> .	The airport-operator company for the relevant airport within the meaning of the <i>Airports Act 1996</i> of the Commonwealth or, if there is no airport-operator company, the Secretary of the Minister responsible for the administration of the <i>Airports Act 1996</i> of the Commonwealth.	To provide expert assessment and direction to the relevant authority on potential impacts on the safety and operation of aviation activities.	Development of a class to which Schedule 9 clause 3 item 1 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

### Historic Area Overlay

### Assessment Provisions (AP)

## Desired Outcome

DO 1	Historic themes and characteristics are reinforced through conservation and contextually responsive development, design and adaptive reuse that responds to existing coherent patterns of land division, site configuration, streetscapes, building siting and built scale, form and features as exhibited in the Historic Area and expressed in the Historic Area Statement.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All Development	
PO 1.1  All development is undertaken having consideration to the historic streetscapes and built form as expressed in the Historic Area Statement.	DTS/DPF 1.1  None are applicable.
Built Form	
PO 2.1  The form and scale of new buildings and structures that are visible from the public realm are consistent with the prevailing historic characteristics of the historic area.	DTS/DPF 2.1  None are applicable.
PO 2.2  Development is consistent with the prevailing building and wall heights in the historic area.	DTS/DPF 2.2  None are applicable.
PO 2.3  Design and architectural detailing of street-facing buildings (including but not limited to roof pitch and form, openings, chimneys and verandahs) complement the prevailing characteristics in the historic area.	DTS/DPF 2.3  None are applicable.
PO 2.4  Development is consistent with the prevailing front and side boundary setback pattern in the historic area.	DTS/DPF 2.4  None are applicable.
PO 2.5  Materials are either consistent with or complement those within the historic area.	DTS/DPF 2.5  None are applicable.
Alterations and additions	
PO 3.1  Alterations and additions complement the subject building, employ a contextual design approach and are sited to ensure they do not dominate the primary façade.	DTS/DPF 3.1  Alterations and additions are fully contained within the roof space of an existing building with no external alterations made to the building elevation facing the primary street.



PO 3.2	DTS/DPF 3.2
Adaptive reuse and revitalisation of buildings to support retention consistent with the Historic Area Statement.	None are applicable.
Ancillary development	
PO 4.1	DTS/DPF 4.1
Ancillary development, including carports, outbuildings and garages, complements the historic character of the area and associated buildings.	None are applicable.
PO 4.2	DTS/DPF 4.2
Ancillary development, including carports, outbuildings and garages, is located behind the building line of the principal building(s) and does not dominate the building or its setting.	None are applicable.
PO 4.3	DTS/DPF 4.3
Advertising and advertising hoardings are located and designed to complement the building, be unobtrusive, be below the parapet line, not conceal or obstruct significant architectural elements and detailing, or dominate the building or its setting.	None are applicable.
PO 4.4	DTS/DPF 4.4
Fencing and gates closer to a street boundary (other than a laneway) than the elevation of the associated building are consistent with the traditional period, style and form of the associated building.	None are applicable.
Land Division	
PO 5.1	DTS/DPF 5.1
Land division creates allotments that are: <ul style="list-style-type: none"> <li>(a) compatible with the surrounding pattern of subdivision in the historic area</li> <li>(b) of a dimension to accommodate buildings of a bulk and scale that reflect existing buildings and setbacks in the historic area</li> </ul>	None are applicable.
Context and Streetscape Amenity	
PO 6.1	DTS/DPF 6.1
The width of driveways and other vehicle access ways are consistent with the prevailing width of existing driveways of the historic area.	None are applicable.
PO 6.2	DTS/DPF 6.2
Development maintains the valued landscape patterns and characteristics that contribute to the historic area, except where they compromise safety, create nuisance, or impact adversely on buildings or infrastructure.	None are applicable.
Demolition	
PO 7.1	DTS/DPF 7.1

Buildings and structures, or features thereof, that demonstrate the historic characteristics as expressed in the Historic Area Statement are not demolished, unless: <ul style="list-style-type: none"> <li>(a) the front elevation of the building has been substantially altered and cannot be reasonably restored in a manner consistent with the building's original style or</li> <li>(b) the structural integrity or safe condition of the original building is beyond reasonable repair.</li> </ul>	None are applicable.
PO 7.2 Partial demolition of a building where that portion to be demolished does not contribute to the historic character of the streetscape.	DTS/DPF 7.2 None are applicable.
PO 7.3 Buildings or elements of buildings that do not conform with the values described in the Historic Area Statement may be demolished.	DTS/DPF 7.3 None are applicable.
Ruins	
PO 8.1 Development conserves and complements features and ruins associated with former activities of significance.	DTS/DPF 8.1 None are applicable.

## Historic Area Statements

Statement#	Statement						
<b>Historic Areas affecting City of Unley</b>							
	<p><b>Residential Compact Parkside (South) Historic Area Statement (Un27)</b></p> <p>The Historic Area Overlay identifies localities that comprise characteristics of an identifiable historic, economic and / or social theme of recognised importance. They can comprise land divisions, development patterns, built form characteristics and natural features that provide a legible connection to the historic development of a locality.</p> <p>These attributes have been identified in the below table. In some cases State and / or Local Heritage Places within the locality contribute to the attributes of an Historic Area.</p> <p>The preparation of an Historic Impact Statement can assist in determining potential additional attributes of an Historic Area where these are not stated in the below table.</p> <table border="1"> <tr> <td>Eras, themes and context</td><td>1880 to 1930 built development.</td></tr> <tr> <td>Allotments, subdivision and built form patterns</td><td>Simple grid layout pattern of roads, with longitudinal axis perpendicular to narrow roads. Regular large allotment sizes and site frontages. Prevailing and coherent rhythm of building siting, street setbacks, side boundary setbacks, spacing between buildings and garden landscape setting.</td></tr> <tr> <td>Architectural styles, detailing and built form features</td><td>Victorian and Turn-of-the-Century double-fronted, single-fronted as well as attached cottages. Victorian and Turn-of-the-Century symmetrical and asymmetrical villas. Inter-War Bungalows. Hipped and gable roof forms, chimneys, open verandahs, feature ornamentation (plasterwork, ironwork and timberwork), lattice work and associated front fences. Carports, garages and side additions are separate and recessed from the main</td></tr> </table>	Eras, themes and context	1880 to 1930 built development.	Allotments, subdivision and built form patterns	Simple grid layout pattern of roads, with longitudinal axis perpendicular to narrow roads. Regular large allotment sizes and site frontages. Prevailing and coherent rhythm of building siting, street setbacks, side boundary setbacks, spacing between buildings and garden landscape setting.	Architectural styles, detailing and built form features	Victorian and Turn-of-the-Century double-fronted, single-fronted as well as attached cottages. Victorian and Turn-of-the-Century symmetrical and asymmetrical villas. Inter-War Bungalows. Hipped and gable roof forms, chimneys, open verandahs, feature ornamentation (plasterwork, ironwork and timberwork), lattice work and associated front fences. Carports, garages and side additions are separate and recessed from the main
Eras, themes and context	1880 to 1930 built development.						
Allotments, subdivision and built form patterns	Simple grid layout pattern of roads, with longitudinal axis perpendicular to narrow roads. Regular large allotment sizes and site frontages. Prevailing and coherent rhythm of building siting, street setbacks, side boundary setbacks, spacing between buildings and garden landscape setting.						
Architectural styles, detailing and built form features	Victorian and Turn-of-the-Century double-fronted, single-fronted as well as attached cottages. Victorian and Turn-of-the-Century symmetrical and asymmetrical villas. Inter-War Bungalows. Hipped and gable roof forms, chimneys, open verandahs, feature ornamentation (plasterwork, ironwork and timberwork), lattice work and associated front fences. Carports, garages and side additions are separate and recessed from the main						

Un27		building and façade, and are a minor, unobtrusive presence in the streetscape.
	Building height	Wall Height in the order of 3.5 metres. Total Roof Height in the order of 5.7 metres; and Roof Pitch in the order of 27 degrees and 35 degrees. Verandahs in the order of 2.1 metre fascia height and 3.0 metre pitching height. Consistent and recognisable pattern of traditional building proportions including wall heights and widths of facades, and roof height, volumes and shapes associated with the identified architectural styles.
	Materials	Sandstone. Bluestone. Timber joinery including window frames, door frames, doors, fascias, bargeboards and verandah posts. Brick quoins, occasionally rendered, around windows and doors. Brick or rendered string courses and plinths. Rendered masonry. Corrugated iron roof cladding. Tiled roof cladding on some post 1900s buildings.
	Fencing	Typical of the historic character of the area, street and architectural style and materials of the associated building. Where forward of the front façade of the principle building, low in height, typically less than 1.0 metre but up to 1.2 metres. Larger sites and of more than 16 metres street frontage may include vertical elements up to 1.8 metres in total height. Open, see-through and maintaining an open streetscape presence of the associated building, including typical styles comprising: Timber picket, dowel or paling with top rail; Corrugated iron or mini orb or steel strap panels within timber framing and posts; Woven crimped wire, wire mesh on timber or galvanised steel tube framing; Simple masonry plinth (500mm) and widely spaced minimum numbers of piers with decorative see-through iron palisade or steel bar inserts; Stone, brick and/or stucco masonry low in height with wrought iron or steel bar inserts (typically geometric pattern); hedges, with or without fencing.
	Setting, landscaping, streetscape and public realm features	Compact streetscape character. Simple grid of short and narrow streets. Narrow verges. Modest street trees.
	Representative Buildings	[Not identified]

### Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

### Prescribed Wells Area Overlay

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Sustainable water use in prescribed wells areas.



Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>All development, but in particular involving any of the following:</p> <ul style="list-style-type: none"> <li>(a) horticulture</li> <li>(b) activities requiring irrigation</li> <li>(c) aquaculture</li> <li>(d) industry</li> <li>(e) intensive animal husbandry</li> <li>(f) commercial forestry</li> </ul> <p>has a lawful, sustainable and reliable water supply that does not place undue strain on water resources in prescribed wells areas.</p>	<p>DTS/DPF 1.1</p> <p>Development satisfies either of the following:</p> <ul style="list-style-type: none"> <li>(a) the applicant has a current water licence in which sufficient spare capacity exists to accommodate the water needs of the proposed use or</li> <li>(b) the proposal does not involve the taking of water for which a licence would be required under the <i>Landscape South Australia Act 2019</i>.</li> </ul>

### Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Any of the following classes of development that require or may require water to be taken in addition to any allocation that has already been granted under the <i>Landscape South Australia Act 2019</i>:</p> <ul style="list-style-type: none"> <li>(a) horticulture</li> <li>(b) activities requiring irrigation</li> <li>(c) aquaculture</li> <li>(d) industry</li> <li>(e) intensive animal husbandry</li> <li>(f) commercial forestry.</li> </ul>	<p>The Chief Executive of the Department of the Minister responsible for the administration of the <i>Landscape South Australia Act 2019</i>.</p>	<p>To provide expert technical assessment and direction to the relevant authority on the taking of water to ensure development is undertaken sustainably.</p>	<p>Development of a class to which Schedule 9 clause 3 item 13 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.</p>
<p>Commercial forestry that requires a forest water licence under Part 8 Division 6 of the <i>Landscape South Australia Act 2019</i>.</p>			

### Regulated and Significant Tree Overlay

#### Assessment Provisions (AP)

Desired Outcome	
DO 1	Conservation of regulated and significant trees to provide aesthetic and environmental benefits and mitigate tree loss.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Tree Retention and Health	
<p>PO 1.1</p> <p>Regulated trees are retained where they:</p> <ul style="list-style-type: none"> <li>(a) make an important visual contribution to local character and amenity</li> <li>(b) are indigenous to the local area and listed under the <i>National Parks and Wildlife Act 1972</i> as a rare or endangered native species and / or</li> <li>(c) provide an important habitat for native fauna.</li> </ul>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Significant trees are retained where they:</p> <ul style="list-style-type: none"> <li>(a) make an important contribution to the character or amenity of the local area</li> <li>(b) are indigenous to the local area and are listed under the <i>National Parks and Wildlife Act 1972</i> as a rare or endangered native species</li> <li>(c) represent an important habitat for native fauna</li> <li>(d) are part of a wildlife corridor of a remnant area of native vegetation</li> <li>(e) are important to the maintenance of biodiversity in the local environment and / or</li> <li>(f) form a notable visual element to the landscape of the local area.</li> </ul>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>PO 1.3</p> <p>A tree damaging activity not in connection with other development satisfies (a) and (b):</p> <ul style="list-style-type: none"> <li>(a) tree damaging activity is only undertaken to: <ul style="list-style-type: none"> <li>(i) remove a diseased tree where its life expectancy is short</li> <li>(ii) mitigate an unacceptable risk to public or private safety due to limb drop or the like</li> <li>(iii) rectify or prevent extensive damage to a building of value as comprising any of the following: <ul style="list-style-type: none"> <li>A. a Local Heritage Place</li> <li>B. a State Heritage Place</li> <li>C. a substantial building of value</li> </ul> </li> </ul> </li> </ul> <p>and there is no reasonable alternative to rectify</p>	<p>DTS/DPF 1.3</p> <p>None are applicable.</p>

<p>or prevent such damage other than to undertake a tree damaging activity</p> <p>(iv) reduce an unacceptable hazard associated with a tree within 20m of an existing residential, tourist accommodation or other habitable building from bushfire</p> <p>(v) treat disease or otherwise in the general interests of the health of the tree and / or</p> <p>(vi) maintain the aesthetic appearance and structural integrity of the tree</p> <p>(b) in relation to a significant tree, tree-damaging activity is avoided unless all reasonable remedial treatments and measures have been determined to be ineffective.</p>	
<p>PO 1.4</p> <p>A tree-damaging activity in connection with other development satisfies all the following:</p> <p>(a) it accommodates the reasonable development of land in accordance with the relevant zone or subzone where such development might not otherwise be possible</p> <p>(b) in the case of a significant tree, all reasonable development options and design solutions have been considered to prevent substantial tree-damaging activity occurring.</p>	<p>DTS/DPF 1.4</p> <p>None are applicable.</p>
Ground work affecting trees	
<p>PO 2.1</p> <p>Regulated and significant trees, including their root systems, are not unduly compromised by excavation and / or filling of land, or the sealing of surfaces within the vicinity of the tree to support their retention and health.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
Land Division	
<p>PO 3.1</p> <p>Land division results in an allotment configuration that enables its subsequent development and the retention of regulated and significant trees as far as is reasonably practicable.</p>	<p>DTS/DPF 3.1</p> <p>Land division where:</p> <p>(a) there are no regulated or significant trees located within or adjacent to the plan of division</p> <p>or</p> <p>(b) the application demonstrates that an area exists to accommodate subsequent development of proposed allotments after an allowance has been made for a tree protection zone around any regulated tree within and adjacent to the plan of division.</p>

### Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None



## Stormwater Management Overlay

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Development incorporates water sensitive urban design techniques to capture and re-use stormwater.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature									
<p>PO 1.1</p> <p>Residential development is designed to capture and re-use stormwater to:</p> <ul style="list-style-type: none"><li>(a) maximise conservation of water resources</li><li>(b) manage peak stormwater runoff flows and volume to ensure the carrying capacities of downstream systems are not overloaded</li><li>(c) manage stormwater runoff quality.</li></ul>	<p>DTS/DPF 1.1</p> <p>Residential development comprising detached, semi-detached or row dwellings, or less than 5 group dwellings or dwellings within a residential flat building:</p> <ul style="list-style-type: none"><li>(a) includes rainwater tank storage:<ul style="list-style-type: none"><li>(i) connected to at least:<ul style="list-style-type: none"><li>A. in relation to a detached dwelling (not in a battle-axe arrangement), semi-detached dwelling or row dwelling, 60% of the roof area</li><li>B. in all other cases, 80% of the roof area</li></ul></li><li>(ii) connected to either a toilet, laundry cold water outlets or hot water service for sites less than 200m<sup>2</sup></li><li>(iii) connected to one toilet and either the laundry cold water outlets or hot water service for sites of 200m<sup>2</sup> or greater</li><li>(iv) with a minimum total capacity in accordance with Table 1</li><li>(v) where detention is required, includes a 20-25 mm diameter slow release orifice at the bottom of the detention component of the tank</li></ul></li><li>(b) incorporates dwelling roof area comprising at least 80% of the site's impervious area</li></ul> <p>Table 1: Rainwater Tank</p> <table><tr><th>Site size (m<sup>2</sup>)</th><th>Minimum retention volume (Litres)</th><th>Minimum detention volume (Litres)</th></tr><tr><td>&lt;200</td><td>1000</td><td>1000</td></tr><tr><td>200-400</td><td>2000</td><td>Site perviousness &lt;30%: 1000  Site perviousness ≥30%: N/A</td></tr></table>	Site size (m <sup>2</sup> )	Minimum retention volume (Litres)	Minimum detention volume (Litres)	<200	1000	1000	200-400	2000	Site perviousness <30%: 1000  Site perviousness ≥30%: N/A
Site size (m <sup>2</sup> )	Minimum retention volume (Litres)	Minimum detention volume (Litres)								
<200	1000	1000								
200-400	2000	Site perviousness <30%: 1000  Site perviousness ≥30%: N/A								

		>401	4000	Site perviousness <35%: 1000  Site perviousness ≥35%: N/A

### Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

### Urban Tree Canopy Overlay

#### Assessment Provisions (AP)

Desired Outcome	
DO 1	Residential development preserves and enhances urban tree canopy through the planting of new trees and retention of existing mature trees where practicable.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature			
PO 1.1  Trees are planted or retained to contribute to an urban tree canopy.	DTS/DPF 1.1  Tree planting is provided in accordance with the following:			
	Site size per dwelling (m <sup>2</sup> )		Tree size* and number required per dwelling	
	<450		1 small tree	
	450-800		1 medium tree or 2 small trees	
	>800		1 large tree or 2 medium trees or 4 small trees	
	*refer Table 1 Tree Size			
	Table 1 Tree Size			
	Tree size	Mature	Mature	Soil area around tree

	height (minimum)	spread (minimum)	within development site (minimum)
Small	4 m	2m	10m <sup>2</sup> and min. dimension of 1.5m
Medium	6 m	4 m	30m <sup>2</sup> and min. dimension of 2m
Large	12 m	8m	60m <sup>2</sup> and min. dimension of 4m

The discount in Column D of Table 2 discounts the number of trees required to be planted in DTS/DPF 1.1 where existing tree(s) are retained on the subject land that meet the criteria in Columns A, B and C of Table 2, and are not a species identified in Regulation 3F(4)(b) of the Planning Development and Infrastructure (General) Regulations 2017.

Table 2 Tree Discounts

Retained tree height (Column A)	Retained tree spread (Column B)	Retained soil area around tree within development site (Column C)	Discount applied (Column D)
4-6m	2-4m	10m <sup>2</sup> and min. dimension of 1.5m	2 small trees (or 1 medium tree)
6-12m	4-8m	30m <sup>2</sup> and min. dimension of 3m	2 medium trees (or 4 small trees)
>12m	>8m	60m <sup>2</sup> and min. dimension of 6m	2 large trees (or 4 medium trees, or 8 small trees)

Note: In order to satisfy DTS/DPF 1.1, payment may be made in accordance with a relevant off-set scheme established by the Minister under section 197 of the Planning, Development and Infrastructure Act 2016, provided the provisions and requirements of that scheme are satisfied. For the purposes of section 102(4) of the Planning, Development and Infrastructure Act 2016, an applicant may elect for any of the matters in DTS/DPF 1.1 to be reserved.



## Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

## Water Resources Overlay

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Protection of the quality of surface waters considering adverse water quality impacts associated with projected reductions in rainfall and warmer air temperatures as a result of climate change.
DO 2	Maintain the conveyance function and natural flow paths of watercourses to assist in the management of flood waters and stormwater runoff.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Water Catchment	
PO 1.1 Watercourses and their beds, banks, wetlands and floodplains (1% AEP flood extent) are not damaged or modified and are retained in their natural state, except where modification is required for essential access or maintenance purposes.	DTS/DPF 1.1 None are applicable.
PO 1.2 Development avoids interfering with the existing hydrology or water regime of swamps and wetlands other than to improve the existing conditions to enhance environmental values.	DTS/DPF 1.2 None are applicable.
PO 1.3 Wetlands and low-lying areas providing habitat for native flora and fauna are not drained, except temporarily for essential management purposes to enhance environmental values.	DTS/DPF 1.3 None are applicable.
PO 1.4	DTS/DPF 1.4

Watercourses, areas of remnant native vegetation, or areas prone to erosion that are capable of natural regeneration are fenced off to limit stock access.	None are applicable.
<p>PO 1.5</p> <p>Development that increases surface water run-off includes a suitably sized strip of vegetated land on each side of a watercourse to filter runoff to:</p> <ul style="list-style-type: none"> <li>(a) reduce the impacts on native aquatic ecosystems</li> <li>(b) minimise soil loss eroding into the watercourse.</li> </ul>	<p>DTS/DPF 1.5</p> <p>A strip of land 20m or more wide measured from the top of existing banks on each side of the watercourse is free from development, livestock use and revegetated with locally indigenous vegetation.</p>
<p>PO 1.6</p> <p>Development resulting in the depositing or placing of an object or solid material in a watercourse or lake occurs only where it involves any of the following:</p> <ul style="list-style-type: none"> <li>(a) the construction of an erosion control structure</li> <li>(b) devices or structures used to extract or regulate water flowing in a watercourse</li> <li>(c) devices used for scientific purposes</li> <li>(d) the rehabilitation of watercourses.</li> </ul>	<p>DTS/DPF 1.6</p> <p>None are applicable.</p>
<p>PO 1.7</p> <p>Watercourses, floodplains (1% AEP flood extent) and wetlands protected and enhanced by retaining and protecting existing native vegetation.</p>	<p>DTS/DPF 1.7</p> <p>None are applicable.</p>
<p>PO 1.8</p> <p>Watercourses, floodplains (1% AEP flood extent) and wetlands are protected and enhanced by stabilising watercourse banks and reducing sediments and nutrients entering the watercourse.</p>	<p>DTS/DPF 1.8</p> <p>None are applicable.</p>
<p>PO 1.9</p> <p>Dams, water tanks and diversion drains are located and constructed to maintain the quality and quantity of flows required to meet environmental and downstream needs.</p>	<p>DTS/DPF 1.9</p> <p>None are applicable.</p>

### Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

## Part 4 - General Development Policies

## Advertisements

### Assessment Provisions (AP)

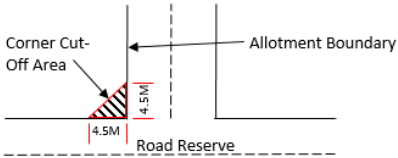
Desired Outcome	
DO 1	Advertisements and advertising hoardings are appropriate to context, efficient and effective in communicating with the public, limited in number to avoid clutter, and do not create hazard.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Appearance	
<p>PO 1.1</p> <p>Advertisements are compatible and integrated with the design of the building and/or land they are located on.</p>	<p>DTS/DPF 1.1</p> <p>Advertisements attached to a building satisfy all of the following:</p> <ul style="list-style-type: none"> <li>(a) are not located in a Neighbourhood-type zone</li> <li>(b) where they are flush with a wall: <ul style="list-style-type: none"> <li>(i) if located at canopy level, are in the form of a fascia sign</li> <li>(ii) if located above canopy level: <ul style="list-style-type: none"> <li>A. do not have any part rising above parapet height</li> <li>B. are not attached to the roof of the building</li> </ul> </li> </ul> </li> <li>(c) where they are not flush with a wall: <ul style="list-style-type: none"> <li>(i) if attached to a verandah, no part of the advertisement protrudes beyond the outer limits of the verandah structure</li> <li>(ii) if attached to a two-storey building: <ul style="list-style-type: none"> <li>A. has no part located above the finished floor level of the second storey of the building</li> <li>B. does not protrude beyond the outer limits of any verandah structure below</li> <li>C. does not have a sign face that exceeds 1m<sup>2</sup> per side.</li> </ul> </li> </ul> </li> <li>(d) if located below canopy level, are flush with a wall</li> <li>(e) if located at canopy level, are in the form of a fascia sign</li> <li>(f) if located above a canopy: <ul style="list-style-type: none"> <li>(i) are flush with a wall</li> <li>(ii) do not have any part rising above parapet height</li> </ul> </li> </ul>



	<p>(iii) are not attached to the roof of the building.</p> <p>(g) if attached to a verandah, no part of the advertisement protrudes beyond the outer limits of the verandah structure</p> <p>(h) if attached to a two-storey building, have no part located above the finished floor level of the second storey of the building</p> <p>(i) where they are flush with a wall, do not, in combination with any other existing sign, cover more than 15% of the building facade to which they are attached.</p>
<p>PO 1.2</p> <p>Advertising hoardings do not disfigure the appearance of the land upon which they are situated or the character of the locality.</p>	<p>DTS/DPF 1.2</p> <p>Where development comprises an advertising hoarding, the supporting structure is:</p> <p>(a) concealed by the associated advertisement and decorative detailing or</p> <p>(b) not visible from an adjacent public street or thoroughfare, other than a support structure in the form of a single or dual post design.</p>
<p>PO 1.3</p> <p>Advertising does not encroach on public land or the land of an adjacent allotment.</p>	<p>DTS/DPF 1.3</p> <p>Advertisements and/or advertising hoardings are contained within the boundaries of the site.</p>
<p>PO 1.4</p> <p>Where possible, advertisements on public land are integrated with existing structures and infrastructure.</p>	<p>DTS/DPF 1.4</p> <p>Advertisements on public land that meet at least one of the following:</p> <p>(a) achieves Advertisements DTS/DPF 1.1</p> <p>(b) are integrated with a bus shelter.</p>
<p>PO 1.5</p> <p>Advertisements and/or advertising hoardings are of a scale and size appropriate to the character of the locality.</p>	<p>DTS/DPF 1.5</p> <p>None are applicable.</p>
Proliferation of Advertisements	
<p>PO 2.1</p> <p>Proliferation of advertisements is minimised to avoid visual clutter and untidiness.</p>	<p>DTS/DPF 2.1</p> <p>No more than one freestanding advertisement is displayed per occupancy.</p>
<p>PO 2.2</p> <p>Multiple business or activity advertisements are co-located and coordinated to avoid visual clutter and untidiness.</p>	<p>DTS/DPF 2.2</p> <p>Advertising of a multiple business or activity complex is located on a single advertisement fixture or structure.</p>
<p>PO 2.3</p> <p>Proliferation of advertisements attached to buildings is minimised to avoid visual clutter and untidiness.</p>	<p>DTS/DPF 2.3</p> <p>Advertisements satisfy all of the following:</p> <p>(a) are attached to a building</p> <p>(b) other than in a Neighbourhood-type zone, where they are flush with a wall, cover no more than 15% of the building facade to which they are attached</p> <p>(c) do not result in more than one sign per occupancy that</p>

	is not flush with a wall.
Advertising Content	
<p>PO 3.1</p> <p>Advertisements are limited to information relating to the lawful use of land they are located on to assist in the ready identification of the activity or activities on the land and avoid unrelated content that contributes to visual clutter and untidiness.</p>	<p>DTS/DPF 3.1</p> <p>Advertisements contain information limited to a lawful existing or proposed activity or activities on the same site as the advertisement.</p>
Amenity Impacts	
<p>PO 4.1</p> <p>Light spill from advertisement illumination does not unreasonably compromise the amenity of sensitive receivers.</p>	<p>DTS/DPF 4.1</p> <p>Advertisements do not incorporate any illumination.</p>
Safety	
<p>PO 5.1</p> <p>Advertisements and/or advertising hoardings erected on a verandah or projecting from a building wall are designed and located to allow for safe and convenient pedestrian access.</p>	<p>DTS/DPF 5.1</p> <p>Advertisements have a minimum clearance of 2.5m between the top of the footpath and base of the underside of the sign.</p>
<p>PO 5.2</p> <p>Advertisements and/or advertising hoardings do not distract or create a hazard to drivers through excessive illumination.</p>	<p>DTS/DPF 5.2</p> <p>No advertisement illumination is proposed.</p>
<p>PO 5.3</p> <p>Advertisements and/or advertising hoardings do not create a hazard to drivers by:</p> <ul style="list-style-type: none"> <li>(a) being liable to interpretation by drivers as an official traffic sign or signal</li> <li>(b) obscuring or impairing drivers' view of official traffic signs or signals</li> <li>(c) obscuring or impairing drivers' view of features of a road that are potentially hazardous (such as junctions, bends, changes in width and traffic control devices) or other road or rail vehicles at/or approaching level crossings.</li> </ul>	<p>DTS/DPF 5.3</p> <p>Advertisements satisfy all of the following:</p> <ul style="list-style-type: none"> <li>(a) are not located in a public road or rail reserve</li> <li>(b) are located wholly outside the land shown as 'Corner Cut-Off Area' in the following diagram</li> </ul> 
<p>PO 5.4</p> <p>Advertisements and/or advertising hoardings do not create a hazard by distracting drivers from the primary driving task at a location where the demands on driver concentration are high.</p>	<p>DTS/DPF 5.4</p> <p>Advertisements and/or advertising hoardings are not located along or adjacent to a road having a speed limit of 80km/h or more.</p>
<p>PO 5.5</p> <p>Advertisements and/or advertising hoardings provide sufficient clearance from the road carriageway to allow for safe and convenient movement by all road users.</p>	<p>DTS/DPF 5.5</p> <p>Where the advertisement or advertising hoarding is:</p> <ul style="list-style-type: none"> <li>(a) on a kerbed road with a speed zone of 60km/h or less, the advertisement or advertising hoarding is located at least 0.6m from the roadside edge of the kerb</li> <li>(b) on an unkerbed road with a speed zone of 60km/h or less, the advertisement or advertising hoarding is located at least 5.5m from the edge of the seal</li> <li>(c) on any other kerbed or unkerbed road, the</li> </ul>

	<p>advertisement or advertising hoarding is located a minimum of the following distance from the roadside edge of the kerb or the seal:</p> <ul style="list-style-type: none"> <li>(a) 110 km/h road - 14m</li> <li>(b) 100 km/h road - 13m</li> <li>(c) 90 km/h road - 10m</li> <li>(d) 70 or 80 km/h road - 8.5m.</li> </ul>
<p>PO 5.6</p> <p>Advertising near signalised intersections does not cause unreasonable distraction to road users through illumination, flashing lights, or moving or changing displays or messages.</p>	<p>DTS/DPF 5.6</p> <p>Advertising:</p> <ul style="list-style-type: none"> <li>(a) is not illuminated</li> <li>(b) does not incorporate a moving or changing display or message</li> <li>(c) does not incorporate a flashing light(s).</li> </ul>

## Animal Keeping and Horse Keeping

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Animals are kept at a density that is not beyond the carrying capacity of the land and in a manner that minimises their adverse effects on the environment, local amenity and surrounding development.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting and Design	
<p>PO 1.1</p> <p>Animal keeping, horse keeping and associated activities do not create adverse impacts on the environment or the amenity of the locality.</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Animal keeping and horse keeping is located and managed to minimise the potential transmission of disease to other operations where animals are kept.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
Horse Keeping	
<p>PO 2.1</p> <p>Water from stable wash-down areas is directed to appropriate absorption areas and/or drainage pits to minimise pollution of</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>



land and water.	
<p>PO 2.2</p> <p>Stables, horse shelters or associated yards are sited appropriate distances away from sensitive receivers and/or allotments in other ownership to avoid adverse impacts from dust, erosion and odour.</p>	<p>DTS/DPF 2.2</p> <p>Stables, horse shelters and associated yards are sited in accordance with all of the following:</p> <ul style="list-style-type: none"> <li>(a) 30m or more from any sensitive receivers (existing or approved) on land in other ownership</li> <li>(b) where an adjacent allotment is vacant and in other ownership, 30m or more from the boundary of that allotment.</li> </ul>
<p>PO 2.3</p> <p>All areas accessible to horses are separated from septic tank effluent disposal areas to protect the integrity of that system. Stable flooring is constructed with an impervious material to facilitate regular cleaning.</p>	<p>DTS/DPF 2.3</p> <p>Septic tank effluent disposal areas are enclosed with a horse-proof barrier such as a fence to exclude horses from this area.</p>
<p>PO 2.4</p> <p>To minimise environmental harm and adverse impacts on water resources, stables, horse shelters and associated yards are appropriately set back from a watercourse.</p>	<p>DTS/DPF 2.4</p> <p>Stables, horse shelters and associated yards are set back 50m or more from a watercourse.</p>
<p>PO 2.5</p> <p>Stables, horse shelters and associated yards are located on slopes that are stable to minimise the risk of soil erosion and water runoff.</p>	<p>DTS/DPF 2.5</p> <p>Stables, horse shelters and associated yards are not located on land with a slope greater than 10% (1-in-10).</p>
Kennels	
<p>PO 3.1</p> <p>Kennel flooring is constructed with an impervious material to facilitate regular cleaning.</p>	<p>DTS/DPF 3.1</p> <p>The floors of kennels satisfy all of the following:</p> <ul style="list-style-type: none"> <li>(a) are constructed of impervious concrete</li> <li>(b) are designed to be self-draining when washed down.</li> </ul>
<p>PO 3.2</p> <p>Kennels and exercise yards are designed and sited to minimise noise nuisance to neighbours through measures such as:</p> <ul style="list-style-type: none"> <li>(a) adopting appropriate separation distances</li> <li>(b) orientating openings away from sensitive receivers.</li> </ul>	<p>DTS/DPF 3.2</p> <p>Kennels are sited 500m or more from the nearest sensitive receiver on land in other ownership.</p>
<p>PO 3.3</p> <p>Dogs are regularly observed and managed to minimise nuisance impact on adjoining sensitive receivers from animal behaviour.</p>	<p>DTS/DPF 3.3</p> <p>Kennels are sited in association with a permanent dwelling on the land.</p>
Wastes	
<p>PO 4.1</p> <p>Storage of manure, used litter and other wastes (other than wastewater lagoons) is designed, constructed and managed to minimise attracting and harbouring vermin.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
PO 4.2	DTS/DPF 4.2

Facilities for the storage of manure, used litter and other wastes (other than wastewater lagoons) are located to minimise the potential for polluting water resources.	Waste storage facilities (other than wastewater lagoons) are located outside the 1% AEP flood event areas.
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## Aquaculture

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Aquaculture facilities are developed in an ecologically, economically and socially sustainable manner to support an equitable sharing of marine, coastal and inland resources and mitigate conflict with other water-based and land-based uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land-based Aquaculture	
PO 1.1 Land-based aquaculture and associated components are sited and designed to mitigate adverse impacts on nearby sensitive receivers.	DTS/DPF 1.1 Land-based aquaculture and associated components are located to satisfy all of the following:  (a) 200m or more from a sensitive receiver in other ownership (b) 500m or more from the boundary of a zone primarily intended to accommodate sensitive receivers.
PO 1.2 Land-based aquaculture and associated components are sited and designed to prevent surface flows from entering ponds in a 1% AEP sea flood level event.	DTS/DPF 1.2 None are applicable.
PO 1.3 Land-based aquaculture and associated components are sited and designed to prevent pond leakage that would pollute groundwater.	DTS/DPF 1.3 None are applicable.
PO 1.4 Land-based aquaculture and associated components are sited and designed to prevent farmed species escaping and entering into any waters.	DTS/DPF 1.4 None are applicable.
PO 1.5 Land-based aquaculture and associated components, including	DTS/DPF 1.5 None are applicable.

intake and discharge pipes, are designed to minimise the need to traverse sensitive areas to minimise impact on the natural environment.	
PO 1.6  Pipe inlets and outlets associated with land-based aquaculture are sited and designed to minimise the risk of disease transmission.	DTS/DPF 1.6  None are applicable.
PO 1.7  Storage areas associated with aquaculture activity are integrated with the use of the land and sited and designed to minimise their visual impact on the surrounding environment.	DTS/DPF 1.7  None are applicable.
Marine Based Aquaculture	
PO 2.1  Marine aquaculture is sited and designed to minimise its adverse impacts on sensitive ecological areas including:  (a) creeks and estuaries (b) wetlands (c) significant seagrass and mangrove communities (d) marine habitats and ecosystems.	DTS/DPF 2.1  None are applicable.
PO 2.2  Marine aquaculture is sited in areas with adequate water current to disperse sediments and dissolve particulate wastes to prevent the build-up of waste that may cause environmental harm.	DTS/DPF 2.2  None are applicable.
PO 2.3  Marine aquaculture is designed to not involve discharge of human waste on the site, on any adjacent land or into nearby waters.	DTS/DPF 2.3  None are applicable.
PO 2.4  Marine aquaculture (other than inter-tidal aquaculture) is located an appropriate distance seaward of the high water mark.	DTS/DPF 2.4  Marine aquaculture development is located 100m or more seaward of the high water mark.
PO 2.5  Marine aquaculture is sited and designed to not obstruct or interfere with:  (a) areas of high public use (b) areas, including beaches, used for recreational activities such as swimming, fishing, skiing, sailing and other water sports (c) areas of outstanding visual or environmental value (d) areas of high tourism value (e) areas of important regional or state economic activity, including commercial ports, wharfs and jetties (f) the operation of infrastructure facilities including inlet and outlet pipes associated with the desalination of sea water.	DTS/DPF 2.5  None are applicable.



PO 2.6 Marine aquaculture is sited and designed to minimise interference and obstruction to the natural processes of the coastal and marine environment.	DTS/DPF 2.6 None are applicable.
PO 2.7 Marine aquaculture is designed to be as unobtrusive as practicable by incorporating measures such as:  (a) using feed hoppers painted in subdued colours and suspending them as close as possible to the surface of the water (b) positioning structures to protrude the minimum distance practicable above the surface of the water (c) avoiding the use of shelters and structures above cages and platforms unless necessary to exclude predators and protected species from interacting with the farming structures and/or stock inside the cages, or for safety reasons (d) positioning racks, floats and other farm structures in unobtrusive locations landward from the shoreline.	DTS/DPF 2.7 None are applicable.
PO 2.8 Access, launching and maintenance facilities utilise existing established roads, tracks, ramps and paths to or from the sea where possible to minimise environmental and amenity impacts.	DTS/DPF 2.8 None are applicable.
PO 2.9 Access, launching and maintenance facilities are developed as common user facilities and are co-located where practicable to mitigate adverse impacts on coastal areas.	DTS/DPF 2.9 None are applicable.
PO 2.10 Marine aquaculture is sited to minimise potential impacts on, and to protect the integrity of, reserves under the <i>National Parks and Wildlife Act 1972</i> .	DTS/DPF 2.10 Marine aquaculture is located 1000m or more seaward of the boundary of any reserve under the <i>National Parks and Wildlife Act 1972</i> .
PO 2.11 Onshore storage, cooling and processing facilities do not impair the coastline and its visual amenity by:  (a) being sited, designed, landscaped and of a scale to reduce the overall bulk and appearance of buildings and complement the coastal landscape (b) making provision for appropriately sited and designed vehicular access arrangements, including using existing vehicular access arrangements as far as practicable (c) incorporating appropriate waste treatment and disposal.	DTS/DPF 2.11 None are applicable.
Navigation and Safety	
PO 3.1 Marine aquaculture sites are suitably marked to maintain navigational safety.	DTS/DPF 3.1 None are applicable.

PO 3.2 Marine aquaculture is sited to provide adequate separation between farms for safe navigation.	DTS/DPF 3.2 None are applicable.
Environmental Management	
PO 4.1 Marine aquaculture is maintained to prevent hazards to people and wildlife, including breeding grounds and habitats of native marine mammals and terrestrial fauna, especially migratory species.	DTS/DPF 4.1 None are applicable.
PO 4.2 Marine aquaculture is designed to facilitate the relocation or removal of structures in the case of emergency such as oil spills, algal blooms and altered water flows.	DTS/DPF 4.2 None are applicable.
PO 4.3 Marine aquaculture provides for progressive or future reclamation of disturbed areas ahead of, or upon, decommissioning.	DTS/DPF 4.3 None are applicable.
PO 4.4 Aquaculture operations incorporate measures for the removal and disposal of litter, disused material, shells, debris, detritus, dead animals and animal waste to prevent pollution of waters, wetlands, or the nearby coastline.	DTS/DPF 4.4 None are applicable.

## Beverage Production in Rural Areas

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Mitigation of potential amenity and environmental impacts of value-adding beverage production facilities such as wineries, distilleries, cideries and breweries.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Odour and Noise	
PO 1.1 Beverage production activities are designed and sited to	DTS/DPF 1.1 None are applicable.

minimise odour impacts on rural amenity.	
PO 1.2 Beverage production activities are designed and sited to minimise noise impacts on sensitive receivers.	DTS/DPF 1.2 None are applicable.
PO 1.3 Fermentation, distillation, manufacturing, storage, packaging and bottling activities occur within enclosed buildings to improve the visual appearance within a locality and manage noise associated with these activities.	DTS/DPF 1.3 None are applicable.
PO 1.4 Breweries are designed to minimise odours emitted during boiling and fermentation stages of production.	DTS/DPF 1.4 Brew kettles are fitted with a vapour condenser.
PO 1.5 Beverage production solid wastes are stored in a manner that minimises odour impacts on sensitive receivers in other ownership.	DTS/DPF 1.5 Solid waste from beverage production is collected and stored in sealed containers and removed from the site within 48 hours.
Water Quality	
PO 2.1 Beverage production wastewater management systems (including wastewater irrigation) are set back from watercourses to minimise adverse impacts on water resources.	DTS/DPF 2.1 Wastewater management systems are set back 50m or more from the banks of watercourses and bores.
PO 2.2 The storage or disposal of chemicals or hazardous substances is undertaken in a manner to prevent pollution of water resources.	DTS/DPF 2.2 None are applicable.
PO 2.3 Stormwater runoff from areas that may cause contamination due to beverage production activities (including vehicle movements and machinery operations) is drained to an onsite stormwater treatment system to manage potential environmental impacts.	DTS/DPF 2.3 None are applicable.
PO 2.4 Stormwater runoff from areas unlikely to cause contamination by beverage production and associated activities (such as roof catchments and clean hard-paved surfaces) is diverted away from beverage production areas and wastewater management systems.	DTS/DPF 2.4 None are applicable.
Wastewater Irrigation	
PO 3.1 Beverage production wastewater irrigation systems are designed and located to not contaminate soil and surface and ground water resources or damage crops.	DTS/DPF 3.1 None are applicable.
PO 3.2 Beverage production wastewater irrigation systems are	DTS/DPF 3.2 Beverage production wastewater is not irrigated within 50m of



designed and located to minimise impact on amenity and avoid spray drift onto adjoining land.	any dwelling in other ownership.
<p>PO 3.3</p> <p>Beverage production wastewater is not irrigated onto areas that pose an undue risk to the environment or amenity such as:</p> <ul style="list-style-type: none"> <li>(a) waterlogged areas</li> <li>(b) land within 50m of a creek, swamp or domestic or stock water bore</li> <li>(c) land subject to flooding</li> <li>(d) steeply sloping land</li> <li>(e) rocky or highly permeable soil overlaying an unconfined aquifer.</li> </ul>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>

## Bulk Handling and Storage Facilities

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Facilities for the bulk handling and storage of agricultural, mineral, petroleum, rock, ore or other similar commodities are designed to minimise adverse impacts on transport networks, the landscape and surrounding land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting and Design	
<p>PO 1.1</p> <p>Bulk handling and storage facilities are sited and designed to minimise risks of adverse air quality and noise impacts on sensitive receivers.</p>	<p>DTS/DPF 1.1</p> <p>Facilities for the handling, storage and dispatch of commodities in bulk (excluding processing) meet the following minimum separation distances from sensitive receivers:</p> <ul style="list-style-type: none"> <li>(a) bulk handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals at a wharf or wharf side facility (including sea-port grain terminals), where the handling of these materials into or from vessels does not exceed 100 tonnes per day: 300m or more from residential premises not associated with the facility</li> <li>(b) bulk handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals to or from any commercial storage facility: 300m or more from residential premises not associated with the facility</li> <li>(c) bulk petroleum storage involving individual containers with a capacity up to 200 litres and a total on-site</li> </ul>

	<p>storage capacity not exceeding 1,000 cubic metres: 500m or more</p> <p>(d) coal handling with:</p> <p>a. capacity up to 1 tonne per day or a storage capacity up to 50 tonnes: 500m or more</p> <p>b. capacity exceeding 1 tonne per day but not exceeding 100 tonnes per day or a storage capacity exceeding 50 tonnes but not exceeding 5000 tonnes: 1000m or more.</p>
Buffers and Landscaping	
<p>PO 2.1</p> <p>Bulk handling and storage facilities incorporate a buffer area for the establishment of dense landscaping adjacent road frontages to enhance the appearance of land and buildings from public thoroughfares.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
<p>PO 2.2</p> <p>Bulk handling and storage facilities incorporate landscaping to assist with screening and dust filtration.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
Access and Parking	
<p>PO 3.1</p> <p>Roadways and vehicle parking areas associated with bulk handling and storage facilities are designed and surfaced to control dust emissions and prevent drag out of material from the site.</p>	<p>DTS/DPF 3.1</p> <p>Roadways and vehicle parking areas are sealed with an all-weather surface.</p>
Slipways, Wharves and Pontoons	
<p>PO 4.1</p> <p>Slipways, wharves and pontoons used for the handling of bulk materials (such as fuel, oil, catch, bait and the like) incorporate catchment devices to avoid the release of materials into adjacent waters.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>

## Clearance from Overhead Powerlines

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Protection of human health and safety when undertaking development in the vicinity of overhead transmission powerlines.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1	DTS/DPF 1.1

Buildings are adequately separated from aboveground powerlines to minimise potential hazard to people and property.	<p>One of the following is satisfied:</p> <p>(a) a declaration is provided by or on behalf of the applicant to the effect that the proposal would not be contrary to the regulations prescribed for the purposes of section 86 of the <i>Electricity Act 1996</i></p> <p>(b) there are no aboveground powerlines adjoining the site that are the subject of the proposed development.</p>
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## Design

### Assessment Provisions (AP)

Desired Outcome	
DO 1	<p>Development is:</p> <p>(a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributes to the character of the immediate area</p> <p>(b) durable - fit for purpose, adaptable and long lasting</p> <p>(c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access, and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors</p> <p>(d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.</p>

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All development	
External Appearance	
PO 1.1 Buildings reinforce corners through changes in setback, articulation, materials, colour and massing (including height, width, bulk, roof form and slope).	DTS/DPF 1.1 None are applicable.
PO 1.2 Where zero or minor setbacks are desirable, development provides shelter over footpaths ( <u>in the form of verandahs, awnings, canopies and the like, with adequate lighting</u> ) to positively contribute to the walkability, comfort and safety of the public realm.	DTS/DPF 1.2 None are applicable.
PO 1.3 Building elevations facing the primary street (other than ancillary buildings) are designed and detailed to convey purpose, identify	DTS/DPF 1.3 None are applicable.



main access points and complement the streetscape.	
<p>PO 1.4</p> <p>Plant, exhaust and intake vents and other technical equipment is integrated into the building design to minimise visibility from the public realm and negative impacts on residential amenity by:</p> <ul style="list-style-type: none"> <li>(a) positioning plant and equipment in unobtrusive locations viewed from public roads and spaces</li> <li>(b) screening rooftop plant and equipment from view</li> <li>(c) when located on the roof of non-residential development, locating the plant and equipment as far as practicable from adjacent sensitive land uses.</li> </ul>	<p>DTS/DPF 1.4</p> <p>Development does not incorporate any structures that protrude beyond the roofline.</p>
<p>PO 1.5</p> <p>The negative visual impact of outdoor storage, waste management, loading and service areas is minimised by integrating them into the building design and screening them from public view (such as fencing, landscaping and built form) taking into account the form of development contemplated in the relevant zone.</p>	<p>DTS/DPF 1.5</p> <p>None are applicable.</p>
Safety	
<p>PO 2.1</p> <p>Development maximises opportunities for passive surveillance of the public realm by providing clear lines of sight, appropriate lighting and the use of visually permeable screening wherever practicable.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
<p>PO 2.2</p> <p>Development is designed to differentiate public, communal and private areas.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
<p>PO 2.3</p> <p>Buildings are designed with safe, perceptible and direct access from public street frontages and vehicle parking areas.</p>	<p>DTS/DPF 2.3</p> <p>None are applicable.</p>
<p>PO 2.4</p> <p>Development at street level is designed to maximise opportunities for passive surveillance of the adjacent public realm.</p>	<p>DTS/DPF 2.4</p> <p>None are applicable.</p>
<p>PO 2.5</p> <p>Common areas and entry points of buildings (such as the foyer areas of residential buildings), and non-residential land uses at street level, maximise passive surveillance from the public realm to the inside of the building at night.</p>	<p>DTS/DPF 2.5</p> <p>None are applicable.</p>
Landscaping	
<p>PO 3.1</p> <p>Soft landscaping and tree planting is incorporated to:</p> <ul style="list-style-type: none"> <li>(a) minimise heat absorption and reflection</li> </ul>	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>

<ul style="list-style-type: none"> <li>(b) maximise shade and shelter</li> <li>(c) maximise stormwater infiltration</li> <li>(d) enhance the appearance of land and streetscapes</li> <li>(e) contribute to biodiversity.</li> </ul>	
<p>PO 3.2</p> <p>Soft landscaping and tree planting maximises the use of locally indigenous plant species, incorporates plant species best suited to current and future climate conditions and avoids pest plant and weed species.</p>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>
Environmental Performance	
<p>PO 4.1</p> <p>Buildings are sited, oriented and designed to maximise natural sunlight access and ventilation to main activity areas, habitable rooms, common areas and open spaces.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p> <p>Buildings are sited and designed to maximise passive environmental performance and minimise energy consumption and reliance on mechanical systems, such as heating and cooling.</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>
<p>PO 4.3</p> <p>Buildings incorporate climate-responsive techniques and features such as building and window orientation, use of eaves, verandahs and shading structures, water harvesting, at ground landscaping, green walls, green roofs and photovoltaic cells.</p>	<p>DTS/DPF 4.3</p> <p>None are applicable.</p>
Water Sensitive Design	
<p>PO 5.1</p> <p>Development is sited and designed to maintain natural hydrological systems without negatively impacting:</p> <ul style="list-style-type: none"> <li>(a) the quantity and quality of surface water and groundwater</li> <li>(b) the depth and directional flow of surface water and groundwater</li> <li>(c) the quality and function of natural springs.</li> </ul>	<p>DTS/DPF 5.1</p> <p>None are applicable.</p>
On-site Waste Treatment Systems	
<p>PO 6.1</p> <p>Dedicated on-site effluent disposal areas do not include any areas to be used for, or could be reasonably foreseen to be used for, private open space, driveways or car parking.</p>	<p>DTS/DPF 6.1</p> <p>Effluent disposal drainage areas do not:</p> <ul style="list-style-type: none"> <li>(a) encroach within an area used as private open space or result in less private open space than that specified in Design Table 1 - Private Open Space</li> <li>(b) use an area also used as a driveway</li> <li>(c) encroach within an area used for on-site car parking or result in less on-site car parking than that specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.</li> </ul>

Carparking Appearance	
<p>PO 7.1</p> <p>Development facing the street is designed to minimise the negative impacts of any semi-basement and undercroft car parking on the streetscapes through techniques such as:</p> <ul style="list-style-type: none"> <li>(a) limiting protrusion above finished ground level</li> <li>(b) screening through appropriate planting, fencing and mounding</li> <li>(c) limiting the width of openings and integrating them into the building structure.</li> </ul>	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>
<p>PO 7.2</p> <p>Vehicle parking areas are appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced and the like.</p>	<p>DTS/DPF 7.2</p> <p>None are applicable.</p>
<p>PO 7.3</p> <p>Safe, legible, direct and accessible pedestrian connections are provided between parking areas and the development.</p>	<p>DTS/DPF 7.3</p> <p>None are applicable.</p>
<p>PO 7.4</p> <p>Street level vehicle parking areas incorporate tree planting to provide shade and reduce solar heat absorption and reflection.</p>	<p>DTS/DPF 7.4</p> <p>None are applicable.</p>
<p>PO 7.5</p> <p>Street level parking areas incorporate soft landscaping to improve visual appearance when viewed from within the site and from public places.</p>	<p>DTS/DPF 7.5</p> <p>None are applicable.</p>
<p>PO 7.6</p> <p>Vehicle parking areas and associated driveways are landscaped to provide shade and positively contribute to amenity.</p>	<p>DTS/DPF 7.6</p> <p>None are applicable.</p>
<p>PO 7.7</p> <p>Vehicle parking areas and access ways incorporate integrated stormwater management techniques such as permeable or porous surfaces, infiltration systems, drainage swales or rain gardens that integrate with soft landscaping.</p>	<p>DTS/DPF 7.7</p> <p>None are applicable.</p>
Earthworks and sloping land	
<p>PO 8.1</p> <p>Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.</p>	<p>DTS/DPF 8.1</p> <p>Development does not involve any of the following:</p> <ul style="list-style-type: none"> <li>(a) excavation exceeding a vertical height of 1m</li> <li>(b) filling exceeding a vertical height of 1m</li> <li>(c) a total combined excavation and filling vertical height of 2m or more.</li> </ul>
<p>PO 8.2</p>	<p>DTS/DPF 8.2</p>



Driveways and access tracks are designed and constructed to allow safe and convenient access on sloping land (with a gradient exceeding 1 in 8).	Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8) satisfy (a) and (b):  (a) do not have a gradient exceeding 25% (1-in-4) at any point along the driveway (b) are constructed with an all-weather trafficable surface.
PO 8.3  Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8):  (a) do not contribute to the instability of embankments and cuttings (b) provide level transition areas for the safe movement of people and goods to and from the development (c) are designed to integrate with the natural topography of the land.	DTS/DPF 8.3  None are applicable.
PO 8.4  Development on sloping land (with a gradient exceeding 1 in 8) avoids the alteration of natural drainage lines and includes on-site drainage systems to minimise erosion.	DTS/DPF 8.4  None are applicable.
PO 8.5  Development does not occur on land at risk of landslide nor increases the potential for landslide or land surface instability.	DTS/DPF 8.5  None are applicable.
Fences and Walls	
PO 9.1  Fences, walls and retaining walls are of sufficient height to maintain privacy and security without unreasonably impacting the visual amenity and adjoining land's access to sunlight or the amenity of public places.	DTS/DPF 9.1  None are applicable.
PO 9.2  Landscaping incorporated on the low side of retaining walls is visible from public roads and public open space to minimise visual impacts.	DTS/DPF 9.2  A vegetated landscaped strip 1m wide or more is provided against the low side of a retaining wall.
Overlooking / Visual Privacy (in building 3 storeys or less)	
PO 10.1  Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses.	DTS/DPF 10.1  Upper level windows facing side or rear boundaries shared with a residential allotment/site satisfy one of the following:  (a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 200mm  (b) have sill heights greater than or equal to 1.5m above finished floor level  (c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5 m above the finished floor level.

PO 10.2	DTS/DPF 10.2
Development mitigates direct overlooking from balconies, terraces and decks to habitable rooms and private open space of adjoining residential uses.	<p>One of the following is satisfied:</p> <ul style="list-style-type: none"> <li>(a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace</li> <li>or</li> <li>(b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: <ul style="list-style-type: none"> <li>(i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land</li> <li>or</li> <li>(ii) 1.7m above finished floor level in all other cases</li> </ul> </li> </ul>
All Residential development	
Front elevations and passive surveillance	
PO 11.1	DTS/DPF 11.1
Dwellings incorporate windows along primary street frontages to encourage passive surveillance and make a positive contribution to the streetscape.	<p>Each dwelling with a frontage to a public street:</p> <ul style="list-style-type: none"> <li>(a) includes at least one window facing the primary street from a habitable room that has a minimum internal room dimension of 2.4m</li> <li>(b) has an aggregate window area of at least 2m<sup>2</sup> facing the primary street.</li> </ul>
PO 11.2	DTS/DPF 11.2
Dwellings incorporate entry doors within street frontages to address the street and provide a legible entry point for visitors.	Dwellings with a frontage to a public street have an entry door visible from the primary street boundary.
Outlook and amenity	
PO 12.1	DTS/DPF 12.1
Living rooms have an external outlook to provide a high standard of amenity for occupants.	A living room of a dwelling incorporates a window with an outlook towards the street frontage or private open space, public open space, or waterfront areas.
PO 12.2	DTS/DPF 12.2
Bedrooms are separated or shielded from active communal recreation areas, common access areas and vehicle parking areas and access ways to mitigate noise and artificial light intrusion.	None are applicable.
Ancillary Development	
PO 13.1	DTS/DPF 13.1
Residential ancillary buildings and structures are sited and designed to not detract from the streetscape or appearance of buildings on the site or neighbouring properties.	<p>Ancillary buildings:</p> <ul style="list-style-type: none"> <li>(a) are ancillary to a dwelling erected on the same site</li> <li>(b) have a floor area not exceeding 60m<sup>2</sup></li> <li>(c) are not constructed, added to or altered so that any part is situated: <ul style="list-style-type: none"> <li>(i) in front of any part of the building line of the</li> </ul> </li> </ul>

- dwelling to which it is ancillary  
or
- (ii) within 900mm of a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads)
- (d) in the case of a garage or carport, the garage or carport:
- (i) is set back at least 5.5m from the boundary of the primary street
- (ii) when facing a primary street or secondary street, has a total door / opening not exceeding:
- A. for dwellings of single building level - 7m in width or 50% of the site frontage, whichever is the lesser
- B. for dwellings comprising two or more building levels at the building line fronting the same public street - 7m in width
- (e) if situated on a boundary (not being a boundary with a primary street or secondary street), do not exceed a length of 11.5m unless:
- (i) a longer wall or structure exists on the adjacent site and is situated on the same allotment boundary
- and
- (ii) the proposed wall or structure will be built along the same length of boundary as the existing adjacent wall or structure to the same or lesser extent
- (f) if situated on a boundary of the allotment (not being a boundary with a primary street or secondary street), all walls or structures on the boundary will not exceed 45% of the length of that boundary
- (g) will not be located within 3m of any other wall along the same boundary unless on an adjacent site on that boundary there is an existing wall of a building that would be adjacent to or about the proposed wall or structure
- (h) have a wall height or post height not exceeding 3m above natural ground level
- (i) have a roof height where no part of the roof is more than 5m above the natural ground level
- (j) if clad in sheet metal, is pre-colour treated or painted in a non-reflective colour
- (k) retains a total area of soft landscaping in accordance with (i) or (ii), whichever is less:
- (i) a total area as determined by the following table:

Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m <sup>2</sup> )	Minimum percentage of site
<150	10%
150-200	15%



		201-450	20%
		>450	25%
	(ii)	the amount of existing soft landscaping prior to the development occurring.	
PO 13.2	DTS/DPF 13.2		
Ancillary buildings and structures do not impede on-site functional requirements such as private open space provision or car parking requirements and do not result in over-development of the site.	Ancillary buildings and structures do not result in: (a) less private open space than specified in Design in Urban Areas Table 1 - Private Open Space (b) less on-site car parking than specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.		
PO 13.3	DTS/DPF 13.3		
Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa is positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers.	The pump and/or filtration system is ancillary to a dwelling erected on the same site and is: (a) enclosed in a solid acoustic structure that is located at least 5m from the nearest habitable room located on an adjoining allotment or (b) located at least 12m from the nearest habitable room located on an adjoining allotment.		
Garage appearance			
PO 14.1	DTS/DPF 14.1		
Garaging is designed to not detract from the streetscape or appearance of a dwelling.	Garages and carports facing a street: (a) are situated so that no part of the garage or carport is in front of any part of the building line of the dwelling (b) are set back at least 5.5m from the boundary of the primary street (c) have a garage door / opening not exceeding 7m in width (d) have a garage door /opening width not exceeding 50% of the site frontage unless the dwelling has two or more building levels at the building line fronting the same public street.		
Massing			
PO 15.1	DTS/DPF 15.1		
The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.	None are applicable		
Dwelling additions			
PO 16.1	DTS / DPF 16.1		
Dwelling additions are sited and designed to not detract from the streetscape or amenity of adjoining properties and do not impede on-site functional requirements.	Dwelling additions: (a) are not constructed, added to or altered so that any part is situated closer to a public street (b) do not result in:		

	<ul style="list-style-type: none"> <li>(i) excavation exceeding a vertical height of 1m</li> <li>(ii) filling exceeding a vertical height of 1m</li> <li>(iii) a total combined excavation and filling vertical height of 2m or more</li> <li>(iv) less Private Open Space than specified in Design Table 1 - Private Open Space</li> <li>(v) less on-site parking than specified in Transport Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas</li> <li>(vi) upper level windows facing side or rear boundaries unless: <ul style="list-style-type: none"> <li>A. they are permanently obscured to a height of 1.5m above finished floor level that is fixed or not capable of being opened more than 200mm or</li> <li>B. have sill heights greater than or equal to 1.5m above finished floor level or</li> <li>C. incorporate screening to a height of 1.5m above finished floor level</li> </ul> </li> <li>(vii) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: <ul style="list-style-type: none"> <li>A. 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land</li> <li>B. 1.7m above finished floor level in all other cases.</li> </ul> </li> </ul>
Private Open Space	
PO 17.1  Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.	DTS/DPF 17.1  Private open space is provided in accordance with Design Table 1 - Private Open Space.
Water Sensitive Design	
PO 18.1  Residential development creating a common driveway / access includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system, watercourses or other water bodies.	DTS/DPF 18.1  Residential development creating a common driveway / access that services 5 or more dwellings achieves the following stormwater runoff outcomes: <ul style="list-style-type: none"> <li>(a) 80 per cent reduction in average annual total suspended solids</li> <li>(b) 60 per cent reduction in average annual total phosphorus</li> <li>(c) 45 per cent reduction in average annual total nitrogen.</li> </ul>
PO 18.2  Residential development creating a common driveway / access includes a stormwater management system designed to	DTS/DPF 18.2  Development creating a common driveway / access that services 5 or more dwellings:

mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.	<p>(a) maintains the pre-development peak flow rate from the site based upon a 0.35 runoff coefficient for the 18.1% AEP 30-minute storm and the stormwater runoff time to peak is not increased</p> <p>or</p> <p>captures and retains the difference in pre-development runoff volume (based upon a 0.35 runoff coefficient) vs post development runoff volume from the site for an 18.1% AEP 30-minute storm; and</p> <p>(b) manages site generated stormwater runoff up to and including the 1% AEP flood event to avoid flooding of buildings.</p>
Car parking, access and manoeuvrability	
<p>PO 19.1</p> <p>Enclosed parking spaces are of a size and dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 19.1</p> <p>Residential car parking spaces enclosed by fencing, walls or other structures have the following internal dimensions (separate from any waste storage area):</p> <p>(a) single width car parking spaces:</p> <ul style="list-style-type: none"> <li>(i) a minimum length of 5.4m per space</li> <li>(ii) a minimum width of 3.0m</li> <li>(iii) a minimum garage door width of 2.4m</li> </ul> <p>(b) double width car parking spaces (side by side):</p> <ul style="list-style-type: none"> <li>(i) a minimum length of 5.4m</li> <li>(ii) a minimum width of 5.4m</li> <li>(iii) minimum garage door width of 2.4m per space.</li> </ul>
<p>PO 19.2</p> <p>Uncovered parking spaces are of a size and dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 19.2</p> <p>Uncovered car parking spaces have:</p> <p>(a) a minimum length of 5.4m</p> <p>(b) a minimum width of 2.4m</p> <p>(c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m</p>
<p>PO 19.3</p> <p>Driveways are located and designed to facilitate safe access and egress while maximising land available for street tree planting, landscaped street frontages, domestic waste collection and on-street parking.</p>	<p>DTS/DPF 19.3</p> <p>Driveways and access points on sites with a frontage to a public road of 10m or less have a width between 3.0 and 3.2 metres measured at the property boundary and are the only access point provided on the site.</p>
<p>PO 19.4</p> <p>Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.</p>	<p>DTS/DPF 19.4</p> <p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <p>(a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land</p> <p>(b) where newly proposed:</p> <ul style="list-style-type: none"> <li>(i) is set back 6m or more from the tangent point of an intersection of 2 or more roads</li> <li>(ii) is set back outside of the marked lines or</li> </ul>



	<p>(iii) infrastructure dedicating a pedestrian crossing does not involve the removal, relocation or damage to of mature street trees, street furniture or utility infrastructure services.</p>		
<p>PO 19.5</p> <p>Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.</p>	<p>DTS/DPF 19.5</p> <p>Driveways are designed and sited so that:</p> <ul style="list-style-type: none"> <li>(a) the gradient from the place of access on the boundary of the allotment to the finished floor level at the front of the garage or carport is not steeper than 1:4 on average</li> <li>(b) they are aligned relative to the street boundary so that there is no more than a 20 degree deviation from 90 degrees between the centreline of any dedicated car parking space to which it provides access (measured from the front of that space) and the street boundary</li> <li>(c) if located to provide access from an alley, lane or right of way - the alley, land or right of way is at least 6.2m wide along the boundary of the allotment / site</li> </ul>		
<p>PO 19.6</p> <p>Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.</p>	<p>DTS/DPF 19.6</p> <p>Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements:</p> <ul style="list-style-type: none"> <li>(a) minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number)</li> <li>(b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly</li> <li>(c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.</li> </ul>		
Waste storage			
<p>PO 20.1</p> <p>Provision is made for the adequate and convenient storage of waste bins in a location screened from public view.</p>	<p>DTS/DPF 20.1</p> <p>None are applicable.</p>		
Design of Transportable Dwellings			
<p>PO 21.1</p> <p>The sub-floor space beneath transportable buildings is enclosed to give the appearance of a permanent structure.</p>	<p>DTS/DPF 21.1</p> <p>Buildings satisfy (a) or (b):</p> <ul style="list-style-type: none"> <li>(a) are not transportable or</li> <li>(b) the sub-floor space between the building and ground level is clad in a material and finish consistent with the building.</li> </ul>		
Group dwelling, residential flat buildings and battle-axe development			
Amenity			
<p>PO 22.1</p> <p>Dwellings are of a suitable size to accommodate a layout that is well organised and provides a high standard of amenity for occupants.</p>	<p>DTS/DPF 22.1</p> <p>Dwellings have a minimum internal floor area in accordance with the following table:</p> <table border="1"> <tr> <td></td><td></td></tr> </table>		

	Number of bedrooms	Minimum internal floor area
	Studio	35m <sup>2</sup>
	1 bedroom	50m <sup>2</sup>
	2 bedroom	65m <sup>2</sup>
	3+ bedrooms	80m <sup>2</sup> and any dwelling over 3 bedrooms provides an additional 15m <sup>2</sup> for every additional bedroom
PO 22.2	DTS/DPF 22.2	
The orientation and siting of buildings minimises impacts on the amenity, outlook and privacy of occupants and neighbours.	None are applicable.	
PO 22.3	DTS/DPF 22.3	
Development maximises the number of dwellings that face public open space and public streets and limits dwellings oriented towards adjoining properties.	None are applicable.	
PO 22.4	DTS/DPF 22.4	
Battle-axe development is appropriately sited and designed to respond to the existing neighbourhood context.	Dwelling sites/allotments are not in the form of a battle-axe arrangement.	
Communal Open Space		
PO 23.1	DTS/DPF 23.1	
Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.	None are applicable.	
PO 23.2	DTS/DPF 23.2	
Communal open space is of sufficient size and dimensions to cater for group recreation.	Communal open space incorporates a minimum dimension of 5 metres.	
PO 23.3	DTS/DPF 23.3	
Communal open space is designed and sited to:  (a) be conveniently accessed by the dwellings which it services (b) have regard to acoustic, safety, security and wind effects.	None are applicable.	
PO 23.4	DTS/DPF 23.4	
Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.	None are applicable.	
PO 23.5	DTS/DPF 23.5	
Communal open space is designed and sited to:	None are applicable.	

<p>(a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings</p> <p>(b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive surveillance.</p>	
Carparking, access and manoeuvrability	
<p>PO 24.1</p> <p>Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.</p>	<p>DTS/DPF 24.1</p> <p>Where on-street parking is available directly adjacent the site, on-street parking is retained adjacent the subject site in accordance with the following requirements:</p> <ul style="list-style-type: none"> <li>(a) minimum 0.33 on-street car parks per proposed dwellings (rounded up to the nearest whole number)</li> <li>(b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly</li> <li>(c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.</li> </ul>
<p>PO 24.2</p> <p>The number of vehicular access points onto public roads is minimised to reduce interruption of the footpath and positively contribute to public safety and walkability.</p>	<p>DTS/DPF 24.2</p> <p>Access to group dwellings or dwellings within a residential flat building is provided via a single common driveway.</p>
<p>PO 24.3</p> <p>Residential driveways that service more than one dwelling are designed to allow safe and convenient movement.</p>	<p>DTS/DPF 24.3</p> <p>Driveways that service more than 1 dwelling or a dwelling on a battle-axe site:</p> <ul style="list-style-type: none"> <li>(a) have a minimum width of 3m</li> <li>(b) for driveways servicing more than 3 dwellings: <ul style="list-style-type: none"> <li>(i) have a width of 5.5m or more and a length of 6m or more at the kerb of the primary street</li> <li>(ii) where the driveway length exceeds 30m, incorporate a passing point at least every 30 metres with a minimum width of 5.5m and a minimum length of 6m.</li> </ul> </li> </ul>
<p>PO 24.4</p> <p>Residential driveways in a battle-axe configuration are designed to allow safe and convenient movement.</p>	<p>DTS/DPF 24.4</p> <p>Where in a battle-axe configuration, a driveway servicing one dwelling has a minimum width of 3m.</p>
<p>PO 24.5</p> <p>Residential driveways that service more than one dwelling are designed to allow passenger vehicles to enter and exit the site and manoeuvre within the site in a safe and convenient manner.</p>	<p>DTS/DPF 24.5</p> <p>Driveways providing access to more than one dwelling, or a dwelling on a battle-axe site, allow a B85 passenger vehicle to enter and exit the garages or parking spaces in no more than a three-point turn manoeuvre.</p>
<p>PO 24.6</p> <p>Dwellings are adequately separated from common driveways and manoeuvring areas.</p>	<p>DTS/DPF 24.6</p> <p>Dwelling walls with entry doors or ground level habitable room windows are set back at least 1.5m from any driveway or area designated for the movement and manoeuvring of vehicles.</p>
Soft Landscaping	



PO 25.1	DTS/DPF 25.1
Soft landscaping is provided between dwellings and common driveways to improve the outlook for occupants and appearance of common areas.	Other than where located directly in front of a garage or a building entry, soft landscaping with a minimum dimension of 1m is provided between a dwelling and common driveway.
PO 25.2	DTS/DPF 25.2
Soft landscaping is provided that improves the appearance of common driveways.	Where a common driveway is located directly adjacent the side or rear boundary of the site, soft landscaping with a minimum dimension of 1m is provided between the driveway and site boundary (excluding along the perimeter of a passing point).
Site Facilities / Waste Storage	
PO 26.1	DTS/DPF 26.1
Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.	None are applicable.
PO 26.2	DTS/DPF 26.2
Provision is made for suitable external clothes drying facilities.	None are applicable.
PO 26.3	DTS/DPF 26.3
Provision is made for suitable household waste and recyclable material storage facilities which are:	None are applicable.
(a) located away, or screened, from public view, and	
(b) conveniently located in proximity to dwellings and the waste collection point.	
PO 26.4	DTS/DPF 26.4
Waste and recyclable material storage areas are located away from dwellings.	Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.
PO 26.5	DTS/DPF 26.5
Where waste bins cannot be conveniently collected from the street, provision is made for on-site waste collection, designed to accommodate the safe and convenient access, egress and movement of waste collection vehicles.	None are applicable.
PO 26.6	DTS/DPF 26.6
Services including gas and water meters are conveniently located and screened from public view.	None are applicable.
Supported accommodation and retirement facilities	
Siting and Configuration	
PO 27.1	DTS/DPF 27.1
Supported accommodation and housing for aged persons and people with disabilities is located where on-site movement of residents is not unduly restricted by the slope of the land.	None are applicable.
Movement and Access	
PO 28.1	DTS/DPF 28.1

Development is designed to support safe and convenient access and movement for residents by providing:	None are applicable.
<ul style="list-style-type: none"> <li>(a) ground-level access or lifted access to all units</li> <li>(b) level entry porches, ramps, paths, driveways, passenger loading areas and areas adjacent to footpaths that allow for the passing of wheelchairs and resting places</li> <li>(c) car parks with gradients no steeper than 1-in-40 and of sufficient area to provide for wheelchair manoeuvrability</li> <li>(d) kerb ramps at pedestrian crossing points.</li> </ul>	
Communal Open Space	
PO 29.1 Development is designed to provide attractive, convenient and comfortable indoor and outdoor communal areas to be used by residents and visitors.	DTS/DPF 29.1 None are applicable.
PO 29.2 Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.	DTS/DPF 29.2 None are applicable.
PO 29.3 Communal open space is of sufficient size and dimensions to cater for group recreation.	DTS/DPF 29.3 Communal open space incorporates a minimum dimension of 5 metres.
PO 29.4 Communal open space is designed and sited to:	DTS/DPF 29.4 None are applicable.
<ul style="list-style-type: none"> <li>(a) be conveniently accessed by the dwellings which it services</li> <li>(b) have regard to acoustic, safety, security and wind effects.</li> </ul>	
PO 29.5 Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.	DTS/DPF 29.5 None are applicable.
PO 29.6 Communal open space is designed and sited to:	DTS/DPF 29.6 None are applicable.
<ul style="list-style-type: none"> <li>(a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings</li> <li>(b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive surveillance.</li> </ul>	
Site Facilities / Waste Storage	
PO 30.1 Development is designed to provide storage areas for personal items and specialised equipment such as small electric powered vehicles, including facilities for the recharging of small electric powered vehicles.	DTS/DPF 30.1 None are applicable.

PO 30.2 Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.	DTS/DPF 30.2  None are applicable.
PO 30.3 Provision is made for suitable external clothes drying facilities.	DTS/DPF 28.3  None are applicable.
PO 30.4 Provision is made for suitable household waste and recyclable material storage facilities conveniently located and screened from public view.	DTS/DPF 30.4  None are applicable.
PO 30.5 Waste and recyclable material storage areas are located away from dwellings.	DTS/DPF 30.5  Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.
PO 30.6 Provision is made for on-site waste collection where 10 or more bins are to be collected at any one time.	DTS/DPF 30.6  None are applicable.
PO 30.7 Services including gas and water meters are conveniently located and screened from public view.	DTS/DPF 30.7  None are applicable.
All non-residential development	
Water Sensitive Design	
PO 31.1 Development likely to result in significant risk of export of litter, oil or grease includes stormwater management systems designed to minimise pollutants entering stormwater.	DTS/DPF 31.1  None are applicable.
PO 31.2 Water discharged from a development site is of a physical, chemical and biological condition equivalent to or better than its pre-developed state.	DTS/DPF 31.2  None are applicable.
Wash-down and Waste Loading and Unloading	
PO 32.1 Areas for activities including loading and unloading, storage of waste refuse bins in commercial and industrial development or wash-down areas used for the cleaning of vehicles, vessels, plant or equipment are:  (a) designed to contain all wastewater likely to pollute stormwater within a bunded and roofed area to exclude the entry of external surface stormwater run-off (b) paved with an impervious material to facilitate wastewater collection (c) of sufficient size to prevent 'splash-out' or 'over-spray' of wastewater from the wash-down area	DTS/DPF 32.1  None are applicable.

<p>(d) designed to drain wastewater to either:</p> <p>(i) a treatment device such as a sediment trap and coalescing plate oil separator with subsequent disposal to a sewer, private or Community Wastewater Management Scheme or</p> <p>(ii) a holding tank and its subsequent removal off-site on a regular basis.</p>	
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**Table 1 - Private Open Space**

Dwelling Type	Minimum Rate
Dwelling (at ground level)	<p>Total private open space area:</p> <p>(a) Site area &lt;301m<sup>2</sup>: 24m<sup>2</sup> located behind the building line.</p> <p>(b) Site area ≥ 301m<sup>2</sup>: 60m<sup>2</sup> located behind the building line.</p> <p>Minimum directly accessible from a living room: 16m<sup>2</sup> / with a minimum dimension 3m.</p>
Dwelling (above ground level)	<p>Studio (no separate bedroom): 4m<sup>2</sup> with a minimum dimension 1.8m</p> <p>One bedroom: 8m<sup>2</sup> with a minimum dimension 2.1m</p> <p>Two bedroom dwelling: 11m<sup>2</sup> with a minimum dimension 2.4m</p> <p>Three + bedroom dwelling: 15m<sup>2</sup> with a minimum dimension 2.6m</p>
Cabin or caravan (permanently fixed to the ground) in a residential park or a caravan and tourist park	Total area: 16m <sup>2</sup> , which may be used as second car parking space, provided on each site intended for residential occupation.

## Design in Urban Areas

### Assessment Provisions (AP)

Desired Outcome	
DO 1	<p>Development is:</p> <p>(a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributing to the character of the locality</p> <p>(b) durable - fit for purpose, adaptable and long lasting</p> <p>(c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors</p> <p>(d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and</p>



	local amenity and to minimise energy consumption.
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Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All Development	
External Appearance	
PO 1.1 Buildings reinforce corners through changes in setback, articulation, materials, colour and massing (including height, width, bulk, roof form and slope).	DTS/DPF 1.1 None are applicable.
PO 1.2 Where zero or minor setbacks are desirable, development provides shelter over footpaths (in the form of verandahs, awnings, canopies and the like, with adequate lighting) to positively contribute to the walkability, comfort and safety of the public realm.	DTS/DPF 1.2 None are applicable.
PO 1.3 Building elevations facing the primary street (other than ancillary buildings) are designed and detailed to convey purpose, identify main access points and complement the streetscape.	DTS/DPF 1.3 None are applicable.
PO 1.4 Plant, exhaust and intake vents and other technical equipment are integrated into the building design to minimise visibility from the public realm and negative impacts on residential amenity by: <ul style="list-style-type: none"> <li>(a) positioning plant and equipment discretely, in unobtrusive locations as viewed from public roads and spaces</li> <li>(b) screening rooftop plant and equipment from view</li> <li>(c) when located on the roof of non-residential development, locating the plant and equipment as far as practicable from adjacent sensitive land uses.</li> </ul>	DTS/DPF 1.4 Development does not incorporate any structures that protrude beyond the roofline.
PO 1.5 The negative visual impact of outdoor storage, waste management, loading and service areas is minimised by integrating them into the building design and screening them from public view (such as fencing, landscaping and built form), taking into account the form of development contemplated in the relevant zone.	DTS/DPF 1.5 None are applicable.
Safety	
PO 2.1 Development maximises opportunities for passive surveillance of the public realm by providing clear lines of sight, appropriate lighting and the use of visually permeable screening wherever practicable.	DTS/DPF 2.1 None are applicable.

PO 2.2	DTS/DPF 2.2
Development is designed to differentiate public, communal and private areas.	None are applicable.
PO 2.3	DTS/DPF 2.3
Buildings are designed with safe, perceptible and direct access from public street frontages and vehicle parking areas.	None are applicable.
PO 2.4	DTS/DPF 2.4
Development at street level is designed to maximise opportunities for passive surveillance of the adjacent public realm.	None are applicable.
PO 2.5	DTS/DPF 2.5
Common areas and entry points of buildings (such as the foyer areas of residential buildings) and non-residential land uses at street level, maximise passive surveillance from the public realm to the inside of the building at night.	None are applicable.
Landscaping	
PO 3.1	DTS/DPF 3.1
Soft landscaping and tree planting are incorporated to:	None are applicable.
<ul style="list-style-type: none"> <li>(a) minimise heat absorption and reflection</li> <li>(b) maximise shade and shelter</li> <li>(c) maximise stormwater infiltration</li> <li>(d) enhance the appearance of land and streetscapes.</li> </ul>	
Environmental Performance	
PO 4.1	DTS/DPF 4.1
Buildings are sited, oriented and designed to maximise natural sunlight access and ventilation to main activity areas, habitable rooms, common areas and open spaces.	None are applicable.
PO 4.2	DTS/DPF 4.2
Buildings are sited and designed to maximise passive environmental performance and minimise energy consumption and reliance on mechanical systems, such as heating and cooling.	None are applicable.
PO 4.3	DTS/DPF 4.3
Buildings incorporate climate responsive techniques and features such as building and window orientation, use of eaves, verandahs and shading structures, water harvesting, at ground landscaping, green walls, green roofs and photovoltaic cells.	None are applicable.
Water Sensitive Design	
PO 5.1	DTS/DPF 5.1
Development is sited and designed to maintain natural hydrological systems without negatively impacting:	None are applicable.

<ul style="list-style-type: none"> <li>(a) the quantity and quality of surface water and groundwater</li> <li>(b) the depth and directional flow of surface water and groundwater</li> <li>(c) the quality and function of natural springs.</li> </ul>	
On-site Waste Treatment Systems	
<p>PO 6.1</p> <p>Dedicated on-site effluent disposal areas do not include any areas to be used for, or could be reasonably foreseen to be used for, private open space, driveways or car parking.</p>	<p>DTS/DPF 6.1</p> <p>Effluent disposal drainage areas do not:</p> <ul style="list-style-type: none"> <li>(a) encroach within an area used as private open space or result in less private open space than that specified in Design in Urban Areas Table 1 - Private Open Space</li> <li>(b) use an area also used as a driveway</li> <li>(c) encroach within an area used for on-site car parking or result in less on-site car parking than that specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.</li> </ul>
Car parking appearance	
<p>PO 7.1</p> <p>Development facing the street is designed to minimise the negative impacts of any semi-basement and undercroft car parking on streetscapes through techniques such as:</p> <ul style="list-style-type: none"> <li>(a) limiting protrusion above finished ground level</li> <li>(b) screening through appropriate planting, fencing and mounding</li> <li>(c) limiting the width of openings and integrating them into the building structure.</li> </ul>	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>
<p>PO 7.2</p> <p>Vehicle parking areas appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced and the like.</p>	<p>DTS/DPF 7.2</p> <p>None are applicable.</p>
<p>PO 7.3</p> <p>Safe, legible, direct and accessible pedestrian connections are provided between parking areas and the development.</p>	<p>DTS/DPF 7.3</p> <p>None are applicable.</p>
<p>PO 7.4</p> <p>Street-level vehicle parking areas incorporate tree planting to provide shade, reduce solar heat absorption and reflection.</p>	<p>DTS/DPF 7.4</p> <p>Vehicle parking areas that are open to the sky and comprise 10 or more car parking spaces include a shade tree with a mature canopy of 4m diameter spaced for each 10 car parking spaces provided and a landscaped strip on any road frontage of a minimum dimension of 1m.</p>
<p>PO 7.5</p> <p>Street level parking areas incorporate soft landscaping to improve visual appearance when viewed from within the site and from public places.</p>	<p>DTS/DPF 7.5</p> <p>Vehicle parking areas comprising 10 or more car parking spaces include soft landscaping with a minimum dimension of:</p> <ul style="list-style-type: none"> <li>(a) 1m along all public road frontages and allotment boundaries</li> <li>(b) 1m between double rows of car parking spaces.</li> </ul>

PO 7.6 Vehicle parking areas and associated driveways are landscaped to provide shade and positively contribute to amenity.	DTS/DPF 7.6 None are applicable.
PO 7.7 Vehicle parking areas and access ways incorporate integrated stormwater management techniques such as permeable or porous surfaces, infiltration systems, drainage swales or rain gardens that integrate with soft landscaping.	DTS/DPF 7.7 None are applicable.
Earthworks and sloping land	
PO 8.1 Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.	DTS/DPF 8.1 Development does not involve any of the following:  (a) excavation exceeding a vertical height of 1m (b) filling exceeding a vertical height of 1m (c) a total combined excavation and filling vertical height of 2m or more.
PO 8.2 Driveways and access tracks designed and constructed to allow safe and convenient access on sloping land.	DTS/DPF 8.2 Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8) satisfy (a) and (b):  (a) do not have a gradient exceeding 25% (1-in-4) at any point along the driveway (b) are constructed with an all-weather trafficable surface.
PO 8.3 Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8):  (a) do not contribute to the instability of embankments and cuttings (b) provide level transition areas for the safe movement of people and goods to and from the development (c) are designed to integrate with the natural topography of the land.	DTS/DPF 8.3 None are applicable.
PO 8.4 Development on sloping land (with a gradient exceeding 1 in 8) avoids the alteration of natural drainage lines and includes on site drainage systems to minimise erosion.	DTS/DPF 8.4 None are applicable.
PO 8.5 Development does not occur on land at risk of landslip or increase the potential for landslip or land surface instability.	DTS/DPF 8.5 None are applicable.
Fences and walls	
PO 9.1 Fences, walls and retaining walls of sufficient height maintain privacy and security without unreasonably impacting visual amenity and adjoining land's access to sunlight or the amenity of public places.	DTS/DPF 9.1 None are applicable.



PO 9.2	DTS/DPF 9.2
Landscaping is incorporated on the low side of retaining walls that are visible from public roads and public open space to minimise visual impacts.	A vegetated landscaped strip 1m wide or more is provided against the low side of a retaining wall.
Overlooking / Visual Privacy (low rise buildings)	
PO 10.1	DTS/DPF 10.1
Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses in neighbourhood-type zones.	Upper level windows facing side or rear boundaries shared with a residential use in a neighbourhood-type zone: <ul style="list-style-type: none"> <li>(a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 125mm</li> <li>(b) have sill heights greater than or equal to 1.5m above finished floor level</li> <li>(c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5 m above the finished floor level.</li> </ul>
PO 10.2	DTS/DPF 10.2
Development mitigates direct overlooking from balconies to habitable rooms and private open space of adjoining residential uses in neighbourhood type zones.	One of the following is satisfied: <ul style="list-style-type: none"> <li>(a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace</li> <li>or</li> <li>(b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: <ul style="list-style-type: none"> <li>(i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land</li> <li>or</li> <li>(ii) 1.7m above finished floor level in all other cases</li> </ul> </li> </ul>
Site Facilities / Waste Storage (excluding low rise residential development)	
PO 11.1	DTS/DPF 11.1
Development provides a dedicated area for on-site collection and sorting of recyclable materials and refuse, green organic waste and wash bay facilities for the ongoing maintenance of bins that is adequate in size considering the number and nature of the activities they will serve and the frequency of collection.	None are applicable.
PO 11.2	DTS/DPF 11.2
Communal waste storage and collection areas are located, enclosed and designed to be screened from view from the public domain, open space and dwellings.	None are applicable.
PO 11.3	DTS/DPF 11.3
Communal waste storage and collection areas are designed to be well ventilated and located away from habitable rooms.	None are applicable.
PO 11.4	DTS/DPF 11.4
Communal waste storage and collection areas are designed to allow waste and recycling collection vehicles to enter and leave the site without reversing.	None are applicable.

PO 11.5 For mixed use developments, non-residential waste and recycling storage areas and access provide opportunities for on-site management of food waste through composting or other waste recovery as appropriate.	DTS/DPF 11.5 None are applicable.
All Development - Medium and High Rise	
External Appearance	
PO 12.1 Buildings positively contribute to the character of the local area by responding to local context.	DTS/DPF 12.1 None are applicable.
PO 12.2 Architectural detail at street level and a mixture of materials at lower building levels near the public interface are provided to reinforce a human scale.	DTS/DPF 12.2 None are applicable.
PO 12.3 Buildings are designed to reduce visual mass by breaking up building elevations into distinct elements.	DTS/DPF 12.3 None are applicable.
PO 12.4 Boundary walls visible from public land include visually interesting treatments to break up large blank elevations.	DTS/DPF 12.4 None are applicable.
PO 12.5 External materials and finishes are durable and age well to minimise ongoing maintenance requirements.	DTS/DPF 12.5 Buildings utilise a combination of the following external materials and finishes:  <ul style="list-style-type: none"> <li>(a) masonry</li> <li>(b) natural stone</li> <li>(c) pre-finished materials that minimise staining, discolouring or deterioration.</li> </ul>
PO 12.6 Street-facing building elevations are designed to provide attractive, high quality and pedestrian-friendly street frontages.	DTS/DPF 12.6 Building street frontages incorporate:  <ul style="list-style-type: none"> <li>(a) active uses such as shops or offices</li> <li>(b) prominent entry areas for multi-storey buildings (where it is a common entry)</li> <li>(c) habitable rooms of dwellings</li> <li>(d) areas of communal public realm with public art or the like, where consistent with the zone and/or subzone provisions.</li> </ul>
PO 12.7 Entrances to multi-storey buildings are safe, attractive, welcoming, functional and contribute to streetscape character.	DTS/DPF 12.7 Entrances to multi-storey buildings are:  <ul style="list-style-type: none"> <li>(a) oriented towards the street</li> <li>(b) clearly visible and easily identifiable from the street and vehicle parking areas</li> <li>(c) designed to be prominent, accentuated and a welcoming feature if there are no active or occupied ground floor uses</li> <li>(d) designed to provide shelter, a sense of personal</li> </ul>

	<p>address and transitional space around the entry</p> <p>(e) located as close as practicable to the lift and / or lobby access to minimise the need for long access corridors</p> <p>(f) designed to avoid the creation of potential areas of entrapment.</p>																								
<p>PO 12.8</p> <p>Building services, plant and mechanical equipment are screened from the public realm.</p>	<p>DTS/DPF 12.8</p> <p>None are applicable.</p>																								
Landscaping																									
<p>PO 13.1</p> <p>Development facing a street provides a well landscaped area that contains a deep soil space to accommodate a tree of a species and size adequate to provide shade, contribute to tree canopy targets and soften the appearance of buildings.</p>	<p>DTS/DPF 13.1</p> <p>Buildings provide a 4m by 4m deep soil space in front of the building that accommodates a medium to large tree, except where no building setback from front property boundaries is desired.</p>																								
<p>PO 13.2</p> <p>Deep soil zones are provided to retain existing vegetation or provide areas that can accommodate new deep root vegetation, including tall trees with large canopies to provide shade and soften the appearance of multi-storey buildings.</p>	<p>DTS/DPF 13.2</p> <p>Multi-storey development provides deep soil zones and incorporates trees at not less than the following rates, except in a location or zone where full site coverage is desired.</p> <table><tr><th>Site area</th><th>Minimum deep soil area</th><th>Minimum dimension</th><th>Tree / deep soil zones</th></tr><tr><td>&lt;300 m<sup>2</sup></td><td>10 m<sup>2</sup></td><td>1.5m</td><td>1 small tree / 10 m<sup>2</sup></td></tr><tr><td>300-1500 m<sup>2</sup></td><td>7% site area</td><td>3m</td><td>1 medium tree / 30 m<sup>2</sup></td></tr><tr><td>&gt;1500 m<sup>2</sup></td><td>7% site area</td><td>6m</td><td>1 large or medium tree / 60 m<sup>2</sup></td></tr></table> <p><b>Tree size and site area definitions</b></p> <table><tr><td>Small tree</td><td>4-6m mature height and 2-4m canopy spread</td></tr><tr><td>Medium tree</td><td>6-12m mature height and 4-8m canopy spread</td></tr><tr><td>Large tree</td><td>12m mature height and &gt;8m canopy spread</td></tr><tr><td>Site area</td><td>The total area for development site, not average area per dwelling</td></tr></table>	Site area	Minimum deep soil area	Minimum dimension	Tree / deep soil zones	<300 m <sup>2</sup>	10 m <sup>2</sup>	1.5m	1 small tree / 10 m <sup>2</sup>	300-1500 m <sup>2</sup>	7% site area	3m	1 medium tree / 30 m <sup>2</sup>	>1500 m <sup>2</sup>	7% site area	6m	1 large or medium tree / 60 m <sup>2</sup>	Small tree	4-6m mature height and 2-4m canopy spread	Medium tree	6-12m mature height and 4-8m canopy spread	Large tree	12m mature height and >8m canopy spread	Site area	The total area for development site, not average area per dwelling
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<p>PO 13.3</p> <p>Deep soil zones with access to natural light are provided to assist in maintaining vegetation health.</p>	<p>DTS/DPF 13.3</p> <p>None are applicable.</p>																								
<p>PO 13.4</p>	<p>DTS/DPF 13.4</p>																								

Unless separated by a public road or reserve, development sites adjacent to any zone that has a primary purpose of accommodating low-rise residential development incorporate a deep soil zone along the common boundary to enable medium to large trees to be retained or established to assist in screening new buildings of 3 or more building levels in height.	Building elements of 3 or more building levels in height are set back at least 6m from a zone boundary in which a deep soil zone area is incorporated.
Environmental	
PO 14.1 Development minimises detrimental micro-climatic impacts on adjacent land and buildings.	DTS/DPF 14.1 None are applicable.
PO 14.2 Development incorporates sustainable design techniques and features such as window orientation, eaves and shading structures, water harvesting and use, green walls and roof designs that enable the provision of rain water tanks (where they are not provided elsewhere on site), green roofs and photovoltaic cells.	DTS/DPF 14.2 None are applicable.
PO 14.3 Development of 5 or more building levels, or 21m or more in height (as measured from natural ground level and excluding roof-mounted mechanical plant and equipment) is designed to minimise the impacts of wind through measures such as:  (a) a podium at the base of a tall tower and aligned with the street to deflect wind away from the street (b) substantial verandahs around a building to deflect downward travelling wind flows over pedestrian areas (c) the placement of buildings and use of setbacks to deflect the wind at ground level (d) avoiding tall shear elevations that create windy conditions at street level.	DTS/DPF 14.3 None are applicable.
Car Parking	
PO 15.1 Multi-level vehicle parking structures are designed to contribute to active street frontages and complement neighbouring buildings.	DTS/DPF 15.1 Multi-level vehicle parking structures within buildings:  (a) provide land uses such as commercial, retail or other non-car parking uses along ground floor street frontages (b) incorporate facade treatments in building elevations facing along major street frontages that are sufficiently enclosed and detailed to complement adjacent buildings.
PO 15.2 Multi-level vehicle parking structures within buildings complement the surrounding built form in terms of height, massing and scale.	DTS/DPF 15.2 None are applicable.
Overlooking/Visual Privacy	
PO 16.1	DTS/DPF 16.1



Development mitigates direct overlooking of habitable rooms and private open spaces of adjacent residential uses in neighbourhood-type zones through measures such as:	None are applicable.
All residential development	
Front elevations and passive surveillance	
PO 17.1 Dwellings incorporate windows facing primary street frontages to encourage passive surveillance and make a positive contribution to the streetscape.	DTS/DPF 17.1 Each dwelling with a frontage to a public street:  (a) includes at least one window facing the primary street from a habitable room that has a minimum internal room dimension of 2.4m (b) has an aggregate window area of at least 2m <sup>2</sup> facing the primary street.
PO 17.2 Dwellings incorporate entry doors within street frontages to address the street and provide a legible entry point for visitors.	DTS/DPF 17.2 Dwellings with a frontage to a public street have an entry door visible from the primary street boundary.
Outlook and Amenity	
PO 18.1 Living rooms have an external outlook to provide a high standard of amenity for occupants.	DTS/DPF 18.1 A living room of a dwelling incorporates a window with an external outlook of the street frontage, private open space, public open space, or waterfront areas.
PO 18.2 Bedrooms are separated or shielded from active communal recreation areas, common access areas and vehicle parking areas and access ways to mitigate noise and artificial light intrusion.	DTS/DPF 18.2 None are applicable.
Ancillary Development	
PO 19.1 Residential ancillary buildings are sited and designed to not detract from the streetscape or appearance of primary residential buildings on the site or neighbouring properties.	DTS/DPF 19.1 Ancillary buildings: (a) are ancillary to a dwelling erected on the same site (b) have a floor area not exceeding 60m <sup>2</sup> (c) are not constructed, added to or altered so that any part is situated: (i) in front of any part of the building line of the dwelling to which it is ancillary or (ii) within 900mm of a boundary of the allotment

with a secondary street (if the land has boundaries on two or more roads)

- (d) in the case of a garage or carport, the garage or carport:
- (i) is set back at least 5.5m from the boundary of the primary street
  - (ii) when facing a primary street or secondary street, has a total door / opening not exceeding:
    - A. for dwellings of single building level - 7m in width or 50% of the site frontage, whichever is the lesser
    - B. for dwellings comprising two or more building levels at the building line fronting the same public street - 7m in width
- (e) if situated on a boundary (not being a boundary with a primary street or secondary street), do not exceed a length of 11.5m unless:
- (i) a longer wall or structure exists on the adjacent site and is situated on the same allotment boundary and
  - (ii) the proposed wall or structure will be built along the same length of boundary as the existing adjacent wall or structure to the same or lesser extent
- (f) if situated on a boundary of the allotment (not being a boundary with a primary street or secondary street), all walls or structures on the boundary will not exceed 45% of the length of that boundary
- (g) will not be located within 3m of any other wall along the same boundary unless on an adjacent site on that boundary there is an existing wall of a building that would be adjacent to or about the proposed wall or structure
- (h) have a wall height or post height not exceeding 3m above natural ground level
- (i) have a roof height where no part of the roof is more than 5m above the natural ground level
- (j) if clad in sheet metal, is pre-colour treated or painted in a non-reflective colour
- (k) retains a total area of soft landscaping in accordance with (i) or (ii), whichever is less:
- (i) a total area as determined by the following table:

Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m <sup>2</sup> )	Minimum percentage of site
<150	10%
150-200	15%
201-450	20%

		>450	25%
	(ii)	the amount of existing soft landscaping prior to the development occurring.	
PO 19.2  Ancillary buildings and structures do not impede on-site functional requirements such as private open space provision, car parking requirements or result in over-development of the site.	DTS/DPF 19.2  Ancillary buildings and structures do not result in:  (a) less private open space than specified in Design in Urban Areas Table 1 - Private Open Space (b) less on-site car parking than specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.		
PO 19.3  Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers.	DTS/DPF 19.3  The pump and/or filtration system is ancillary to a dwelling erected on the same site and is:  (a) enclosed in a solid acoustic structure that is located at least 5m from the nearest habitable room located on an adjoining allotment or (b) located at least 12m from the nearest habitable room located on an adjoining allotment.		
Residential Development - Low Rise			
External appearance			
PO 20.1  Garaging is designed to not detract from the streetscape or appearance of a dwelling.	DTS/DPF 20.1  Garages and carports facing a street:  (a) are situated so that no part of the garage or carport will be in front of any part of the building line of the dwelling (b) are set back at least 5.5m from the boundary of the primary street (c) have a garage door / opening width not exceeding 7m (d) have a garage door / opening width not exceeding 50% of the site frontage unless the dwelling has two or more building levels at the building line fronting the same public street.		
PO 20.2  Dwelling elevations facing public streets and common driveways make a positive contribution to the streetscape and the appearance of common driveway areas.	DTS/DPF 20.2  Each dwelling includes at least 3 of the following design features within the building elevation facing a primary street, and at least 2 of the following design features within the building elevation facing any other public road (other than a laneway) or a common driveway:  (a) a minimum of 30% of the building wall is set back an additional 300mm from the building line (b) a porch or portico projects at least 1m from the building wall (c) a balcony projects from the building wall (d) a verandah projects at least 1m from the building wall (e) eaves of a minimum 400mm width extend along the		

	<p>width of the front elevation</p> <p>(f) a minimum 30% of the width of the upper level projects forward from the lower level primary building line by at least 300mm</p> <p>(g) a minimum of two different materials or finishes are incorporated on the walls of the front building elevation, with a maximum of 80% of the building elevation in a single material or finish.</p>										
PO 20.3	DTS/DPF 20.3										
The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.	None are applicable										
Private Open Space											
PO 21.1	DTS/DPF 21.1										
Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.	Private open space is provided in accordance with Design in Urban Areas Table 1 - Private Open Space.										
PO 21.2	DTS/DPF 21.2										
Private open space is positioned to provide convenient access from internal living areas.	Private open space is directly accessible from a habitable room.										
Landscaping											
PO 22.1	DTS/DPF 22.1										
<p>Soft landscaping is incorporated into development to:</p> <p>(a) minimise heat absorption and reflection</p> <p>(b) contribute shade and shelter</p> <p>(c) provide for stormwater infiltration and biodiversity</p> <p>(d) enhance the appearance of land and streetscapes.</p>	<p>Residential development incorporates soft landscaping with a minimum dimension of 700mm provided in accordance with (a) and (b):</p> <p>(a) a total area as determined by the following table:</p> <table border="1"> <thead> <tr> <th>Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m<sup>2</sup>)</th><th>Minimum percentage of site</th></tr> </thead> <tbody> <tr> <td>&lt;150</td><td>10%</td></tr> <tr> <td>150-200</td><td>15%</td></tr> <tr> <td>&gt;200-450</td><td>20%</td></tr> <tr> <td>&gt;450</td><td>25%</td></tr> </tbody> </table> <p>(b) at least 30% of any land between the primary street boundary and the primary building line.</p>	Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m <sup>2</sup> )	Minimum percentage of site	<150	10%	150-200	15%	>200-450	20%	>450	25%
Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m <sup>2</sup> )	Minimum percentage of site										
<150	10%										
150-200	15%										
>200-450	20%										
>450	25%										
Car parking, access and manoeuvrability											
PO 23.1	DTS/DPF 23.1										



<p>Enclosed car parking spaces are of dimensions to be functional, accessible and convenient.</p>	<p>Residential car parking spaces enclosed by fencing, walls or other structures have the following internal dimensions (separate from any waste storage area):</p> <ul style="list-style-type: none"> <li>(a) single width car parking spaces: <ul style="list-style-type: none"> <li>(i) a minimum length of 5.4m per space</li> <li>(ii) a minimum width of 3.0m</li> <li>(iii) a minimum garage door width of 2.4m</li> </ul> </li> <li>(b) double width car parking spaces (side by side): <ul style="list-style-type: none"> <li>(i) a minimum length of 5.4m</li> <li>(ii) a minimum width of 5.4m</li> <li>(iii) minimum garage door width of 2.4m per space.</li> </ul> </li> </ul>
<p>PO 23.2</p> <p>Uncovered car parking space are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 23.2</p> <p>Uncovered car parking spaces have:</p> <ul style="list-style-type: none"> <li>(a) a minimum length of 5.4m</li> <li>(b) a minimum width of 2.4m</li> <li>(c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m.</li> </ul>
<p>PO 23.3</p> <p>Driveways and access points are located and designed to facilitate safe access and egress while maximising land available for street tree planting, domestic waste collection, landscaped street frontages and on-street parking.</p>	<p>DTS/DPF 23.3</p> <p>Driveways and access points satisfy (a) or (b):</p> <ul style="list-style-type: none"> <li>(a) sites with a frontage to a public road of 10m or less, have a width between 3.0 and 3.2 metres measured at the property boundary and are the only access point provided on the site</li> <li>(b) sites with a frontage to a public road greater than 10m: <ul style="list-style-type: none"> <li>(i) have a maximum width of 5m measured at the property boundary and are the only access point provided on the site;</li> <li>(ii) have a width between 3.0 metres and 3.2 metres measured at the property boundary and no more than two access points are provided on site, separated by no less than 1m.</li> </ul> </li> </ul>
<p>PO 23.4</p> <p>Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.</p>	<p>DTS/DPF 23.4</p> <p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <ul style="list-style-type: none"> <li>(a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land</li> <li>(b) where newly proposed, is set back: <ul style="list-style-type: none"> <li>(i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner</li> <li>(ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance</li> <li>(iii) 6m or more from the tangent point of an intersection of 2 or more roads</li> <li>(iv) outside of the marked lines or infrastructure</li> </ul> </li> </ul>

	dedicating a pedestrian crossing.
<p>PO 23.5</p> <p>Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.</p>	<p>DTS/DPF 23.5</p> <p>Driveways are designed and sited so that:</p> <ul style="list-style-type: none"> <li>(a) the gradient from the place of access on the boundary of the allotment to the finished floor level at the front of the garage or carport is not steeper than 1-in-4 on average</li> <li>(b) they are aligned relative to the street so that there is no more than a 20 degree deviation from 90 degrees between the centreline of any dedicated car parking space to which it provides access (measured from the front of that space) and the road boundary.</li> <li>(c) if located so as to provide access from an alley, lane or right of way - the alley, lane or right of way is at least 6.2m wide along the boundary of the allotment / site</li> </ul>
<p>PO 23.6</p> <p>Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.</p>	<p>DTS/DPF 23.6</p> <p>Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements:</p> <ul style="list-style-type: none"> <li>(a) minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number)</li> <li>(b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly</li> <li>(c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.</li> </ul>
Waste storage	
<p>PO 24.1</p> <p>Provision is made for the convenient storage of waste bins in a location screened from public view.</p>	<p>DTS/DPF 24.1</p> <p>Where dwellings abut both side boundaries a waste bin storage area is provided behind the building line of each dwelling that:</p> <ul style="list-style-type: none"> <li>(a) has a minimum area of 2m<sup>2</sup> with a minimum dimension of 900mm (separate from any designated car parking spaces or private open space); and</li> <li>(b) has a continuous unobstructed path of travel (excluding moveable objects like gates, vehicles and roller doors) with a minimum width of 800mm between the waste bin storage area and the street.</li> </ul>
Design of Transportable Buildings	
<p>PO 25.1</p> <p>The sub-floor space beneath transportable buildings is enclosed to give the appearance of a permanent structure.</p>	<p>DTS/DPF 25.1</p> <p>Buildings satisfy (a) or (b):</p> <ul style="list-style-type: none"> <li>(a) are not transportable</li> <li>(b) the sub-floor space between the building and ground level is clad in a material and finish consistent with the building.</li> </ul>
Residential Development - Medium and High Rise (including serviced apartments)	
Outlook and Visual Privacy	
<p>PO 26.1</p>	<p>DTS/DPF 26.1</p>

Ground level dwellings have a satisfactory short range visual outlook to public, communal or private open space.	Buildings:  (a) provide a habitable room at ground or first level with a window facing toward the street (b) limit the height / extent of solid walls or fences facing the street to 1.2m high above the footpath level or, where higher, to 50% of the site frontage.
PO 26.2  The visual privacy of ground level dwellings within multi-level buildings is protected.	DTS/DPF 26.2  The finished floor level of ground level dwellings in multi-storey developments is raised by up to 1.2m.
Private Open Space	
PO 27.1  Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.	DTS/DPF 27.1  Private open space provided in accordance with Design in Urban Areas Table 1 - Private Open Space.
Residential amenity in multi-level buildings	
PO 28.1  Residential accommodation within multi-level buildings have habitable rooms, windows and balconies designed and positioned to be separated from those of other dwellings and accommodation to provide visual and acoustic privacy and allow for natural ventilation and the infiltration of daylight into interior and outdoor spaces.	DTS/DPF 28.1  Habitable rooms and balconies of independent dwellings and accommodation are separated by at least 6m from one another where there is a direct line of sight between them and 3m or more from a side or rear property boundary.
PO 28.2  Balconies are designed, positioned and integrated into the overall architectural form and detail of the development to:  (a) respond to daylight, wind, and acoustic conditions to maximise comfort and provide visual privacy (b) allow views and casual surveillance of the street while providing for safety and visual privacy of nearby living spaces and private outdoor areas.	DTS/DPF 28.2  Balconies utilise one or a combination of the following design elements:  (a) sun screens (b) pergolas (c) louvres (d) green facades (e) openable walls.
PO 28.3  Balconies are of sufficient size and depth to accommodate outdoor seating and promote indoor / outdoor living.	DTS/DPF 28.3  Balconies open directly from a habitable room and incorporate a minimum dimension of 2m.
PO 28.4  Dwellings are provided with sufficient space for storage to meet likely occupant needs.	DTS/DPF 28.4  Dwellings (not including student accommodation or serviced apartments) are provided with storage at the following rates with at least 50% or more of the storage volume to be provided within the dwelling:  (a) studio: not less than 6m <sup>3</sup> (b) 1 bedroom dwelling / apartment: not less than 8m <sup>3</sup> (c) 2 bedroom dwelling / apartment: not less than 10m <sup>3</sup> (d) 3+ bedroom dwelling / apartment: not less than 12m <sup>3</sup> .
PO 28.5  Dwellings that use light wells for access to daylight, outlook and	DTS/DPF 28.5  Light wells:

- (a) are not used as the primary source of outlook for living rooms
- (b) up to 18m in height have a minimum horizontal dimension of 3m, or 6m if overlooked by bedrooms
- (c) above 18m in height have a minimum horizontal dimension of 6m, or 9m if overlooked by bedrooms.

PO 28.6

Attached or abutting dwellings are designed to minimise the transmission of sound between dwellings and, in particular, to protect bedrooms from possible noise intrusions.

DTS/DPF 28.6

None are applicable.

PO 28.7

Dwellings are designed so that internal structural columns correspond with the position of internal walls to ensure that the space within the dwelling/apartment is useable.

DTS/DPF 28.7

None are applicable.

## Dwelling Configuration

PO 29.1

Buildings containing in excess of 10 dwellings provide a variety of dwelling sizes and a range in the number of bedrooms per dwelling to contribute to housing diversity.

DTS/DPF 29.1

Buildings containing in excess of 10 dwellings provide at least one of each of the following:

- (a) studio (where there is no separate bedroom)
- (b) 1 bedroom dwelling / apartment with a floor area of at least 50m<sup>2</sup>
- (c) 2 bedroom dwelling / apartment with a floor area of at least 65m<sup>2</sup>
- (d) 3+ bedroom dwelling / apartment with a floor area of at least 80m<sup>2</sup>, and any dwelling over 3 bedrooms provides an additional 15m<sup>2</sup> for every additional bedroom.

PO 29.2

Dwellings located on the ground floor of multi-level buildings with 3 or more bedrooms have the windows of their habitable rooms overlooking internal courtyard space or other public space, where possible.

DTS/DPF 29.2

None are applicable.

## Common Areas

PO 30.1

The size of lifts, lobbies and corridors is sufficient to accommodate movement of bicycles, strollers, mobility aids and visitor waiting areas.

DTS/DPF 30.1

Common corridor or circulation areas:

- (a) have a minimum ceiling height of 2.7m
- (b) provide access to no more than 8 dwellings
- (c) incorporate a wider section at apartment entries where the corridors exceed 12m in length from a core.

## Group Dwellings, Residential Flat Buildings and Battle axe Developments

## Amenity

PO 31.1

Dwellings are of a suitable size to provide a high standard of amenity for occupants.

DTS/DPF 31.1

Dwellings have a minimum internal floor area in accordance with the following table:

<b>Number of bedrooms</b>	<b>Minimum internal floor area</b>
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	Studio	35m <sup>2</sup>
	1 bedroom	50m <sup>2</sup>
	2 bedroom	65m <sup>2</sup>
	3+ bedrooms	80m <sup>2</sup> and any dwelling over 3 bedrooms provides an additional 15m <sup>2</sup> for every additional bedroom
PO 31.2  The orientation and siting of buildings minimises impacts on the amenity, outlook and privacy of occupants and neighbours.	DTS/DPF 31.2  None are applicable.	
PO 31.3  Development maximises the number of dwellings that face public open space and public streets and limits dwellings oriented towards adjoining properties.	DTS/DPF 31.3  None are applicable.	
PO 31.4  Battle-axe development is appropriately sited and designed to respond to the existing neighbourhood context.	DTS/DPF 31.4  Dwelling sites/allotments are not in the form of a battle-axe arrangement.	
Communal Open Space		
PO 32.1  Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.	DTS/DPF 32.1  None are applicable.	
PO 32.2  Communal open space is of sufficient size and dimensions to cater for group recreation.	DTS/DPF 32.2  Communal open space incorporates a minimum dimension of 5 metres.	
PO 32.3  Communal open space is designed and sited to:  (a) be conveniently accessed by the dwellings which it services (b) have regard to acoustic, safety, security and wind effects.	DTS/DPF 32.3  None are applicable.	
PO 32.4  Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.	DTS/DPF 32.4  None are applicable.	
PO 32.5  Communal open space is designed and sited to:  (a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings (b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive	DTS/DPF 32.5  None are applicable.	

surveillance.	
Car parking, access and manoeuvrability	
PO 33.1  Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.	DTS/DPF 33.1  Where on-street parking is available directly adjacent the site, on-street parking is retained adjacent the subject site in accordance with the following requirements:  (a) minimum 0.33 on-street car parks per proposed dwelling (rounded up to the nearest whole number) (b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly (c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.
PO 33.2  The number of vehicular access points onto public roads is minimised to reduce interruption of the footpath and positively contribute to public safety and walkability.	DTS/DPF 33.2  Access to group dwellings or dwellings within a residential flat building is provided via a single common driveway.
PO 33.3  Residential driveways that service more than one dwelling are designed to allow safe and convenient movement.	DTS/DPF 33.3  Driveways that service more than 1 dwelling or a dwelling on a battle-axe site:  (a) have a minimum width of 3m (b) for driveways servicing more than 3 dwellings: (i) have a width of 5.5m or more and a length of 6m or more at the kerb of the primary street (ii) where the driveway length exceeds 30m, incorporate a passing point at least every 30 metres with a minimum width of 5.5m and a minimum length of 6m.
PO 33.4  Residential driveways that service more than one dwelling or a dwelling on a battle-axe site are designed to allow passenger vehicles to enter and exit and manoeuvre within the site in a safe and convenient manner.	DTS/DPF 33.4  Driveways providing access to more than one dwelling, or a dwelling on a battle-axe site, allow a B85 passenger vehicle to enter and exit the garages or parking spaces in no more than a three-point turn manoeuvre.
PO 33.5  Dwellings are adequately separated from common driveways and manoeuvring areas.	DTS/DPF 33.5  Dwelling walls with entry doors or ground level habitable room windows are set back at least 1.5m from any driveway or area designated for the movement and manoeuvring of vehicles.
Soft landscaping	
PO 34.1  Soft landscaping is provided between dwellings and common driveways to improve the outlook for occupants and appearance of common areas.	DTS/DPF 34.1  Other than where located directly in front of a garage or building entry, soft landscaping with a minimum dimension of 1m is provided between a dwelling and common driveway.
PO 34.2  Battle-axe or common driveways incorporate landscaping and	DTS/DPF 34.2  Battle-axe or common driveways satisfy (a) and (b):

permeability to improve appearance and assist in stormwater management.	<p>(a) are constructed of a minimum of 50% permeable or porous material</p> <p>(b) where the driveway is located directly adjacent the side or rear boundary of the site, soft landscaping with a minimum dimension of 1m is provided between the driveway and site boundary (excluding along the perimeter of a passing point).</p>
Site Facilities / Waste Storage	
<p>PO 35.1</p> <p>Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.</p>	<p>DTS/DPF 35.1</p> <p>None are applicable.</p>
<p>PO 35.2</p> <p>Provision is made for suitable external clothes drying facilities.</p>	<p>DTS/DPF 35.2</p> <p>None are applicable.</p>
<p>PO 35.3</p> <p>Provision is made for suitable household waste and recyclable material storage facilities which are:</p> <p>(a) located away, or screened, from public view, and</p> <p>(b) conveniently located in proximity to dwellings and the waste collection point.</p>	<p>DTS/DPF 35.3</p> <p>None are applicable.</p>
<p>PO 35.4</p> <p>Waste and recyclable material storage areas are located away from dwellings.</p>	<p>DTS/DPF 35.4</p> <p>Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.</p>
<p>PO 35.5</p> <p>Where waste bins cannot be conveniently collected from the street, provision is made for on-site waste collection, designed to accommodate the safe and convenient access, egress and movement of waste collection vehicles.</p>	<p>DTS/DPF 35.5</p> <p>None are applicable.</p>
<p>PO 35.6</p> <p>Services including gas and water meters are conveniently located and screened from public view.</p>	<p>DTS/DPF 35.6</p> <p>None are applicable.</p>
Water sensitive urban design	
<p>PO 36.1</p> <p>Residential development creating a common driveway / access includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system, watercourses or other water bodies.</p>	<p>DTS/DPF 36.1</p> <p>None are applicable.</p>
<p>PO 36.2</p> <p>Residential development creating a common driveway / access includes a stormwater management system designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the</p>	<p>DTS/DPF 36.2</p> <p>None are applicable.</p>

development does not increase the peak flows in downstream systems.	
Supported Accommodation and retirement facilities	
Siting, Configuration and Design	
PO 37.1 Supported accommodation and housing for aged persons and people with disabilities is located where on-site movement of residents is not unduly restricted by the slope of the land.	DTS/DPF 37.1 None are applicable.
PO 37.2 Universal design features are incorporated to provide options for people living with disabilities or limited mobility and / or to facilitate ageing in place.	DTS/DPF 37.2 None are applicable.
Movement and Access	
PO 38.1 Development is designed to support safe and convenient access and movement for residents by providing:  (a) ground-level access or lifted access to all units (b) level entry porches, ramps, paths, driveways, passenger loading areas and areas adjacent to footpaths that allow for the passing of wheelchairs and resting places (c) car parks with gradients no steeper than 1-in-40, and of sufficient area to provide for wheelchair manoeuvrability (d) kerb ramps at pedestrian crossing points.	DTS/DPF 38.1 None are applicable.
Communal Open Space	
PO 39.1 Development is designed to provide attractive, convenient and comfortable indoor and outdoor communal areas to be used by residents and visitors.	DTS/DPF 39.1 None are applicable.
PO 39.2 Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.	DTS/DPF 39.2 None are applicable.
PO 39.3 Communal open space is of sufficient size and dimensions to cater for group recreation.	DTS/DPF 39.3 Communal open space incorporates a minimum dimension of 5 metres.
PO 39.4 Communal open space is designed and sited to:  (a) be conveniently accessed by the dwellings which it services (b) have regard to acoustic, safety, security and wind effects.	DTS/DPF 39.4 None are applicable.
PO 39.5 Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.	DTS/DPF 39.5 None are applicable.



PO 39.6	DTS/DPF 39.6
Communal open space is designed and sited to:	None are applicable.
(a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings	
(b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive surveillance.	
Site Facilities / Waste Storage	
PO 40.1	DTS/DPF 40.1
Development is designed to provide storage areas for personal items and specialised equipment such as small electric powered vehicles, including facilities for the recharging of small electric-powered vehicles.	None are applicable.
PO 40.2	DTS/DPF 40.2
Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.	None are applicable.
PO 40.3	DTS/DPF 40.3
Provision is made for suitable external clothes drying facilities.	None are applicable.
PO 40.4	DTS/DPF 40.4
Provision is made for suitable household waste and recyclable material storage facilities conveniently located away, or screened, from view.	None are applicable.
PO 40.5	DTS/DPF 40.5
Waste and recyclable material storage areas are located away from dwellings.	Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.
PO 40.6	DTS/DPF 40.6
Provision is made for on-site waste collection where 10 or more bins are to be collected at any one time.	None are applicable.
PO 40.7	DTS/DPF 40.7
Services, including gas and water meters, are conveniently located and screened from public view.	None are applicable.
Student Accommodation	
PO 41.1	DTS/DPF 41.1
Student accommodation is designed to provide safe, secure, attractive, convenient and comfortable living conditions for residents, including an internal layout and facilities that are designed to provide sufficient space and amenity for the requirements of student life and promote social interaction.	<p>Student accommodation provides:</p> <ul style="list-style-type: none"> <li>(a) a range of living options to meet a variety of accommodation needs, such as one-bedroom, two-bedroom and disability access units</li> <li>(b) common or shared facilities to enable a more efficient use of space, including: <ul style="list-style-type: none"> <li>(i) shared cooking, laundry and external drying</li> </ul> </li> </ul>

	<p>facilities</p> <ul style="list-style-type: none"> <li>(ii) internal and external communal and private open space provided in accordance with Design in Urban Areas Table 1 - Private Open Space</li> <li>(iii) common storage facilities at the rate of 8m<sup>3</sup> for every 2 dwellings or students</li> <li>(iv) common on-site parking in accordance with Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas</li> <li>(v) bicycle parking at the rate of one space for every 2 students.</li> </ul>
<p>PO 41.2</p> <p>Student accommodation is designed to provide easy adaptation of the building to accommodate an alternative use of the building in the event it is no longer required for student housing.</p>	<p>DTS/DPF 41.2</p> <p>None are applicable.</p>
All non-residential development	
Water Sensitive Design	
<p>PO 42.1</p> <p>Development likely to result in risk of export of sediment, suspended solids, organic matter, nutrients, oil and grease include stormwater management systems designed to minimise pollutants entering stormwater.</p>	<p>DTS/DPF 42.1</p> <p>None are applicable.</p>
<p>PO 42.2</p> <p>Water discharged from a development site is of a physical, chemical and biological condition equivalent to or better than its pre-developed state.</p>	<p>DTS/DPF 42.2</p> <p>None are applicable.</p>
<p>PO 42.3</p> <p>Development includes stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that development does not increase peak flows in downstream systems.</p>	<p>DTS/DPF 42.3</p> <p>None are applicable.</p>
Wash-down and Waste Loading and Unloading	
<p>PO 43.1</p> <p>Areas for activities including loading and unloading, storage of waste refuse bins in commercial and industrial development or wash-down areas used for the cleaning of vehicles, plant or equipment are:</p> <ul style="list-style-type: none"> <li>(a) designed to contain all wastewater likely to pollute stormwater within a bunded and roofed area to exclude the entry of external surface stormwater run-off</li> <li>(b) paved with an impervious material to facilitate wastewater collection</li> <li>(c) of sufficient size to prevent 'splash-out' or 'over-spray' of wastewater from the wash-down area</li> <li>(d) are designed to drain wastewater to either: <ul style="list-style-type: none"> <li>(i) a treatment device such as a sediment trap and</li> </ul> </li> </ul>	<p>DTS/DPF 43.1</p> <p>None are applicable.</p>

<p>coalescing plate oil separator with subsequent disposal to a sewer, private or Community Wastewater Management Scheme</p> <p>or</p> <p>(ii) a holding tank and its subsequent removal off-site on a regular basis.</p>	
Laneway Development	
Infrastructure and Access	
<p>PO 44.1</p> <p>Development with a primary street comprising a laneway, alley, lane, right of way or similar minor thoroughfare only occurs where:</p> <ul style="list-style-type: none"> <li>(a) existing utility infrastructure and services are capable of accommodating the development</li> <li>(b) the primary street can support access by emergency and regular service vehicles (such as waste collection)</li> <li>(c) it does not require the provision or upgrading of infrastructure on public land (such as footpaths and stormwater management systems)</li> <li>(d) safety of pedestrians or vehicle movement is maintained</li> <li>(e) any necessary grade transition is accommodated within the site of the development to support an appropriate development intensity and orderly development of land fronting minor thoroughfares.</li> </ul>	<p>DTS/DPF 44.1</p> <p>Development with a primary street frontage that is not an alley, lane, right of way or similar public thoroughfare.</p>

Table 1 - Private Open Space

Dwelling Type	Dwelling / Site Configuration	Minimum Rate
Dwelling (at ground level, other than a residential flat building that includes above ground dwellings)		<p>Total private open space area:</p> <ul style="list-style-type: none"> <li>(a) Site area &lt;301m<sup>2</sup>: 24m<sup>2</sup> located behind the building line.</li> <li>(b) Site area ≥ 301m<sup>2</sup>: 60m<sup>2</sup> located behind the building line.</li> </ul> <p>Minimum directly accessible from a living room: 16m<sup>2</sup> / with a minimum dimension 3m.</p>
Cabin or caravan (permanently fixed to the ground) in a residential park or caravan and tourist park		Total area: 16m <sup>2</sup> , which may be used as second car parking space, provided on each site intended for residential occupation.
Dwelling in a residential flat building or mixed use building which incorporate above ground level dwellings	Dwellings at ground level:	15m <sup>2</sup> / minimum dimension 3m
	Dwellings above ground level:	
	Studio (no separate bedroom)	4m <sup>2</sup> / minimum dimension 1.8m

	One bedroom dwelling	8m <sup>2</sup> / minimum dimension 2.1m
	Two bedroom dwelling	11m <sup>2</sup> / minimum dimension 2.4m
	Three + bedroom dwelling	15 m <sup>2</sup> / minimum dimension 2.6m

## Forestry

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Commercial forestry is designed and sited to maximise economic benefits whilst managing potential negative impacts on the environment, transport networks, surrounding land uses and landscapes.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting	
PO 1.1 Commercial forestry plantations are established where there is no detrimental effect on the physical environment or scenic quality of the rural landscape.	DTS/DPF 1.1 None are applicable.
PO 1.2 Commercial forestry plantations are established on slopes that are stable to minimise the risk of soil erosion.	DTS/DPF 1.2 Commercial forestry plantations are not located on land with a slope exceeding 20% (1-in-5).
PO 1.3 Commercial forestry plantations and operations associated with their establishment, management and harvesting are appropriately set back from any sensitive receiver to minimise fire risk and noise disturbance.	DTS/DPF 1.3 Commercial forestry plantations and operations associated with their establishment, management and harvesting are set back 50m or more from any sensitive receiver.
PO 1.4 Commercial forestry plantations are separated from reserves gazetted under the <i>National Parks and Wildlife Act 1972</i> and/or <i>Wilderness Protection Act 1992</i> to minimise fire risk and potential for weed infestation.	DTS/DPF 1.4 Commercial forestry plantations and operations associated with their establishment, management and harvesting are set back 50m or more from a reserve gazetted under the <i>National Parks and Wildlife Act 1972</i> and/or <i>Wilderness Protection Act 1992</i> .
Water Protection	
PO 2.1	DTS/DPF 2.1



Commercial forestry plantations incorporate artificial drainage lines (i.e. culverts, runoffs and constructed drains) integrated with natural drainage lines to minimise concentrated water flows onto or from plantation areas.	None are applicable.		
PO 2.2  Appropriate siting, layout and design measures are adopted to minimise the impact of commercial forestry plantations on surface water resources.	DTS/DPF 2.2  Commercial forestry plantations:  (a) do not involve cultivation (excluding spot cultivation) in drainage lines (b) are set back 20m or more from the banks of any major watercourse (a third order or higher watercourse), lake, reservoir, wetland or sinkhole (with direct connection to an aquifer) (c) are set back 10m or more from the banks of any first or second order watercourse or sinkhole ( with no direct connection to an aquifer).		
Fire Management			
PO 3.1  Commercial forestry plantations incorporate appropriate firebreaks and fire management design elements.	DTS/DPF 3.1  Commercial forestry plantations provide:  (a) 7m or more wide external boundary firebreaks for plantations of 40ha or less (b) 10m or more wide external boundary firebreaks for plantations of between 40ha and 100ha (c) 20m or more wide external boundary firebreaks, or 10m with an additional 10m or more of fuel-reduced plantation, for plantations of 100ha or greater.		
PO 3.2  Commercial forestry plantations incorporate appropriate fire management access tracks.	DTS/DPF 3.2  Commercial forestry plantation fire management access tracks:  (a) are incorporated within all firebreaks (b) are 7m or more wide with a vertical clearance of 4m or more (c) are aligned to provide straight through access at junctions, or if they are a no through access track are appropriately signposted and provide suitable turnaround areas for fire-fighting vehicles (d) partition the plantation into units of 40ha or less in area.		
Power-line Clearances			
PO 4.1  Commercial forestry plantations achieve and maintain appropriate clearances from aboveground powerlines.	DTS/DPF 4.1  Commercial forestry plantations incorporating trees with an expected mature height of greater than 6m meet the clearance requirements listed in the following table:		
	Voltage of transmission line	Tower or Pole	Minimum horizontal clearance distance between plantings and transmission lines
	500 kV	Tower	38m

	275 kV	Tower	25m
	132 kV	Tower	30m
	132 kV	Pole	20m
	66 kV	Pole	20m
	Less than 66 kV	Pole	20m

## Housing Renewal

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Renewed residential environments replace older social housing and provide new social housing infrastructure and other housing options and tenures to enhance the residential amenity of the local area.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
PO 1.1 Residential development provides a range of housing choices.	DTS/DPF 1.1 Development comprises one or more of the following:  (a) detached dwellings (b) semi-detached dwellings (c) row dwellings (d) group dwellings (e) residential flat buildings.
PO 1.2 Medium-density housing options or higher are located in close proximity to public transit, open space and/or activity centres.	DTS/DPF 1.2 None are applicable.
Building Height	
PO 2.1 Buildings generally do not exceed 3 building levels unless in locations close to public transport, centres and/or open space.	DTS/DPF 2.1 Building height (excluding garages, carports and outbuildings) does not exceed 3 building levels and 12m and wall height does not exceed 9m (not including a gable end).

PO 2.2	DTS/DPF 2.2
Medium or high rise residential flat buildings located within or at the interface with zones which restrict heights to a maximum of 2 building levels transition down in scale and height towards the boundary of that zone, other than where it is a street boundary.	None are applicable.
Primary Street Setback	
PO 3.1	DTS/DPF 3.1
Buildings are set back from the primary street boundary to contribute to an attractive streetscape character.	Buildings are no closer to the primary street (excluding any balcony, verandah, porch, awning or similar structure) than 3m.
Secondary Street Setback	
PO 4.1	DTS/DPF 4.1
Buildings are set back from secondary street boundaries to maintain separation between building walls and public streets and contribute to a suburban streetscape character.	Buildings are set back at least 900mm from the boundary of the allotment with a secondary street frontage.
Boundary Walls	
PO 5.1	DTS/DPF 5.1
Boundary walls are limited in height and length to manage visual impacts and access to natural light and ventilation.	<p>Except where the dwelling is located on a central site within a row dwelling or terrace arrangement, dwellings with side boundary walls are sited on only one side boundary and satisfy (a) or (b):</p> <ul style="list-style-type: none"> <li>(a) adjoin or abut a boundary wall of a building on adjoining land for the same length and height</li> <li>(b) do not: <ul style="list-style-type: none"> <li>(i) exceed 3.2m in height from the lower of the natural or finished ground level</li> <li>(ii) exceed 11.5m in length</li> <li>(iii) when combined with other walls on the boundary of the subject development site, a maximum 45% of the length of the boundary</li> <li>(iv) encroach within 3 metres of any other existing or proposed boundary walls on the subject land.</li> </ul> </li> </ul>
PO 5.2	DTS/DPF 5.2
Dwellings in a semi-detached, row or terrace arrangement maintain space between buildings consistent with a suburban streetscape character.	Dwellings in a semi-detached or row arrangement are set back 900mm or more from side boundaries shared with allotments outside the development site, except for a carport or garage.
Side Boundary Setback	
PO 6.1	DTS/DPF 6.1
<p>Buildings are set back from side boundaries to provide:</p> <ul style="list-style-type: none"> <li>(a) separation between dwellings in a way that contributes to a suburban character</li> <li>(b) access to natural light and ventilation for neighbours.</li> </ul>	<p>Other than walls located on a side boundary, buildings are set back from side boundaries:</p> <ul style="list-style-type: none"> <li>(a) at least 900mm where the wall height is up to 3m</li> <li>(b) other than for a wall facing a southern side boundary, at least 900mm plus 1/3 of the wall height above 3m</li> <li>(c) at least 1.9m plus 1/3 of the wall height above 3m for walls facing a southern side boundary.</li> </ul>

Rear Boundary Setback	
<p>PO 7.1</p> <p>Buildings are set back from rear boundaries to provide:</p> <ul style="list-style-type: none"> <li>(a) separation between dwellings in a way that contributes to a suburban character</li> <li>(b) access to natural light and ventilation for neighbours</li> <li>(c) private open space</li> <li>(d) space for landscaping and vegetation.</li> </ul>	<p>DTS/DPF 7.1</p> <p>Dwellings are set back from the rear boundary:</p> <ul style="list-style-type: none"> <li>(a) 3m or more for the first building level</li> <li>(b) 5m or more for any subsequent building level.</li> </ul>
Buildings elevation design	
<p>PO 8.1</p> <p>Dwelling elevations facing public streets and common driveways make a positive contribution to the streetscape and common driveway areas.</p>	<p>DTS/DPF 8.1</p> <p>Each dwelling includes at least 3 of the following design features within the building elevation facing a primary street, and at least 2 of the following design features within the building elevation facing any other public road (other than a laneway) or a common driveway:</p> <ul style="list-style-type: none"> <li>(a) a minimum of 30% of the building elevation is set back an additional 300mm from the building line</li> <li>(b) a porch or portico projects at least 1m from the building elevation</li> <li>(c) a balcony projects from the building elevation</li> <li>(d) a verandah projects at least 1m from the building elevation</li> <li>(e) eaves of a minimum 400mm width extend along the width of the front elevation</li> <li>(f) a minimum 30% of the width of the upper level projects forward from the lower level primary building line by at least 300mm.</li> <li>(g) a minimum of two different materials or finishes are incorporated on the walls of the building elevation, with a maximum of 80% of the building elevation in a single material or finish.</li> </ul>
<p>PO 8.2</p> <p>Dwellings incorporate windows along primary street frontages to encourage passive surveillance and make a positive contribution to the streetscape.</p>	<p>DTS/DPF 8.2</p> <p>Each dwelling with a frontage to a public street:</p> <ul style="list-style-type: none"> <li>(a) includes at least one window facing the primary street from a habitable room that has a minimum internal room dimension of 2.4m</li> <li>(b) has an aggregate window area of at least 2m<sup>2</sup> facing the primary street</li> </ul>
<p>PO 8.3</p> <p>The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.</p>	<p>DTS/DPF 8.3</p> <p>None are applicable.</p>
<p>PO 8.4</p> <p>Built form considers local context and provides a quality design response through scale, massing, materials, colours and architectural expression.</p>	<p>DTS/DPF 8.4</p> <p>None are applicable.</p>
<p>PO 8.5</p>	<p>DTS/DPF 8.5</p>



Entrances to multi-storey buildings are:  (a)    oriented towards the street (b)    visible and easily identifiable from the street (c)    designed to include a common mail box structure.	None are applicable.		
Outlook and amenity			
PO 9.1  Living rooms have an external outlook to provide a high standard of amenity for occupants.	DTS/DPF 9.1  A living room of a dwelling incorporates a window with an external outlook towards the street frontage or private open space.		
PO 9.2  Bedrooms are separated or shielded from active communal recreation areas, common access areas and vehicle parking areas and access ways to mitigate noise and artificial light intrusion.	DTS/DPF 9.2  None are applicable.		
Private Open Space			
PO 10.1  Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.	DTS/DPF 10.1  Private open space is provided in accordance with the following table:		
	Dwelling Type	Dwelling / Site Configuration	Minimum Rate
	Dwelling (at ground level)		Total area: 24m <sup>2</sup> located behind the building line  Minimum adjacent to a living room: 16m <sup>2</sup> with a minimum dimension 3m
	Dwelling (above ground level)	Studio	4m <sup>2</sup> / minimum dimension 1.8m
		One bedroom dwelling	8m <sup>2</sup> / minimum dimension 2.1m
		Two bedroom dwelling	11m <sup>2</sup> / minimum dimension 2.4m
Three + bedroom dwelling		15 m <sup>2</sup> / minimum dimension 2.6m	
PO 10.2  Private open space positioned to provide convenient access from internal living areas.	DTS/DPF 10.2  At least 50% of the required area of private open space is accessible from a habitable room.		

PO 10.3	DTS/DPF 10.3				
Private open space is positioned and designed to:	None are applicable.				
<ul style="list-style-type: none"> <li>(a) provide useable outdoor space that suits the needs of occupants;</li> <li>(b) take advantage of desirable orientation and vistas; and</li> <li>(c) adequately define public and private space.</li> </ul>					
Visual privacy					
PO 11.1	DTS/DPF 11.1				
Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses.	Upper level windows facing side or rear boundaries shared with another residential allotment/site satisfy one of the following: <ul style="list-style-type: none"> <li>(a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 200mm</li> <li>(b) have sill heights greater than or equal to 1.5m above finished floor level</li> <li>(c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5m above the finished floor.</li> </ul>				
PO 11.2	DTS/DPF 11.2				
Development mitigates direct overlooking from upper level balconies and terraces to habitable rooms and private open space of adjoining residential uses.	One of the following is satisfied: <ul style="list-style-type: none"> <li>(a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace</li> <li>or</li> <li>(b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: <ul style="list-style-type: none"> <li>(i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land</li> <li>or</li> <li>(ii) 1.7m above finished floor level in all other cases</li> </ul> </li> </ul>				
Landscaping					
PO 12.1	DTS/DPF 12.1				
Soft landscaping is incorporated into development to:	Residential development incorporates pervious areas for soft landscaping with a minimum dimension of 700mm provided in accordance with (a) and (b): <ul style="list-style-type: none"> <li>(a) a total area as determined by the following table:</li> </ul> <table border="1"> <tr> <th>Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m<sup>2</sup>)</th><th>Minimum percentage of site</th></tr> <tr> <td>&lt;150</td><td>10%</td></tr> </table>	Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m <sup>2</sup> )	Minimum percentage of site	<150	10%
Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m <sup>2</sup> )	Minimum percentage of site				
<150	10%				
<ul style="list-style-type: none"> <li>(a) minimise heat absorption and reflection</li> <li>(b) maximise shade and shelter</li> <li>(c) maximise stormwater infiltration and biodiversity</li> <li>(d) enhance the appearance of land and streetscapes.</li> </ul>					

	<200	15%
	200-450	20%
	>450	25%
	(b) at least 30% of land between the road boundary and the building line.	
Water Sensitive Design		
PO 13.1  Residential development is designed to capture and use stormwater to:  (a) maximise efficient use of water resources (b) manage peak stormwater runoff flows and volume to ensure the carrying capacities of downstream systems are not overloaded (c) manage runoff quality to maintain, as close as practical, pre-development conditions.	DTS/DPF 13.1  None are applicable.	
Car Parking		
PO 14.1  On-site car parking is provided to meet the anticipated demand of residents, with less on-site parking in areas in close proximity to public transport.	DTS/DPF 14.1  On-site car parking is provided at the following rates per dwelling:  (a) 2 or fewer bedrooms - 1 car parking space (b) 3 or more bedrooms - 2 car parking spaces.	
PO 14.2  Enclosed car parking spaces are of dimensions to be functional, accessible and convenient.	DTS/DPF 14.2  Residential parking spaces enclosed by fencing, walls or other obstructions with the following internal dimensions (separate from any waste storage area):  (a) single parking spaces: (i) a minimum length of 5.4m (ii) a minimum width of 3.0m (iii) a minimum garage door width of 2.4m  (b) double parking spaces (side by side): (i) a minimum length of 5.4m (ii) a minimum width of 5.5m (iii) minimum garage door width of 2.4m per space.	
PO 14.3  Uncovered car parking spaces are of dimensions to be functional, accessible and convenient.	DTS/DPF 14.3  Uncovered car parking spaces have:  (a) a minimum length of 5.4m (b) a minimum width of 2.4m (c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m.	
PO 14.4  Residential flat buildings and group dwelling developments provide sufficient on-site visitor car parking to cater for anticipated demand.	DTS/DPF 14.4  Visitor car parking for group and residential flat buildings incorporating 4 or more dwellings is provided on-site at a minimum ratio of 0.25 car parking spaces per dwelling.	

PO 14.5 Residential flat buildings provide dedicated areas for bicycle parking.	DTS/DPF 14.5 Residential flat buildings provide one bicycle parking space per dwelling.
Overshadowing	
PO 15.1 Development minimises overshadowing of the private open spaces of adjoining land by ensuring that ground level open space associated with residential buildings receive direct sunlight for a minimum of 2 hours between 9am and 3pm on 21 June.	DTS/DPF 15.1 None are applicable.
Waste	
PO 16.1 Provision is made for the convenient storage of waste bins in a location screened from public view.	DTS/DPF 16.1 A waste bin storage area is provided behind the primary building line that:  (a) has a minimum area of 2m <sup>2</sup> with a minimum dimension of 900mm (separate from any designated car parking spaces or private open space).; and (b) has a continuous unobstructed path of travel (excluding moveable objects like gates, vehicles and roller doors) with a minimum width of 800mm between the waste bin storage area and the street.
PO 16.2 Residential flat buildings provide a dedicated area for the on-site storage of waste which is:  (a) easily and safely accessible for residents and for collection vehicles (b) screened from adjoining land and public roads (c) of sufficient dimensions to be able to accommodate the waste storage needs of the development considering the intensity and nature of the development and the frequency of collection.	DTS/DPF 16.2 None are applicable.
Vehicle Access	
PO 17.1 Driveways are located and designed to facilitate safe access and egress while maximising land available for street tree planting, landscaped street frontages and on-street parking.	DTS/DPF 17.1 None are applicable.
PO 17.2 Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.	DTS/DPF 17.2 Vehicle access to designated car parking spaces satisfy (a) or (b):  (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner



	<ul style="list-style-type: none"> <li>(ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance</li> <li>(iii) 6m or more from the tangent point of an intersection of 2 or more roads</li> <li>(iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.</li> </ul>
<p>PO 17.3</p> <p>Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.</p>	<p>DTS/DPF 17.3</p> <p>Driveways are designed and sited so that:</p> <ul style="list-style-type: none"> <li>(a) the gradient from the place of access on the boundary of the allotment to the finished floor level at the front of the garage or carport is not more than 1-in-4 on average</li> <li>(b) they are aligned relative to the street so that there is no more than a 20 degree deviation from 90 degrees between the centreline of any dedicated car parking space to which it provides access (measured from the front of that space) and the road boundary.</li> <li>(c) if located so as to provide access from an alley, lane or right of way - the alley, lane or right of way is at least 6.2m wide along the boundary of the allotment / site.</li> </ul>
<p>PO 17.4</p> <p>Driveways and access points are designed and distributed to optimise the provision of on-street parking.</p>	<p>DTS/DPF 17.4</p> <p>Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements:</p> <ul style="list-style-type: none"> <li>1. minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number)</li> <li>2. Minimum car park length of 5.4m where a vehicle can enter or exit a space directly</li> <li>3. minimum car park length of 6m for an intermediate space located between two other parking spaces.</li> </ul>
<p>PO 17.5</p> <p>Residential driveways that service more than one dwelling of a dimension to allow safe and convenient movement.</p>	<p>DTS/DPF 17.5</p> <p>Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements:</p> <ul style="list-style-type: none"> <li>(a) minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number)</li> <li>(b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly</li> <li>(c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.</li> </ul>
<p>PO 17.6</p> <p>Residential driveways that service more than one dwelling are designed to allow passenger vehicles to enter and exit the site and manoeuvre within the site in a safe and convenient manner.</p>	<p>DTS/DPF 17.6</p> <p>Driveways providing access to more than one dwelling, or a dwelling on a battle-axe site, allow a B85 passenger vehicle to enter and exit the garages or parking spaces in no more than a three-point turn manoeuvre</p>
<p>PO 17.7</p> <p>Dwellings are adequately separated from common driveways and manoeuvring areas.</p>	<p>DTS/DPF 17.7</p> <p>Dwelling walls with entry doors or ground level habitable room windows are set back at least 1.5m from any driveway or area</p>

	designated for the movement and manoeuvring of vehicles.
Storage	
PO 18.1  Dwellings are provided with sufficient and accessible space for storage to meet likely occupant needs.	DTS/DPF 18.1  Dwellings are provided with storage at the following rates and 50% or more of the storage volume is provided within the dwelling:  (a) studio: not less than 6m <sup>3</sup> (b) 1 bedroom dwelling / apartment: not less than 8m <sup>3</sup> (c) 2 bedroom dwelling / apartment: not less than 10m <sup>3</sup> (d) 3+ bedroom dwelling / apartment: not less than 12m <sup>3</sup> .
Earthworks	
PO 19.1  Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.	DTS/DPF 19.1  The development does not involve:  (a) excavation exceeding a vertical height of 1m or (b) filling exceeding a vertical height of 1m or (c) a total combined excavation and filling vertical height exceeding 2m.
Service connections and infrastructure	
PO 20.1  Dwellings are provided with appropriate service connections and infrastructure.	DTS/DPF 20.1  The site and building:  (a) have the ability to be connected to a permanent potable water supply (b) have the ability to be connected to a sewerage system, or a wastewater system approved under the <i>South Australian Public Health Act 2011</i> (c) have the ability to be connected to electricity supply (d) have the ability to be connected to an adequate water supply (and pressure) for fire-fighting purposes (e) would not be contrary to the Regulations prescribed for the purposes of Section 86 of the <i>Electricity Act 1996</i> .
Site contamination	
PO 21.1  Land that is suitable for sensitive land uses to provide a safe environment.	DTS/DPF 21.1  Development satisfies (a), (b), (c) or (d):  (a) does not involve a change in the use of land (b) involves a change in the use of land that does not constitute a change to a <u>more sensitive use</u> (c) involves a change in the use of land to a <u>more sensitive use</u> on land at which <u>site contamination</u> does not exist (as demonstrated in a <u>site contamination declaration form</u> ) (d) involves a change in the use of land to a <u>more sensitive use</u> on land at which <u>site contamination</u> exists, or may exist (as demonstrated in a site contamination declaration form), and satisfies both of the following: (i) a <u>site contamination audit report</u> has been

	<p>prepared under Part 10A of the <i>Environment Protection Act 1993</i> in relation to the land within the previous 5 years which states that</p> <p>A. <u>site contamination</u> does not exist (or no longer exists) at the land or</p> <p>B. the land is suitable for the proposed use or range of uses (without the need for any further <u>remediation</u>) or</p> <p>C. where <u>remediation</u> is, or remains, necessary for the proposed use (or range of uses), <u>remediation work</u> has been carried out or will be carried out (and the applicant has provided a written undertaking that the remediation works will be implemented in association with the development)</p> <p>and</p> <p>(ii) no other <u>class 1 activity</u> or <u>class 2 activity</u> has taken place at the land since the preparation of the site contamination audit report (as demonstrated in a <u>site contamination declaration form</u>).</p>
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## Infrastructure and Renewable Energy Facilities

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Efficient provision of infrastructure networks and services, renewable energy facilities and ancillary development in a manner that minimises hazard, is environmentally and culturally sensitive and manages adverse visual impacts on natural and rural landscapes and residential amenity.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
General	
PO 1.1 Development is located and designed to minimise hazard or nuisance to adjacent development and land uses.	DTS/DPF 1.1 None are applicable.
Visual Amenity	
PO 2.1	DTS/DPF 2.1

<p>The visual impact of above-ground infrastructure networks and services (excluding high voltage transmission lines), renewable energy facilities (excluding wind farms), energy storage facilities and ancillary development is minimised from townships, scenic routes and public roads by:</p> <ul style="list-style-type: none"> <li>(a) utilising features of the natural landscape to obscure views where practicable</li> <li>(b) siting development below ridgelines where practicable</li> <li>(c) avoiding visually sensitive and significant landscapes</li> <li>(d) using materials and finishes with low-reflectivity and colours that complement the surroundings</li> <li>(e) using existing vegetation to screen buildings</li> <li>(f) incorporating landscaping or landscaped mounding around the perimeter of a site and between adjacent allotments accommodating or zoned to primarily accommodate sensitive receivers.</li> </ul>	None are applicable.
<p>PO 2.2</p> <p>Pumping stations, battery storage facilities, maintenance sheds and other ancillary structures incorporate vegetation buffers to reduce adverse visual impacts on adjacent land.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
<p>PO 2.3</p> <p>Surfaces exposed by earthworks associated with the installation of storage facilities, pipework, penstock, substations and other ancillary plant are reinstated and revegetated to reduce adverse visual impacts on adjacent land.</p>	<p>DTS/DPF 2.3</p> <p>None are applicable.</p>
Rehabilitation	
<p>PO 3.1</p> <p>Progressive rehabilitation (incorporating revegetation) of disturbed areas, ahead of or upon decommissioning of areas used for renewable energy facilities and transmission corridors.</p>	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>
Hazard Management	
<p>PO 4.1</p> <p>Infrastructure and renewable energy facilities and ancillary development located and operated to not adversely impact maritime or air transport safety, including the operation of ports, airfields and landing strips.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p> <p>Facilities for energy generation, power storage and transmission are separated as far as practicable from dwellings, tourist accommodation and frequently</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>



visited public places (such as viewing platforms / lookouts) to reduce risks to public safety from fire or equipment malfunction.	
PO 4.3  Bushfire hazard risk is minimised for renewable energy facilities by providing appropriate access tracks, safety equipment and water tanks and establishing cleared areas around substations, battery storage and operations compounds.	DTS/DPF 4.3  None are applicable.
Electricity Infrastructure and Battery Storage Facilities	
PO 5.1  Electricity infrastructure is located to minimise visual impacts through techniques including:  (a) siting utilities and services: (i) on areas already cleared of native vegetation (ii) where there is minimal interference or disturbance to existing native vegetation or biodiversity  (b) grouping utility buildings and structures with non-residential development, where practicable.	DTS/DPF 5.1  None are applicable.
PO 5.2  Electricity supply (excluding transmission lines) serving new development in urban areas and townships installed underground, excluding lines having a capacity exceeding or equal to 33kV.	DTS/DPF 5.2  None are applicable.
PO 5.3  Battery storage facilities are co-located with substation infrastructure where practicable to minimise the development footprint and reduce environmental impacts.	DTS/DPF 5.3  None are applicable.
Telecommunication Facilities	
PO 6.1  The proliferation of telecommunications facilities in the form of towers/monopoles in any one locality is managed, where technically feasible, by co-locating a facility with other communications facilities to mitigate impacts from clutter on visual amenity.	DTS/DPF 6.1  None are applicable.
PO 6.2  Telecommunications antennae are located as close as practicable to support structures to manage overall bulk and mitigate impacts on visual amenity.	DTS/DPF 6.2  None are applicable.
PO 6.3	DTS/DPF 6.3

<p>Telecommunications facilities, particularly towers/monopoles, are located and sized to mitigate visual impacts by the following methods:</p> <p>(a) where technically feasible, incorporating the facility within an existing structure that may serve another purpose</p> <p>or all of the following:</p> <p>(b) using existing buildings and landscape features to obscure or interrupt views of a facility from nearby public roads, residential areas and places of high public amenity to the extent practical without unduly hindering the effective provision of telecommunications services</p> <p>(c) using materials and finishes that complement the environment</p> <p>(d) screening using landscaping and vegetation, particularly for equipment shelters and huts.</p>	<p>None are applicable.</p>
Renewable Energy Facilities	
<p>PO 7.1</p> <p>Renewable energy facilities are located as close as practicable to existing transmission infrastructure to facilitate connections and minimise environmental impacts as a result of extending transmission infrastructure.</p>	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>
Renewable Energy Facilities (Wind Farm)	
<p>PO 8.1</p> <p>Visual impact of wind turbine generators on the amenity of residential and tourist development is reduced through appropriate separation.</p>	<p>DTS/DPF 8.1</p> <p>Wind turbine generators are:</p> <p>(a) set back at least 2000m from the base of a turbine to any of the following zones:</p> <ul style="list-style-type: none"> <li>(i) Rural Settlement Zone</li> <li>(ii) Township Zone</li> <li>(iii) Rural Living Zone</li> <li>(iv) Rural Neighbourhood Zone</li> </ul> <p>with an additional 10m setback per additional metre over 150m overall turbine height (measured from the base of the turbine).</p> <p>(b) set back at least 1500m from the base of the turbine to non-associated (non-stakeholder) dwellings and tourist accommodation</p>
<p>PO 8.2</p> <p>The visual impact of wind turbine generators on natural landscapes is managed by:</p> <p>(a) designing wind turbine generators to be uniform in colour, size and shape</p> <p>(b) coordinating blade rotation and direction</p> <p>(c) mounting wind turbine generators on tubular towers as opposed to lattice towers.</p>	<p>DTS/DPF 8.2</p> <p>None are applicable.</p>
<p>PO 8.3</p>	<p>DTS/DPF 8.3</p>

Wind turbine generators and ancillary development minimise potential for bird and bat strike.	None are applicable.				
PO 8.4  Wind turbine generators incorporate recognition systems or physical markers to minimise the risk to aircraft operations.	DTS/DPF 8.4  No Commonwealth air safety (CASA / ASA) or Defence requirement is applicable.				
PO 8.5  Meteorological masts and guidewires are identifiable to aircraft through the use of colour bands, marker balls, high visibility sleeves or flashing strobes.	DTS/DPF 8.5  None are applicable.				
Renewable Energy Facilities (Solar Power)					
PO 9.1  Ground mounted solar power facilities generating 5MW or more are not located on land requiring the clearance of areas of intact native vegetation or on land of high environmental, scenic or cultural value.	DTS/DPF 9.1  None are applicable.				
PO 9.2  Ground mounted solar power facilities allow for movement of wildlife by:  (a) incorporating wildlife corridors and habitat refuges  (b) avoiding the use of extensive security or perimeter fencing or incorporating fencing that enables the passage of small animals without unreasonably compromising the security of the facility.	DTS/DPF 9.2  None are applicable.				
PO 9.3  Amenity impacts of solar power facilities are minimised through separation from conservation areas and sensitive receivers in other ownership.	DTS/DPF 9.3  Ground mounted solar power facilities are set back from land boundaries, conservation areas and relevant zones in accordance with the following criteria:				
	Generation Capacity	Approximate size of array	Setback from adjoining land boundary	Setback from conservation areas	Setback from Township, Rural Settlement, Rural Neighbourhood and Rural Living Zones <sup>1</sup>
	50MW>	80ha+	30m	500m	2km
	10MW<50MW	16ha-<80ha	25m	500m	1.5km
	5MW<10MW	8ha to <16ha	20m	500m	1km

	1MW<5MW	1.6ha to <8ha	15m	500m	500m
	100kW<1MW	0.5ha<1.6ha	10m	500m	100m
	<100kW	<0.5ha	5m	500m	25m
	Notes:  1. Does not apply when the site of the proposed ground mounted solar power facility is located within one of these zones.				
PO 9.4  Ground mounted solar power facilities incorporate landscaping within setbacks from adjacent road frontages and boundaries of adjacent allotments accommodating non-host dwellings, where balanced with infrastructure access and bushfire safety considerations.	DTS/DPF 9.4  None are applicable.				
Hydropower / Pumped Hydropower Facilities					
PO 10.1  Hydropower / pumped hydropower facility storage is designed and operated to minimise the risk of storage dam failure.	DTS/DPF 10.1  None are applicable.				
PO 10.2  Hydropower / pumped hydropower facility storage is designed and operated to minimise water loss through increased evaporation or system leakage, with the incorporation of appropriate liners, dam covers, operational measures or detection systems.	DTS/DPF 10.2  None are applicable.				
PO 10.3  Hydropower / pumped hydropower facilities on existing or former mine sites minimise environmental impacts from site contamination, including from mine operations or water sources subject to such processes, now or in the future.	DTS/DPF 10.3  None are applicable.				
Water Supply					
PO 11.1  Development is connected to an appropriate water supply to meet the ongoing requirements of the intended use.	DTS/DPF 11.1  Development is connected, or will be connected, to a reticulated water scheme or mains water supply with the capacity to meet the on-going requirements of the development.				
PO 11.2  Dwellings are connected to a reticulated water scheme or mains water supply with the capacity to meet the requirements of the intended use. Where this is not available an appropriate rainwater tank or storage system for domestic use is provided.	DTS/DPF 11.2  A dwelling is connected, or will be connected, to a reticulated water scheme or mains water supply with the capacity to meet the requirements of the development. Where this is not available it is serviced by a rainwater tank or tanks capable of holding at least 50,000 litres of water which is:  (a) exclusively for domestic use				



	(b) connected to the roof drainage system of the dwelling.
Wastewater Services	
<p>PO 12.1</p> <p>Development is connected to an approved common wastewater disposal service with the capacity to meet the requirements of the intended use. Where this is not available an appropriate on-site service is provided to meet the ongoing requirements of the intended use in accordance with the following:</p> <ul style="list-style-type: none"> <li>(a) it is wholly located and contained within the allotment of the development it will service</li> <li>(b) in areas where there is a high risk of contamination of surface, ground, or marine water resources from on-site disposal of liquid wastes, disposal systems are included to minimise the risk of pollution to those water resources</li> <li>(c) septic tank effluent drainage fields and other wastewater disposal areas are located away from watercourses and flood prone, sloping, saline or poorly drained land to minimise environmental harm.</li> </ul>	<p>DTS/DPF 12.1</p> <p>Development is connected, or will be connected, to an approved common wastewater disposal service with the capacity to meet the requirements of the development. Where this is not available it is instead capable of being serviced by an on-site waste water treatment system in accordance with the following:</p> <ul style="list-style-type: none"> <li>(a) the system is wholly located and contained within the allotment of development it will service; and</li> <li>(b) the system will comply with the requirements of the South Australian Public Health Act 2011.</li> </ul>
<p>PO 12.2</p> <p>Effluent drainage fields and other wastewater disposal areas are maintained to ensure the effective operation of waste systems and minimise risks to human health and the environment.</p>	<p>DTS/DPF 12.2</p> <p>Development is not built on, or encroaches within, an area that is, or will be, required for a sewerage system or waste control system.</p>
Temporary Facilities	
<p>PO 13.1</p> <p>In rural and remote locations, development that is likely to generate significant waste material during construction, including packaging waste, makes provision for a temporary on-site waste storage enclosure to minimise the incidence of wind-blown litter.</p>	<p>DTS/DPF 13.1</p> <p>A waste collection and disposal service is used to dispose of the volume of waste at the rate it is generated.</p>
<p>PO 13.2</p> <p>Temporary facilities to support the establishment of renewable energy facilities (including borrow pits, concrete batching plants, laydown, storage, access roads and worker amenity areas) are sited and operated to minimise environmental impact.</p>	<p>DTS/DPF 13.2</p> <p>None are applicable.</p>

## Intensive Animal Husbandry and Dairies

### Assessment Provisions (AP)

## Desired Outcome

DO 1	Development of intensive animal husbandry and dairies in locations that are protected from encroachment by sensitive receivers and in a manner that minimises their adverse effects on amenity and the environment.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting and Design	
<b>PO 1.1</b> Intensive animal husbandry, dairies and associated activities are sited, designed, constructed and managed to not unreasonably impact on the environment or amenity of the locality.	<b>DTS/DPF 1.1</b> None are applicable.
<b>PO 1.2</b> Intensive animal husbandry, dairies and associated activities are sited, designed, constructed and managed to prevent the potential transmission of disease to other operations where animals are kept.	<b>DTS/DPF 1.2</b> None are applicable.
<b>PO 1.3</b> Intensive animal husbandry and associated activities such as wastewater lagoons and liquid/solid waste disposal areas are sited, designed, constructed and managed to not unreasonably impact on sensitive receivers in other ownership in terms of noise and air emissions.	<b>DTS/DPF 1.3</b> None are applicable.
<b>PO 1.4</b> Dairies and associated activities such as wastewater lagoons and liquid/solid waste disposal areas are sited, designed, constructed and managed to not unreasonably impact on sensitive receivers in other ownership in terms of noise and air emissions.	<b>DTS/DPF 1.4</b> Dairies, associated wastewater lagoon(s) and liquid/solid waste storage and disposal facilities are located 500m or more from the nearest sensitive receiver in other ownership.
<b>PO 1.5</b> Lagoons for the storage or treatment of milking shed effluent is adequately separated from roads to minimise impacts from odour on the general public.	<b>DTS/DPF 1.5</b> Lagoons for the storage or treatment of milking shed effluent are set back 20m or more from public roads.
Waste	
<b>PO 2.1</b> Storage of manure, used litter and other wastes (other than waste water lagoons) is sited, designed, constructed and managed to: <ul style="list-style-type: none"> <li>(a) avoid attracting and harbouring vermin</li> <li>(b) avoid polluting water resources</li> </ul>	<b>DTS/DPF 2.1</b> None are applicable.

(c) be located outside 1% AEP flood event areas.	
Soil and Water Protection	
<p>PO 3.1</p> <p>To avoid environmental harm and adverse effects on water resources, intensive animal husbandry operations are appropriately set back from:</p> <ul style="list-style-type: none"> <li>(a) public water supply reservoirs</li> <li>(b) major watercourses (third order or higher stream)</li> <li>(c) any other watercourse, bore or well used for domestic or stock water supplies.</li> </ul>	<p>DTS/DPF 3.1</p> <p>Intensive animal husbandry operations are set back:</p> <ul style="list-style-type: none"> <li>(a) 800m or more from a public water supply reservoir</li> <li>(b) 200m or more from a major watercourse (third order or higher stream)</li> <li>(c) 100m or more from any other watercourse, bore or well used for domestic or stock water supplies.</li> </ul>
<p>PO 3.2</p> <p>Intensive animal husbandry operations and dairies incorporate appropriately designed effluent and run-off facilities that:</p> <ul style="list-style-type: none"> <li>(a) have sufficient capacity to hold effluent and runoff from the operations on site</li> <li>(b) ensure effluent does not infiltrate and pollute groundwater, soil or other water resources.</li> </ul>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>

## Interface between Land Uses

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
General Land Use Compatibility	
<p>PO 1.1</p> <p>Sensitive receivers are designed and sited to protect residents and occupants from adverse impacts generated by lawfully existing land uses (or lawfully approved land uses) and land uses desired in the zone.</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Development adjacent to a site containing a sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>

to accommodate sensitive receivers is designed to minimise adverse impacts.									
Hours of Operation									
<p>PO 2.1</p> <p>Non-residential development does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having regard to:</p> <ul style="list-style-type: none"> <li>(a) the nature of the development</li> <li>(b) measures to mitigate off-site impacts</li> <li>(c) the extent to which the development is desired in the zone</li> <li>(d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land.</li> </ul>	<p>DTS/DPF 2.1</p> <p>Development operating within the following hours:</p> <table border="1" data-bbox="834 371 1485 1238"> <thead> <tr> <th>Class of Development</th><th>Hours of operation</th></tr> </thead> <tbody> <tr> <td>Consulting room</td><td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td></tr> <tr> <td>Office</td><td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td></tr> <tr> <td>Shop, other than any one or combination of the following:   <ul style="list-style-type: none"> <li>(a) restaurant</li> <li>(b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone</li> </ul> </td><td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday</td></tr> </tbody> </table>	Class of Development	Hours of operation	Consulting room	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Office	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Shop, other than any one or combination of the following:  <ul style="list-style-type: none"> <li>(a) restaurant</li> <li>(b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone</li> </ul>	7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday
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Overshadowing									
<p>PO 3.1</p> <p>Overshadowing of habitable room windows of adjacent residential land uses in:</p> <ul style="list-style-type: none"> <li>a. a neighbourhood-type zone is minimised to maintain access to direct winter sunlight</li> <li>b. other zones is managed to enable access to direct winter sunlight.</li> </ul>	<p>DTS/DPF 3.1</p> <p>North-facing windows of habitable rooms of adjacent residential land uses in a neighbourhood-type zone receive at least 3 hours of direct sunlight between 9.00am and 3.00pm on 21 June.</p>								
<p>PO 3.2</p> <p>Overshadowing of the primary area of private open space or communal open space of adjacent residential land uses in:</p> <ul style="list-style-type: none"> <li>a. a neighbourhood type zone is minimised to maintain access to direct winter sunlight</li> <li>b. other zones is managed to enable access to direct winter sunlight.</li> </ul>	<p>DTS/DPF 3.2</p> <p>Development maintains 2 hours of direct sunlight between 9.00 am and 3.00 pm on 21 June to adjacent residential land uses in a neighbourhood-type zone in accordance with the following:</p> <ul style="list-style-type: none"> <li>a. for ground level private open space, the smaller of the following: <ul style="list-style-type: none"> <li>i. half the existing ground level open space</li> <li>or</li> <li>ii. 35m<sup>2</sup> of the existing ground level open space (with at least one of the area's dimensions measuring 2.5m)</li> </ul> </li> <li>b. for ground level communal open space, at least half of the existing ground level open space.</li> </ul>								



<p>PO 3.3</p> <p>Development does not unduly reduce the generating capacity of adjacent rooftop solar energy facilities taking into account:</p> <ul style="list-style-type: none"> <li>(a) the form of development contemplated in the zone</li> <li>(b) the orientation of the solar energy facilities</li> <li>(c) the extent to which the solar energy facilities are already overshadowed.</li> </ul>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>
<p>PO 3.4</p> <p>Development that incorporates moving parts, including windmills and wind farms, are located and operated to not cause unreasonable nuisance to nearby dwellings and tourist accommodation caused by shadow flicker.</p>	<p>DTS/DPF 3.4</p> <p>None are applicable.</p>
Activities Generating Noise or Vibration	
<p>PO 4.1</p> <p>Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 4.1</p> <p>Noise that affects sensitive receivers achieves the relevant Environment Protection (Noise) Policy criteria.</p>
<p>PO 4.2</p> <p>Areas for the on-site manoeuvring of service and delivery vehicles, plant and equipment, outdoor work spaces (and the like) are designed and sited to not unreasonably impact the amenity of adjacent sensitive receivers (or lawfully approved sensitive receivers) and zones primarily intended to accommodate sensitive receivers due to noise and vibration by adopting techniques including:</p> <ul style="list-style-type: none"> <li>(a) locating openings of buildings and associated services away from the interface with the adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</li> <li>(b) when sited outdoors, locating such areas as far as practicable from adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</li> <li>(c) housing plant and equipment within an enclosed structure or acoustic enclosure</li> <li>(d) providing a suitable acoustic barrier between the plant and / or equipment and the adjacent sensitive receiver boundary or zone.</li> </ul>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>
<p>PO 4.3</p> <p>Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa are positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 4.3</p> <p>The pump and/or filtration system ancillary to a dwelling erected on the same site is:</p> <ul style="list-style-type: none"> <li>(a) enclosed in a solid acoustic structure located at least 5m from the nearest habitable room located on an adjoining allotment</li> <li>or</li> <li>(b) located at least 12m from the nearest habitable room located on an adjoining allotment.</li> </ul>
PO 4.4	DTS/DPF 4.4

External noise into bedrooms is minimised by separating or shielding these rooms from service equipment areas and fixed noise sources located on the same or an adjoining allotment.	Adjacent land is used for residential purposes.				
PO 4.5  Outdoor areas associated with licensed premises (such as beer gardens or dining areas) are designed and/or sited to not cause unreasonable noise impact on existing adjacent sensitive receivers (or lawfully approved sensitive receivers).	DTS/DPF 4.5  None are applicable.				
PO 4.6  Development incorporating music achieves suitable acoustic amenity when measured at the boundary of an adjacent sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers.	DTS/DPF 4.6  Development incorporating music includes noise attenuation measures that will achieve the following noise levels: <table border="1"> <thead> <tr> <th>Assessment location</th><th>Music noise level</th></tr> </thead> <tbody> <tr> <td>Externally at the nearest existing or envisaged noise sensitive location</td><td>Less than 8dB above the level of background noise (<math>L_{90,15min}</math>) in any octave band of the sound spectrum (<math>LOCT_{10,15} &lt; LOCT_{90,15} + 8dB</math>)</td></tr> </tbody> </table>	Assessment location	Music noise level	Externally at the nearest existing or envisaged noise sensitive location	Less than 8dB above the level of background noise ( $L_{90,15min}$ ) in any octave band of the sound spectrum ( $LOCT_{10,15} < LOCT_{90,15} + 8dB$ )
Assessment location	Music noise level				
Externally at the nearest existing or envisaged noise sensitive location	Less than 8dB above the level of background noise ( $L_{90,15min}$ ) in any octave band of the sound spectrum ( $LOCT_{10,15} < LOCT_{90,15} + 8dB$ )				
Air Quality					
PO 5.1  Development with the potential to emit harmful or nuisance-generating air pollution incorporates air pollution control measures to prevent harm to human health or unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) within the locality and zones primarily intended to accommodate sensitive receivers.	DTS/DPF 5.1  None are applicable.				
PO 5.2  Development that includes chimneys or exhaust flues (including cafes, restaurants and fast food outlets) is designed to minimise nuisance or adverse health impacts to sensitive receivers (or lawfully approved sensitive receivers) by: <ul style="list-style-type: none"> <li>(a) incorporating appropriate treatment technology before exhaust emissions are released</li> <li>(b) locating and designing chimneys or exhaust flues to maximise the dispersion of exhaust emissions, taking into account the location of sensitive receivers.</li> </ul>	DTS/DPF 5.2  None are applicable.				
Light Spill					
PO 6.1  External lighting is positioned and designed to not cause unreasonable light spill impact on adjacent sensitive receivers (or lawfully approved sensitive receivers).	DTS/DPF 6.1  None are applicable.				
PO 6.2  External lighting is not hazardous to motorists and cyclists.	DTS/DPF 6.2  None are applicable.				
Solar Reflectivity / Glare					

PO 7.1  Development is designed and comprised of materials and finishes that do not unreasonably cause a distraction to adjacent road users and pedestrian areas or unreasonably cause heat loading and micro-climatic impacts on adjacent buildings and land uses as a result of reflective solar glare.	DTS/DPF 7.1  None are applicable.
Electrical Interference	
PO 8.1  Development in rural and remote areas does not unreasonably diminish or result in the loss of existing communication services due to electrical interference.	DTS/DPF 8.1  The building or structure:  (a) is no greater than 10m in height, measured from existing ground level or (b) is not within a line of sight between a fixed transmitter and fixed receiver (antenna) other than where an alternative service is available via a different fixed transmitter or cable.
Interface with Rural Activities	
PO 9.1  Sensitive receivers are located and designed to mitigate impacts from lawfully existing horticultural and farming activities (or lawfully approved horticultural and farming activities), including spray drift and noise and do not prejudice the continued operation of these activities.	DTS/DPF 9.1  None are applicable.
PO 9.2  Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing intensive animal husbandry activities and do not prejudice the continued operation of these activities.	DTS/DPF 9.2  None are applicable.
PO 9.3  Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing land-based aquaculture activities and do not prejudice the continued operation of these activities.	DTS/DPF 9.3  Sensitive receivers are located at least 200m from the boundary of a site used for land-based aquaculture and associated components in other ownership.
PO 9.4  Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing dairies including associated wastewater lagoons and liquid/solid waste storage and disposal facilities and do not prejudice the continued operation of these activities.	DTS/DPF 9.4  Sensitive receivers are sited at least 500m from the boundary of a site used for a dairy and associated wastewater lagoon(s) and liquid/solid waste storage and disposal facilities in other ownership.
PO 9.5  Sensitive receivers are located and designed to mitigate the potential impacts from lawfully existing facilities used for the handling, transportation and storage of bulk commodities (recognising the potential for extended hours of operation) and do not prejudice the continued operation of these activities.	DTS/DPF 9.5  Sensitive receivers are located away from the boundary of a site used for the handling, transportation and/or storage of bulk commodities in other ownership in accordance with the following:  (a) 300m or more, where it involves the handling of agricultural crop products, rock, ores, minerals,

	<p>petroleum products or chemicals to or from any commercial storage facility</p> <p>(b) 300m or more, where it involves the handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals at a wharf or wharf side facility (including sea-port grain terminals) where the handling of these materials into or from vessels does not exceed 100 tonnes per day</p> <p>(c) 500m or more, where it involves the storage of bulk petroleum in individual containers with a capacity up to 200 litres and a total on-site storage capacity not exceeding 1000 cubic metres</p> <p>(d) 500m or more, where it involves the handling of coal with a capacity up to 1 tonne per day or a storage capacity up to 50 tonnes</p> <p>(e) 1000m or more, where it involves the handling of coal with a capacity exceeding 1 tonne per day but not exceeding 100 tonnes per day or a storage capacity exceeding 50 tonnes but not exceeding 5000 tonnes.</p>
<p>PO 9.6</p> <p>Setbacks and vegetation plantings along allotment boundaries should be incorporated to mitigate the potential impacts of spray drift and other impacts associated with agricultural and horticultural activities.</p>	<p>DTS/DPF 9.6</p> <p>None are applicable.</p>
<p>PO 9.7</p> <p>Urban development does not prejudice existing agricultural and horticultural activities through appropriate separation and design techniques.</p>	<p>DTS/DPF 9.7</p> <p>None are applicable.</p>
Interface with Mines and Quarries (Rural and Remote Areas)	
<p>PO 10.1</p> <p>Sensitive receivers are separated from existing mines to minimise the adverse impacts from noise, dust and vibration.</p>	<p>DTS/DPF 10.1</p> <p>Sensitive receivers are located no closer than 500m from the boundary of a Mining Production Tenement under the <i>Mining Act 1971</i>.</p>

## Land Division

### Assessment Provisions (AP)

Desired Outcome	
DO 1	<p>Land division:</p> <ul style="list-style-type: none"> <li>(a) creates allotments with the appropriate dimensions and shape for their intended use</li> <li>(b) allows efficient provision of new infrastructure and the optimum use of underutilised infrastructure</li> <li>(c) integrates and allocates adequate and suitable land for the preservation of site features of value, including significant vegetation, watercourses, water bodies and other environmental features</li> <li>(d) facilitates solar access through allotment orientation</li> <li>(e) creates a compact urban form that supports active travel, walkability and the use of public transport</li> <li>(f) avoids areas of high natural hazard risk.</li> </ul>



Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All land division	
Allotment configuration	
PO 1.1 Land division creates allotments suitable for their intended use.	DTS/DPF 1.1 Division of land satisfies (a) or (b):  (a) reflects the site boundaries illustrated and approved in an operative or existing development authorisation for residential development under the <i>Development Act 1993</i> or <i>Planning, Development and Infrastructure Act 2016</i> where the allotments are used or are proposed to be used solely for residential purposes  (b) is proposed as part of a combined land division application with deemed-to-satisfy dwellings on the proposed allotments.
PO 1.2 Land division considers the physical characteristics of the land, preservation of environmental and cultural features of value and the prevailing context of the locality.	DTS/DPF 1.2 None are applicable.
Design and Layout	
PO 2.1 Land division results in a pattern of development that minimises the likelihood of future earthworks and retaining walls.	DTS/DPF 2.1 None are applicable.
PO 2.2 Land division enables the appropriate management of interface impacts between potentially conflicting land uses and/or zones.	DTS/DPF 2.2 None are applicable.
PO 2.3 Land division maximises the number of allotments that face public open space and public streets.	DTS/DPF 2.3 None are applicable.
PO 2.4 Land division is integrated with site features, adjacent land uses, the existing transport network and available infrastructure.	DTS/DPF 2.4 None are applicable.
PO 2.5 Development and infrastructure is provided and staged in a manner that supports an orderly and economic provision of land, infrastructure and services.	DTS/DPF 2.5 None are applicable.
PO 2.6 Land division results in watercourses being retained within open space and development taking place on land not subject to flooding.	DTS/DPF 2.6 None are applicable.

PO 2.7 Land division results in legible street patterns connected to the surrounding street network.	DTS/DPF 2.7 None are applicable.
PO 2.8 Land division is designed to preserve existing vegetation of value including native vegetation and regulated and significant trees.	DTS/DPF 2.8 None are applicable.
Roads and Access	
PO 3.1 Land division provides allotments with access to an all-weather public road.	DTS/DPF 3.1 None are applicable.
PO 3.2 Street patterns and intersections are designed to enable the safe and efficient movement of pedestrian, cycle and vehicular traffic.	DTS/DPF 3.2 None are applicable.
PO 3.3 Land division does not impede access to publicly owned open space and/or recreation facilities.	DTS/DPF 3.3 None are applicable.
PO 3.4 Road reserves provide for safe and convenient movement and parking of projected volumes of vehicles and allow for the efficient movement of service and emergency vehicles.	DTS/DPF 3.4 None are applicable.
PO 3.5 Road reserves are designed to accommodate pedestrian and cycling infrastructure, street tree planting, landscaping and street furniture.	DTS/DPF 3.5 None are applicable.
PO 3.6 Road reserves accommodate stormwater drainage and public utilities.	DTS/DPF 3.6 None are applicable.
PO 3.7 Road reserves provide unobstructed vehicular access and egress to and from individual allotments and sites.	DTS/DPF 3.7 None are applicable.
PO 3.8 Street patterns and intersections are designed to enable the safe and efficient movement of pedestrian, cycle and vehicular traffic.	DTS/DPF 3.8 None are applicable.
PO 3.9 Roads, open space and thoroughfares provide safe and convenient linkages to the surrounding open space and transport network.	DTS/DPF 3.9 None are applicable.
PO 3.10 Public streets are designed to enable tree planting to provide shade and enhance the amenity of streetscapes.	DTS/DPF 3.10 None are applicable.

PO 3.11	DTS/DPF 3.11
Local streets are designed to create low-speed environments that are safe for cyclists and pedestrians.	None are applicable.
Infrastructure	
PO 4.1	DTS/DPF 4.1
Land division incorporates public utility services within road reserves or dedicated easements.	None are applicable.
PO 4.2	DTS/DPF 4.2
Waste water, sewage and other effluent is capable of being disposed of from each allotment without risk to public health or the environment.	Each allotment can be connected to: <ul style="list-style-type: none"> <li>(a) a waste water treatment plant that has the hydraulic volume and pollutant load treatment and disposal capacity for the maximum predicted wastewater volume generated by subsequent development of the proposed allotment</li> <li>or</li> <li>(b) a form of on-site waste water treatment and disposal that meets relevant public health and environmental standards.</li> </ul>
PO 4.3	DTS/DPF 4.3
Septic tank effluent drainage fields and other waste water disposal areas are maintained to ensure the effective operation of waste systems and minimise risks to human health and the environment.	Development is not built on, or encroaches within, an area that is or will be, required for a sewerage system or waste control system.
PO 4.4	DTS/DPF 4.4
Constructed wetland systems, including associated detention and retention basins, are sited and designed to ensure public health and safety is protected, including by minimising potential public health risks arising from the breeding of mosquitoes.	None are applicable.
PO 4.5	DTS/DPF 4.5
Constructed wetland systems, including associated detention and retention basins, are sited and designed to allow sediments to settle prior to discharge into watercourses or the marine environment.	None are applicable.
PO 4.6	DTS/DPF 4.6
Constructed wetland systems, including associated detention and retention basins, are sited and designed to function as a landscape feature.	None are applicable.
Minor Land Division (Under 20 Allotments)	
Open Space	
PO 5.1	DTS/DPF 5.1
Land division proposing an additional allotment under 1 hectare provides or supports the provision of open space.	None are applicable.
Solar Orientation	

PO 6.1 Land division for residential purposes facilitates solar access through allotment orientation.	DTS/DPF 6.1 None are applicable.
Water Sensitive Design	
PO 7.1 Land division creating a new road or common driveway includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system, watercourses or other water bodies.	DTS/DPF 7.1 None are applicable.
PO 7.2 Land division designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.	DTS/DPF 7.2 None are applicable.
Battle-Axe Development	
PO 8.1 Battle-axe development appropriately responds to the existing neighbourhood context.	DTS/DPF 8.1 Allotments are not in the form of a battle-axe arrangement.
PO 8.2 Battle-axe development designed to allow safe and convenient movement.	DTS/DPF 8.2 The handle of a battle-axe development:  (a) has a minimum width of 4m or (b) where more than 3 allotments are proposed, a minimum width of 5.5m.
PO 8.3 Battle-axe allotments and/or common land are of a suitable size and dimension to allow passenger vehicles to enter and exit and manoeuvre within the site in a safe and convenient manner.	DTS/DPF 8.3 Battle-axe development allows a B85 passenger vehicle to enter and exit parking spaces in no more than a three-point turn manoeuvre.
PO 8.4 Battle-axe or common driveways incorporate landscaping and permeability to improve appearance and assist in stormwater management.	DTS/DPF 8.4 Battle-axe or common driveways satisfy (a) and (b):  (a) are constructed of a minimum of 50% permeable or porous material (b) where the driveway is located directly adjacent the side or rear boundary of the site, soft landscaping with a minimum dimension of 1m is provided between the driveway and site boundary (excluding along the perimeter of a passing point).
Major Land Division (20+ Allotments)	
Open Space	
PO 9.1 Land division allocates or retains evenly distributed, high quality areas of open space to improve residential amenity and provide urban heat amelioration.	DTS/DPF 9.1 None are applicable.
PO 9.2	DTS/DPF 9.2



Land allocated for open space is suitable for its intended active and passive recreational use considering gradient and potential for inundation.	None are applicable.
PO 9.3 Land allocated for active recreation has dimensions capable of accommodating a range of active recreational activities.	DTS/DPF 9.3 None are applicable.
Water Sensitive Design	
PO 10.1 Land division creating 20 or more residential allotments includes a stormwater management system designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.	DTS/DPF 10.1 None are applicable.
PO 10.2 Land division creating 20 or more non-residential allotments includes a stormwater management system designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.	DTS/DPF 10.2 None are applicable.
PO 10.3 Land division creating 20 or more allotments includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system, watercourses or other water bodies.	DTS/DPF 10.3 None are applicable.
Solar Orientation	
PO 11.1 Land division creating 20 or more allotments for residential purposes facilitates solar access through allotment orientation and allotment dimensions.	DTS/DPF 11.1 None are applicable.

## Marinas and On-Water Structures

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Marinas and on-water structures are located and designed to minimise the impairment of commercial, recreational and navigational activities and adverse impacts on the environment.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome		Deemed-to-Satisfy Criteria / Designated Performance Feature	
Navigation and Safety			
PO 1.1	Safe public access is provided or maintained to the waterfront, public infrastructure and recreation areas.	DTS/DPF 1.1	None are applicable.
PO 1.2	The operation of wharves is not impaired by marinas and on-water structures.	DTS/DPF 1.2	None are applicable.
PO 1.3	Navigation and access channels are not impaired by marinas and on-water structures.	DTS/DPF 1.3	None are applicable.
PO 1.4	Commercial shipping lanes are not impaired by marinas and on-water structures.	DTS/DPF 1.4	Marinas and on-water structures are set back 250m or more from commercial shipping lanes.
PO 1.5	Marinas and on-water structures are located to avoid interfering with the operation or function of a water supply pumping station.	DTS/DPF 1.5	On-water structures are set back:  (a) 3km or more from upstream water supply pumping station take-off points (b) 500m or more from downstream water supply pumping station take-off points.
PO 1.6	Maintenance of on-water infrastructure, including revetment walls, is not impaired by marinas and on-water structures.	DTS/DPF 1.6	None are applicable.
Environmental Protection			
PO 2.1	Development is sited and designed to facilitate water circulation and exchange.	DTS/DPF 2.1	None are applicable.

## Open Space and Recreation

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Pleasant, functional and accessible open space and recreation facilities are provided at State, regional, district, neighbourhood and local levels for active and passive recreation, biodiversity, community health, urban cooling, tree

	canopy cover, visual amenity, gathering spaces, wildlife and waterway corridors, and a range of other functions and at a range of sizes that reflect the purpose of that open space.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
PO 1.1 Recreation facilities are compatible with surrounding land uses and activities.	DTS/DPF 1.1 None are applicable.
PO 1.2 Open space areas include natural or landscaped areas using locally indigenous plant species and large trees.	DTS/DPF 1.2 None are applicable.
Design and Siting	
PO 2.1 Open space and recreation facilities address adjacent public roads to optimise pedestrian access and visibility.	DTS/DPF 2.1 None are applicable.
PO 2.2 Open space and recreation facilities incorporate park furniture, shaded areas and resting places.	DTS/DPF 2.2 None are applicable.
PO 2.3 Open space and recreation facilities link habitats, wildlife corridors and existing open spaces and recreation facilities.	DTS/DPF 2.3 None are applicable.
Pedestrians and Cyclists	
PO 3.1 Open space incorporates:  (a) pedestrian and cycle linkages to other open spaces, centres, schools and public transport nodes; (b) safe crossing points where pedestrian routes intersect the road network; (c) easily identified access points.	DTS/DPF 3.1 None are applicable.
Usability	
PO 4.1 Land allocated for open space is suitable for its intended active and passive recreational use taking into consideration its gradient and potential for inundation.	DTS/DPF 4.1 None are applicable.
Safety and Security	
PO 5.1 Open space is overlooked by housing, commercial or other	DTS/DPF 5.1 None are applicable.

development to provide casual surveillance where possible.	
PO 5.2 Play equipment is located to maximise opportunities for passive surveillance.	DTS/DPF 5.2 None are applicable.
PO 5.3 Landscaping provided in open space and recreation facilities maximises opportunities for casual surveillance throughout the park.	DTS/DPF 5.3 None are applicable.
PO 5.4 Fenced parks and playgrounds have more than one entrance or exit to minimise potential entrapment.	DTS/DPF 5.4 None are applicable.
PO 5.5 Adequate lighting is provided around toilets, telephones, seating, litter bins, bicycle storage, car parks and other such facilities.	DTS/DPF 5.5 None are applicable.
PO 5.6 Pedestrian and bicycle movement after dark is focused along clearly defined, adequately lit routes with observable entries and exits.	DTS/DPF 5.6 None are applicable.
Signage	
PO 6.1 Signage is provided at entrances to and within the open space and recreation facilities to provide clear orientation to major points of interest such as the location of public toilets, telephones, safe routes, park activities and the like.	DTS/DPF 6.1 None are applicable.
Buildings and Structures	
PO 7.1 Buildings and car parking areas in open space areas are designed, located and of a scale to be unobtrusive.	DTS/DPF 7.1 None are applicable.
PO 7.2 Buildings and structures in open space areas are clustered where practical to ensure that the majority of the site remains open.	DTS/DPF 7.2 None are applicable.
PO 7.3 Development in open space is constructed to minimise the extent of impervious surfaces.	DTS/DPF 7.3 None are applicable.
PO 7.4 Development that abuts or includes a coastal reserve or Crown land used for scenic, conservation or recreational purposes is located and designed to have regard to the purpose, management and amenity of the reserve.	DTS/DPF 7.4 None are applicable.
Landscaping	



PO 8.1 Open space and recreation facilities provide for the planting and retention of large trees and vegetation.	DTS/DPF 8.1 None are applicable.
PO 8.2 Landscaping in open space and recreation facilities provides shade and windbreaks:  (a) along cyclist and pedestrian routes; (b) around picnic and barbecue areas; (c) in car parking areas.	DTS/DPF 8.2 None are applicable.
PO 8.3 Landscaping in open space facilitates habitat for local fauna and facilitates biodiversity.	DTS/DPF 8.3 None are applicable.
PO 8.4 Landscaping including trees and other vegetation passively watered with local rainfall run-off, where practicable.	DTS/DPF 8.4 None are applicable.

## Out of Activity Centre Development

### Assessment Provisions (AP)

Desired Outcome	
DO1	The role of Activity Centres in contributing to the form and pattern of development and enabling equitable and convenient access to a range of shopping, administrative, cultural, entertainment and other facilities in a single trip is maintained and reinforced.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1 Non-residential development outside Activity Centres of a scale and type that does not diminish the role of Activity Centres:  (a) as primary locations for shopping, administrative, cultural, entertainment and community services (b) as a focus for regular social and business gatherings (c) in contributing to or maintaining a pattern of development that supports equitable community access to services and facilities.	DTS/DPF 1.1 None are applicable.
PO 1.2 Out-of-activity centre non-residential development complements Activity Centres through the provision of services and facilities:  (a) that support the needs of local residents and workers, particularly in underserved locations (b) at the edge of Activities Centres where they cannot readily be accommodated within an existing Activity	DTS/DPF 1.2 None are applicable.

Centre to expand the range of services on offer and support the role of the Activity Centre.	
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## Resource Extraction

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Resource extraction activities are developed in a manner that minimises human and environmental impacts.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
PO 1.1 Resource extraction activities minimise landscape damage outside of those areas unavoidably disturbed to access and exploit a resource and provide for the progressive reclamation and betterment of disturbed areas.	DTS/DPF 1.1 None are applicable.
PO 1.2 Resource extraction activities avoid damage to cultural sites or artefacts.	DTS/DPF 1.2 None are applicable.
Water Quality	
PO 2.1 Stormwater and/or wastewater from resource extraction activities is diverted into appropriately sized treatment and retention systems to enable reuse on site.	DTS/DPF 2.1 None are applicable.
Separation Treatments, Buffers and Landscaping	
PO 3.1 Resource extraction activities minimise adverse impacts upon sensitive receivers through incorporation of separation distances and/or mounding/vegetation.	DTS/DPF 3.1 None are applicable.
PO 3.2 Resource extraction activities are screened from view from adjacent land by perimeter landscaping and/or mounding.	DTS/DPF 3.2 None are applicable.

## Site Contamination

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Ensure land is suitable for the proposed use in circumstances where it is, or may have been, subject to site contamination.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>Ensure land is suitable for use when land use changes to a more sensitive use.</p>	<p>DTS/DPF 1.1</p> <p>Development satisfies (a), (b), (c) or (d):</p> <ul style="list-style-type: none"> <li>(a) does not involve a change in the use of land</li> <li>(b) involves a change in the use of land that does not constitute a change to a more sensitive use</li> <li>(c) involves a change in the use of land to a more sensitive use on land at which site contamination is unlikely to exist (as demonstrated in a site contamination declaration form)</li> <li>(d) involves a change in the use of land to a more sensitive use on land at which site contamination exists, or may exist (as demonstrated in a site contamination declaration form), and satisfies both of the following: <ul style="list-style-type: none"> <li>(i) a site contamination audit report has been prepared under Part 10A of the <i>Environment Protection Act 1993</i> in relation to the land within the previous 5 years which states that- <ul style="list-style-type: none"> <li>A. site contamination does not exist (or no longer exists) at the land</li> <li>or</li> <li>B. the land is suitable for the proposed use or range of uses (without the need for any further remediation)</li> <li>or</li> <li>C. where remediation is, or remains, necessary for the proposed use (or range of uses), remediation work has been carried out or will be carried out (and the applicant has provided a written undertaking that the remediation works will be implemented in association with the development)</li> </ul> </li> <li>and</li> <li>(ii) no other class 1 activity or class 2 activity has taken place at the land since the preparation of the site contamination audit report (as demonstrated in a site contamination declaration form).</li> </ul> </li> </ul>

## Tourism Development

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Tourism development is built in locations that cater to the needs of visitors and positively contributes to South Australia's visitor economy.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
General	
PO 1.1 Tourism development complements and contributes to local, natural, cultural or historical context where: <ul style="list-style-type: none"> <li>(a) it supports immersive natural experiences</li> <li>(b) it showcases South Australia's landscapes and produce</li> <li>(c) its events and functions are connected to local food, wine and nature.</li> </ul>	DTS/DPF 1.1 None are applicable.
PO 1.2 Tourism development comprising multiple accommodation units (including any facilities and activities for use by guests and visitors) is clustered to minimise environmental and contextual impact.	DTS/DPF 1.2 None are applicable.
Caravan and Tourist Parks	
PO 2.1 Potential conflicts between long-term residents and short-term tourists are minimised through suitable siting and design measures.	DTS/DPF 2.1 None are applicable.
PO 2.2 Occupants are provided privacy and amenity through landscaping and fencing.	DTS/DPF 2.2 None are applicable.
PO 2.3 Communal open space and centrally located recreation facilities are provided for guests and visitors.	DTS/DPF 2.3 12.5% or more of a caravan park comprises clearly defined communal open space, landscaped areas and areas for recreation.



PO 2.4 Perimeter landscaping is used to enhance the amenity of the locality.	DTS/DPF 2.4 None are applicable.
PO 2.5 Amenity blocks (showers, toilets, laundry and kitchen facilities) are sufficient to serve the full occupancy of the development.	DTS/DPF 2.5 None are applicable.
PO 2.6 Long-term occupation does not displace tourist accommodation, particularly in important tourist destinations such as coastal and riverine locations.	DTS/DPF 2.6 None are applicable.
Tourist accommodation in areas constituted under the National Parks and Wild ife Act 1972	
PO 3.1 Tourist accommodation avoids delicate or environmentally sensitive areas such as sand dunes, cliff tops, estuaries, wetlands or substantially intact strata of native vegetation (including regenerated areas of native vegetation lost through bushfire).	DTS/DPF 3.1 None are applicable.
PO 3.2 Tourist accommodation is sited and designed in a manner that is subservient to the natural environment and where adverse impacts on natural features, landscapes, habitats and cultural assets are avoided.	DTS/DPF 3.2 None are applicable.
PO 3.3 Tourist accommodation and recreational facilities, including associated access ways and ancillary structures, are located on cleared (other than where cleared as a result of bushfire) or degraded areas or where environmental improvements can be achieved.	DTS/DPF 3.3 None are applicable.
PO 3.4 Tourist accommodation is designed to prevent conversion to private dwellings through:  (a) comprising a minimum of 10 accommodation units (b) clustering separated individual accommodation units (c) being of a size unsuitable for a private dwelling (d) ensuring functional areas that are generally associated with a private dwelling such as kitchens and laundries are excluded from, or physically separated from individual accommodation units, or are of a size unsuitable for a private dwelling.	DTS/DPF 3.4 None are applicable.

## Transport, Access and Parking

### Assessment Provisions (AP)

## Desired Outcome

DO 1	A comprehensive, integrated and connected transport system that is safe, sustainable, efficient, convenient and accessible to all users.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

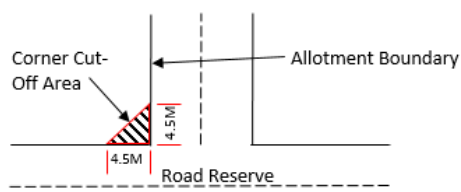
Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Movement Systems	
PO 1.1 Development is integrated with the existing transport system and designed to minimise its potential impact on the functional performance of the transport system.	DTS/DPF 1.1 None are applicable.
PO 1.2 Development is designed to discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive receivers.	DTS/DPF 1.2 None are applicable.
PO 1.3 Industrial, commercial and service vehicle movements, loading areas and designated parking spaces are separated from passenger vehicle car parking areas to ensure efficient and safe movement and minimise potential conflict.	DTS/DPF 1.3 None are applicable.
PO 1.4 Development is sited and designed so that loading, unloading and turning of all traffic avoids interrupting the operation of and queuing on public roads and pedestrian paths.	DTS/DPF 1.4 All vehicle manoeuvring occurs onsite.
Sightlines	
PO 2.1 Sightlines at intersections, pedestrian and cycle crossings, and crossovers to allotments for motorists, cyclists and pedestrians are maintained or enhanced to ensure safety for all road users and pedestrians.	DTS/DPF 2.1 None are applicable.
PO 2.2 Walls, fencing and landscaping adjacent to driveways and corner sites are designed to provide adequate sightlines between vehicles and pedestrians.	DTS/DPF 2.2 None are applicable.
Vehicle Access	
PO 3.1 Safe and convenient access minimises impact or interruption on	DTS/DPF 3.1 The access is:

the operation of public roads.	<ul style="list-style-type: none"> <li>(a) provided via a lawfully existing or authorised driveway or access point or an access point for which consent has been granted as part of an application for the division of land</li> <li>or</li> <li>(b) not located within 6m of an intersection of 2 or more roads or a pedestrian activated crossing.</li> </ul>
<p>PO 3.2</p> <p>Development incorporating vehicular access ramps ensures vehicles can enter and exit a site safely and without creating a hazard to pedestrians and other vehicular traffic.</p>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>
<p>PO 3.3</p> <p>Access points are sited and designed to accommodate the type and volume of traffic likely to be generated by the development or land use.</p>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>
<p>PO 3.4</p> <p>Access points are sited and designed to minimise any adverse impacts on neighbouring properties.</p>	<p>DTS/DPF 3.4</p> <p>None are applicable.</p>
<p>PO 3.5</p> <p>Access points are located so as not to interfere with street trees, existing street furniture (including directional signs, lighting, seating and weather shelters) or infrastructure services to maintain the appearance of the streetscape, preserve local amenity and minimise disruption to utility infrastructure assets.</p>	<p>DTS/DPF 3.5</p> <p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <ul style="list-style-type: none"> <li>(a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land</li> <li>(b) where newly proposed, is set back: <ul style="list-style-type: none"> <li>(i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner</li> <li>(ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance</li> <li>(iii) 6m or more from the tangent point of an intersection of 2 or more roads</li> <li>(iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.</li> </ul> </li> </ul>
<p>PO 3.6</p> <p>Driveways and access points are separated and minimised in number to optimise the provision of on-street visitor parking (where on-street parking is appropriate).</p>	<p>DTS/DPF 3.6</p> <p>Driveways and access points:</p> <ul style="list-style-type: none"> <li>(a) for sites with a frontage to a public road of 20m or less, one access point no greater than 3.5m in width is provided</li> <li>(b) for sites with a frontage to a public road greater than 20m: <ul style="list-style-type: none"> <li>(i) a single access point no greater than 6m in width is provided</li> <li>or</li> <li>(ii) not more than two access points with a width of 3.5m each are provided.</li> </ul> </li> </ul>

PO 3.7	DTS/DPF 3.7
Access points are appropriately separated from level crossings to avoid interference and ensure their safe ongoing operation.	Development does not involve a new or modified access or cause an increase in traffic through an existing access that is located within the following distance from a railway crossing: <ul style="list-style-type: none"> <li>(a) 80 km/h road - 110m</li> <li>(b) 70 km/h road - 90m</li> <li>(c) 60 km/h road - 70m</li> <li>(d) 50km/h or less road - 50m.</li> </ul>
PO 3.8	DTS/DPF 3.8
Driveways, access points, access tracks and parking areas are designed and constructed to allow adequate movement and manoeuvrability having regard to the types of vehicles that are reasonably anticipated.	None are applicable.
PO 3.9	DTS/DPF 3.9
Development is designed to ensure vehicle circulation between activity areas occurs within the site without the need to use public roads.	None are applicable.
Access for People with Disabilities	
PO 4.1	DTS/DPF 4.1
Development is sited and designed to provide safe, dignified and convenient access for people with a disability.	None are applicable.
Vehicle Parking Rates	
PO 5.1	DTS/DPF 5.1
Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as: <ul style="list-style-type: none"> <li>(a) availability of on-street car parking</li> <li>(b) shared use of other parking areas</li> <li>(c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared</li> <li>(d) the adaptive reuse of a State or Local Heritage Place.</li> </ul>	Development provides a number of car parking spaces on-site at a rate no less than the amount calculated using one of the following, whichever is relevant: <ul style="list-style-type: none"> <li>(a) Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements</li> <li>(b) Transport, Access and Parking Table 2 - Off-Street Vehicle Parking Requirements in Designated Areas</li> <li>(c) if located in an area where a lawfully established carparking fund operates, the number of spaces calculated under (a) or (b) less the number of spaces offset by contribution to the fund.</li> </ul>
Vehicle Parking Areas	
PO 6.1	DTS/DPF 6.1
Vehicle parking areas are sited and designed to minimise impact on the operation of public roads by avoiding the use of public roads when moving from one part of a parking area to another.	Movement between vehicle parking areas within the site can occur without the need to use a public road.
PO 6.2	DTS/DPF 6.2
Vehicle parking areas are appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced, and the like.	None are applicable.



PO 6.3 Vehicle parking areas are designed to provide opportunity for integration and shared-use of adjacent car parking areas to reduce the total extent of vehicle parking areas and access points.	DTS/DPF 6.3 None are applicable.
PO 6.4 Pedestrian linkages between parking areas and the development are provided and are safe and convenient.	DTS/DPF 6.4 None are applicable.
PO 6.5 Vehicle parking areas that are likely to be used during non-daylight hours are provided with sufficient lighting to entry and exit points to ensure clear visibility to users.	DTS/DPF 6.5 None are applicable.
PO 6.6 Loading areas and designated parking spaces for service vehicles are provided within the boundary of the site.	DTS/DPF 6.6 Loading areas and designated parking spaces are wholly located within the site.
PO 6.7 On-site visitor parking spaces are sited and designed to be accessible to all visitors at all times.	DTS/DPF 6.7 None are applicable.
Undercroft and Below Ground Garaging and Parking of Vehicles	
PO 7.1 Undercroft and below ground garaging of vehicles is designed to enable safe entry and exit from the site without compromising pedestrian or cyclist safety or causing conflict with other vehicles.	DTS/DPF 7.1 None are applicable.
Internal Roads and Parking Areas in Residential Parks and Caravan and Tourist Parks	
PO 8.1 Internal road and vehicle parking areas are surfaced to prevent dust becoming a nuisance to park residents and occupants.	DTS/DPF 8.1 None are applicable.
PO 8.2 Traffic circulation and movement within the park is pedestrian friendly and promotes low speed vehicle movement.	DTS/DPF 8.2 None are applicable.
Bicycle Parking in Designated Areas	
PO 9.1 The provision of adequately sized on-site bicycle parking facilities encourages cycling as an active transport mode.	DTS/DPF 9.1 Areas and / or fixtures are provided for the parking and storage of bicycles at a rate not less than the amount calculated using Transport, Access and Parking Table 3 - Off Street Bicycle Parking Requirements.
PO 9.2 Bicycle parking facilities provide for the secure storage and tethering of bicycles in a place where casual surveillance is possible, is well lit and signed for the safety and convenience of cyclists and deters property theft.	DTS/DPF 9.2 None are applicable.

PO 9.3 Non-residential development incorporates end-of-journey facilities for employees such as showers, changing facilities and secure lockers, and signage indicating the location of the facilities to encourage cycling as a mode of journey-to-work transport.	DTS/DPF 9.3 None are applicable.
Corner Cut-Offs	
PO 10.1 Development is located and designed to ensure drivers can safely turn into and out of public road junctions.	DTS/DPF 10.1 Development does not involve building work, or building work is located wholly outside the land shown as Corner Cut-Off Area in the following diagram:  

**Table 1 - General Off-Street Car Parking Requirements**

The following parking rates apply and if located in an area where a lawfully established carparking fund operates, the number of spaces is reduced by an amount equal to the number of spaces offset by contribution to the fund.

Class of Development	Car Parking Rate (unless varied by Table 2 onwards)  Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.
<b>Residential Development</b>	
<b>Detached Dwelling</b>	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
<b>Group Dwelling</b>	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p> <p>0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.</p>
<b>Residential Flat Building</b>	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p> <p>0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.</p>

<b>Row Dwelling where vehicle access is from the primary street</b>	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
<b>Row Dwelling where vehicle access is not from the primary street (i.e. rear-loaded)</b>	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
<b>Semi-Detached Dwelling</b>	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
<b>Aged / Supported Accommodation</b>	
<b>Retirement village</b>	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling.</p> <p>0.2 spaces per dwelling for visitor parking.</p>
<b>Supported accommodation</b>	0.3 spaces per bed.
<b>Residential Development (Other)</b>	
<b>Ancillary accommodation</b>	No additional requirements beyond those associated with the main dwelling.
<b>Residential park</b>	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling.</p> <p>0.2 spaces per dwelling for visitor parking.</p>
<b>Student accommodation</b>	0.3 spaces per bed.
<b>Workers' accommodation</b>	0.5 spaces per bed plus 0.2 spaces per bed for visitor parking.
<b>Tourist</b>	
<b>Caravan park / tourist park</b>	<p>Parks with 100 sites or less - a minimum of 1 space per 10 sites to be used for accommodation.</p> <p>Parks with more than 100 sites - a minimum of 1 space per 15 sites used for accommodation.</p> <p>A minimum of 1 space for every caravan (permanently fixed to the ground) or cabin.</p>
<b>Tourist accommodation</b>	1 car parking space per accommodation unit / guest room.
<b>Commercial Uses</b>	

<b>Auction room/ depot</b>	1 space per 100m <sup>2</sup> of building floor area plus an additional 2 spaces.
<b>Automotive collision repair</b>	3 spaces per service bay.
<b>Call centre</b>	8 spaces per 100m <sup>2</sup> of gross leasable floor area.
<b>Motor repair station</b>	3 spaces per service bay.
<b>Office</b>	4 spaces per 100m <sup>2</sup> of gross leasable floor area.
<b>Retail fuel outlet</b>	3 spaces per 100m <sup>2</sup> gross leasable floor area.
<b>Service trade premises</b>	2.5 spaces per 100m <sup>2</sup> of gross leasable floor area  1 space per 100m <sup>2</sup> of outdoor area used for display purposes.
<b>Shop (no commercial kitchen)</b>	5.5 spaces per 100m <sup>2</sup> of gross leasable floor area where not located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.  5 spaces per 100m <sup>2</sup> of gross leasable floor area where located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.
<b>Shop (in the form of a bulky goods outlet)</b>	2.5 spaces per 100m <sup>2</sup> of gross leasable floor area.
<b>Shop (in the form of a restaurant or involving a commercial kitchen)</b>	Premises with a dine-in service only (which may include a take-away component with no drive-through) - 0.4 spaces per seat.  Premises with take-away service but with no seats - 12 spaces per 100m <sup>2</sup> of total floor area plus a drive-through queue capacity of ten vehicles measured from the pick-up point.  Premises with a dine-in and drive-through take-away service - 0.3 spaces per seat plus a drive through queue capacity of 10 vehicles measured from the pick-up point.
<b>Community and Civic Uses</b>	
<b>Childcare centre</b>	0.25 spaces per child
<b>Library</b>	4 spaces per 100m <sup>2</sup> of total floor area.
<b>Community facility</b>	10 spaces per 100m <sup>2</sup> of total floor area.
<b>Hall / meeting hall</b>	0.2 spaces per seat.



<b>Place of worship</b>	1 space for every 3 visitor seats.
<b>Pre-school</b>	1 per employee plus 0.25 per child (drop off/pick up bays)
<b>Educational establishment</b>	<p>For a primary school - 1.1 space per full time equivalent employee plus 0.25 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site.</p> <p>For a secondary school - 1.1 per full time equivalent employee plus 0.1 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site.</p> <p>For a tertiary institution - 0.4 per student based on the maximum number of students on the site at any time.</p>
<b>Health Related Uses</b>	
<b>Hospital</b>	<p>4.5 spaces per bed for a public hospital.</p> <p>1.5 spaces per bed for a private hospital.</p>
<b>Consulting room</b>	4 spaces per consulting room excluding ancillary facilities.
<b>Recreational and Entertainment Uses</b>	
<b>Cinema complex</b>	0.2 spaces per seat.
<b>Concert hall / theatre</b>	0.2 spaces per seat.
<b>Hotel</b>	1 space for every 2m <sup>2</sup> of total floor area in a public bar plus 1 space for every 6m <sup>2</sup> of total floor area available to the public in a lounge, beer garden plus 1 space per 2 gaming machines, plus 1 space per 3 seats in a restaurant.
<b>Indoor recreation facility</b>	<p>6.5 spaces per 100m<sup>2</sup> of total floor area for a Fitness Centre</p> <p>4.5 spaces per 100m<sup>2</sup> of total floor area for all other Indoor recreation facilities.</p>
<b>Industry/Employment Uses</b>	
<b>Fuel depot</b>	<p>1.5 spaces per 100m<sup>2</sup> total floor area</p> <p>1 spaces per 100m<sup>2</sup> of outdoor area used for fuel depot activity purposes.</p>
<b>Industry</b>	1.5 spaces per 100m <sup>2</sup> of total floor area.
<b>Store</b>	0.5 spaces per 100m <sup>2</sup> of total floor area.
<b>Timber yard</b>	1.5 spaces per 100m <sup>2</sup> of total floor area

	1 space per 100m <sup>2</sup> of outdoor area used for display purposes.
<b>Warehouse</b>	0.5 spaces per 100m <sup>2</sup> total floor area.
<b>Other Uses</b>	
<b>Funeral Parlour</b>	1 space per 5 seats in the chapel plus 1 space for each vehicle operated by the parlour.
<b>Radio or Television Station</b>	5 spaces per 100m <sup>2</sup> of total building floor area.

**Table 2 - Off-Street Car Parking Requirements in Designated Areas**

The following parking rates apply in any zone, subzone or other area described in the 'Designated Areas' column subject to the following:

- (a) the location of the development is unable to satisfy the requirements of Table 2 – Criteria (other than where a location is exempted from the application of those criteria)
- or
- (b) the development satisfies Table 2 – Criteria (or is exempt from those criteria) and is located in an area where a lawfully established carparking fund operates, in which case the number of spaces are reduced by an amount equal to the number of spaces offset by contribution to the fund.

Class of Development	Car Parking Rate		Designated Areas
	Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.		
	Minimum number of spaces	Maximum number of spaces	
Development generally			
All classes of development	No minimum.	No maximum except in the Primary Pedestrian Area identified in the Primary Pedestrian Area Concept Plan, where the maximum is:	Capital City Zone
			City Main Street Zone
			City Riverbank Zone
			Adelaide Park Lands Zone
			Business Neighbourhood Zone (within the City of Adelaide)
			The St Andrews Hospital Precinct Subzone and Women's and Children's Hospital Precinct Subzone of the Community Facilities Zone
	1 space for each dwelling with a total floor area less than 75 square metres		
	2 spaces for each dwelling with a total floor area between 75 square metres and 150 square metres		
	3 spaces for each dwelling with a total floor area greater than 150 square metres.		
	Residential flat building or Residential component of a		

		multi-storey building: 1 visitor space for each 6 dwellings.	
<b>Non-residential development</b>			
<b>Non-residential development</b> excluding tourist accommodation	3 spaces per 100m <sup>2</sup> of gross leasable floor area.	5 spaces per 100m <sup>2</sup> of gross leasable floor area.	City Living Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street ) Zone Urban Neighbourhood Zone
<b>Non-residential development</b> excluding tourist accommodation	3 spaces per 100m <sup>2</sup> of gross leasable floor area.	6 spaces per 100m <sup>2</sup> of gross leasable floor area.	Strategic Innovation Zone Suburban Activity Centre Zone Suburban Business Zone Business Neighbourhood Zone Suburban Main Street Zone Urban Activity Centre Zone
<b>Tourist accommodation</b>	1 space for every 4 bedrooms up to 100 bedrooms plus 1 space for every 5 bedrooms over 100 bedrooms	1 space per 2 bedrooms up to 100 bedrooms and 1 space per 4 bedrooms over 100 bedrooms	City Living Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street ) Zone Urban Neighbourhood Zone
<b>Residential development</b>			
<b>Residential component of a multi-storey building</b>	Dwelling with no separate bedroom -0.25 spaces per dwelling  1 bedroom dwelling - 0.75 spaces per dwelling  2 bedroom dwelling - 1 space per dwelling  3 or more bedroom dwelling - 1.25 spaces per dwelling  0.25 spaces per dwelling for visitor parking.	None specified.	City Living Zone Strategic Innovation Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street ) Zone Urban Neighbourhood Zone

<b>Residential flat building</b>	Dwelling with no separate bedroom -0.25 spaces per dwelling	None specified.	City Living Zone
	1 bedroom dwelling - 0.75 spaces per dwelling		Urban Activity Centre Zone
	2 bedroom dwelling - 1 space per dwelling		Urban Corridor (Boulevard) Zone
	3 or more bedroom dwelling - 1.25 spaces per dwelling		Urban Corridor (Business) Zone
	0.25 spaces per dwelling for visitor parking.		Urban Corridor (Living) Zone
			Urban Corridor (Main Street ) Zone
			Urban Neighbourhood Zone

Table 2 - Criteria:

The following criteria are used in conjunction with Table 2. The 'Exception' column identifies locations where the criteria do not apply and the car parking rates in Table 2 are applicable.

Criteria	Exceptions
<p><b>The designated area is wholly located within Metropolitan Adelaide and any part of the development site satisfies one or more of the following:</b></p> <p>(a) is within 200 metres of any section of road reserve along which a bus service operates as a high frequency public transit service<sup>(2)</sup></p> <p>(b) is within 400 metres of a bus interchange<sup>(1)</sup></p> <p>(c) is within 400 metres of an O-Bahn interchange<sup>(1)</sup></p> <p>(d) is within 400 metres of a passenger rail station<sup>(1)</sup></p> <p>(e) is within 400 metres of a passenger tram station<sup>(1)</sup></p> <p>(f) is within 400 metres of the Adelaide Parklands.</p>	<p>(a) All zones in the City of Adelaide</p> <p>(b) Strategic Innovation Zone in the following locations:</p> <p>(i) City of Burnside</p> <p>(ii) City of Marion</p> <p>(iii) City of Mitcham</p> <p>(c) Urban Corridor (Boulevard) Zone</p> <p>(d) Urban Corridor (Business) Zone</p> <p>(e) Urban Corridor (Living) Zone</p> <p>(f) Urban Corridor (Main Street ) Zone</p> <p>(g) Urban Neighbourhood Zone</p>

[NOTE(S): (1) Measured from an area that contains any platform(s), shelter(s) or stop(s) where people congregate for the purpose waiting to board a bus, tram or train, but does not include areas used for the parking of vehicles. (2) A high frequency public transit service is a route serviced every 15 minutes between 7.30am and 6.30pm Monday to Friday and every 30 minutes at night, Saturday, Sunday and public holidays until 10pm.]

**Table 3 - Off-Street Bicycle Parking Requirements**

The bicycle parking rates apply within designated areas located within parts of the State identified in the Schedule to Table 3.

Class of Development	Bicycle Parking Rate
	Where a development comprises more than one development type, then the overall bicycle parking rate will be taken to be the sum of the bicycle parking rates for each development type.



<b>Consulting Room</b>	1 space per 20 employees plus 1 space per 20 consulting rooms for customers.
<b>Educational establishment</b>	For a secondary school - 1 space per 20 full-time time employees plus 10 percent of the total number of employee spaces for visitors.  For tertiary education - 1 space per 20 employees plus 1 space per 10 full time students.
<b>Hospital</b>	1 space per 15 beds plus 1 space per 30 beds for visitors.
<b>Indoor recreation facility</b>	1 space per 4 employees plus 1 space per 200m <sup>2</sup> of gross leasable floor area for visitors.
<b>Licensed Premises</b>	1 per 20 employees, plus 1 per 60 square metres total floor area, plus 1 per 40 square metres of bar floor area, plus 1 per 120 square metres lounge and beer garden floor area, plus 1 per 60 square metres dining floor area, plus 1 per 40 square metres gaming room floor area.
<b>Office</b>	1 space for every 200m <sup>2</sup> of gross leasable floor area plus 2 spaces plus 1 space per 1000m <sup>2</sup> of gross leasable floor area for visitors.
<b>Pre-school</b>	1 space per 20 full time employees plus 1 space per 40 full time children.
<b>Recreation area</b>	1 per 1500 spectator seats for employees plus 1 per 250 visitor and customers.
<b>Residential flat building</b>	Within the City of Adelaide 1 for every dwelling for residents with a total floor area less than 150 square metres, 2 for every dwelling for residents with a total floor area greater than 150 square metres, plus 1 for every 10 dwellings for visitors, and in all other cases 1 space for every 4 dwellings for residents plus 1 for every 10 dwellings for visitors.
<b>Residential component of a multi-storey building</b>	Within the City of Adelaide 1 for every dwelling for residents with a total floor area less than 150 square metres, 2 for every dwelling for residents with a total floor area greater than 150 square metres, plus 1 for every 10 dwellings for visitors, and in all other cases 1 space for every 4 dwellings for residents plus 1 space for every 10 dwellings for visitors.
<b>Shop</b>	1 space for every 300m <sup>2</sup> of gross leasable floor area plus 1 space for every 600m <sup>2</sup> of gross leasable floor area for customers.
<b>Tourist accommodation</b>	1 space for every 20 employees plus 2 for the first 40 rooms and 1 for every additional 40 rooms for visitors.
<b>Schedule to Table 3</b>	
<b>Designated Area</b>	<b>Relevant part of the State</b>  The bicycle parking rate applies to a designated area located in a relevant part of the State described below.
All zones	City of Adelaide
Business Neighbourhood Zone	Metropolitan Adelaide

Strategic Innovation Zone	
Suburban Activity Centre Zone	
Suburban Business Zone	
Suburban Main Street Zone	
Urban Activity Centre Zone	
Urban Corridor (Boulevard) Zone	
Urban Corridor (Business) Zone	
Urban Corridor (Living) Zone	
Urban Corridor (Main Street ) Zone	
Urban Neighbourhood Zone	

## Waste Treatment and Management Facilities

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Mitigation of the potential environmental and amenity impacts of waste treatment and management facilities.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting	
PO 1.1  Waste treatment and management facilities incorporate separation distances and attenuation measures within the site between waste operations areas (including all closed, operating and future cells) and sensitive receivers and sensitive environmental features to mitigate off-site impacts from noise, air and dust emissions.	DTS/DPF 1.1  None are applicable.
Soil and Water Protection	
PO 2.1  Soil, groundwater and surface water are protected from contamination from waste treatment and management facilities through measures such as:	DTS/DPF 2.1  None are applicable.

<p>(a) containing potential groundwater and surface water contaminants within waste operations areas</p> <p>(b) diverting clean stormwater away from waste operations areas and potentially contaminated areas</p> <p>(c) providing a leachate barrier between waste operations areas and underlying soil and groundwater.</p>	
<p>PO 2.2</p> <p>Wastewater lagoons are set back from watercourses to minimise environmental harm and adverse effects on water resources.</p>	<p>DTS/DPF 2.2</p> <p>Wastewater lagoons are set back 50m or more from watercourse banks.</p>
<p>PO 2.3</p> <p>Wastewater lagoons are designed and sited to:</p> <p>(a) avoid intersecting underground waters;</p> <p>(b) avoid inundation by flood waters;</p> <p>(c) ensure lagoon contents do not overflow;</p> <p>(d) include a liner designed to prevent leakage.</p>	<p>DTS/DPF 2.3</p> <p>None are applicable.</p>
<p>PO 2.4</p> <p>Waste operations areas of landfills and organic waste processing facilities are set back from watercourses to minimise adverse impacts on water resources.</p>	<p>DTS/DPF 2.4</p> <p>Waste operations areas are set back 100m or more from watercourse banks.</p>
Amenity	
<p>PO 3.1</p> <p>Waste treatment and management facilities are screened, located and designed to minimise adverse visual impacts on amenity.</p>	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>
<p>PO 3.2</p> <p>Access routes to waste treatment and management facilities via residential streets is avoided.</p>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>
<p>PO 3.3</p> <p>Litter control measures minimise the incidence of windblown litter.</p>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>
<p>PO 3.4</p> <p>Waste treatment and management facilities are designed to minimise adverse impacts on both the site and surrounding areas from weed and vermin infestation.</p>	<p>DTS/DPF 3.4</p> <p>None are applicable.</p>
Access	
<p>PO 4.1</p> <p>Traffic circulation movements within any waste treatment or management site are designed to enable vehicles to enter and exit the site in a forward direction.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p> <p>Suitable access for emergency vehicles is provided to and within</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>

waste treatment or management sites.	
Fencing and Security	
PO 5.1 Security fencing provided around waste treatment and management facilities prevents unauthorised access to operations and potential hazard to the public.	DTS/DPF 5.1 Chain wire mesh or pre-coated painted metal fencing 2m or more in height is erected along the perimeter of the waste treatment or waste management facility site.
Landfill	
PO 6.1 Landfill gas emissions are managed in an environmentally acceptable manner.	DTS/DPF 6.1 None are applicable.
PO 6.2 Landfill facilities are separated from areas of environmental significance and land used for public recreation and enjoyment.	DTS/DPF 6.2 Landfill facilities are set back 250m or more from a public open space reserve, forest reserve, national park or Conservation Zone.
PO 6.3 Landfill facilities are located on land that is not subject to land slip.	DTS/DPF 6.3 None are applicable.
PO 6.4 Landfill facilities are separated from areas subject to flooding.	DTS/DPF 6.4 Landfill facilities are set back 500m or more from land inundated in a 1% AEP flood event.
Organic Waste Processing Facilities	
PO 7.1 Organic waste processing facilities are separated from the coast to avoid potential environment harm.	DTS/DPF 7.1 Organic waste processing facilities are set back 500m or more from the coastal high water mark.
PO 7.2 Organic waste processing facilities are located on land where the engineered liner and underlying seasonal water table cannot intersect.	DTS/DPF 7.2 None are applicable.
PO 7.3 Organic waste processing facilities are sited away from areas of environmental significance and land used for public recreation and enjoyment.	DTS/DPF 7.3 Organic waste processing facilities are set back 250m or more from a public open space reserve, forest reserve, national park or a Conservation Zone.
PO 7.4 Organic waste processing facilities are located on land that is not subject to land slip.	DTS/DPF 7.4 None are applicable.
PO 7.5 Organic waste processing facilities separated from areas subject to flooding.	DTS/DPF 7.5 Organic waste processing facilities are set back 500m or more from land inundated in a 1% AEP flood event.
Major Wastewater Treatment Facilities	
PO 8.1 Major wastewater treatment and disposal systems, including	DTS/DPF 8.1 None are applicable.



lagoons, are designed to minimise potential adverse odour impacts on sensitive receivers, minimise public and environmental health risks and protect water quality.	
PO 8.2  Artificial wetland systems for the storage of treated wastewater are designed and sited to minimise potential public health risks arising from the breeding of mosquitoes.	DTS/DPF 8.2  None are applicable.

## Workers' accommodation and Settlements

### Assessment Provisions (AP)

Desired Outcome	
DO 1	Appropriately designed and located accommodation for seasonal and short-term workers in rural areas that minimises environmental and social impacts.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1  Workers' accommodation and settlements are obscured from scenic routes, tourist destinations and areas of conservation significance or otherwise designed to complement the surrounding landscape.	DTS/DPF 1.1  None are applicable.
PO 1.2  Workers' accommodation and settlements are sited and designed to minimise nuisance impacts on the amenity of adjacent users of land.	DTS/DPF 1.2  None are applicable.
PO 1.3  Workers' accommodation and settlements are built with materials and colours that blend with the landscape.	DTS/DPF 1.3  None are applicable.
PO 1.4  Workers' accommodation and settlements are supplied with service infrastructure such as power, water and effluent disposal sufficient to satisfy the living requirements of workers.	DTS/DPF 1.4  None are applicable.

No criteria applies to this land use. Please check the definition of the land use for further detail.